

**STATE ENVIRONMENTAL QUALITY REVIEW  
(SEQR)**

**FINAL SITE SPECIFIC/GENERIC ENVIRONMENTAL IMPACT  
STATEMENT  
FEIS**

**Proposed Action:  
City of Rochester Port Public Marina and Mixed Use Development Project**

**SEQR Environmental Impact Statement  
NEPA Environmental Assessment**

**LEAD AGENCY:  
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**Date of Acceptance;  
March 2, 2012**

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## I. Summary/SEQR Process

This is the Final Environmental Impact Statement (FEIS) for the proposed Port of Rochester Public Marina and Mixed Use Redevelopment Project. This FEIS follows, incorporates in its entirety by reference, and is supplementary to the Draft Environmental Impact Statement (DEIS) that was accepted by the Lead Agency on October 7, 2011. The proposed action was classified as a Type I action in accordance with State Environmental Quality Review Act (SEQR) and Chapter 48 of the City Code. The City of Rochester Mayor, as Lead Agency, initiated a coordinated SEQR review and issued a Positive Declaration on May 3, 2010, stating that the project would be the subject of an Environmental Impact Statement. Prior to beginning preparation of the DEIS, the lead agency elected to conduct an optional scoping process. A draft scope of the DEIS was issued by the Lead Agency on May 3, 2010 to all Involved and Interested Agencies, and a notice was mailed to those who had attended any of the previous public meetings on the proposed action. A scoping meeting was held on May 17, 2010. The final scope was issued by the Lead Agency on June 10, 2010, and is included in Appendix D of the DEIS.

The DEIS was accepted by the Lead Agency on October 7, 2011, and a Notice of Completion of the DEIS and Public Hearing issued. The notice was announced in the Democrat & Chronicle on October 7, 2011 and appeared in the Environmental Notice Bulletin on October 19, 2011. The DEIS was properly filed with all Involved and Interested agencies and made available for public review at the following locations:

1. City Hall, Rms 300A and 300B
2. Rochester Public Library- Charlotte Branch
3. Rundel Central Library
4. City of Rochester Website - [www.cityofrochester.gov/marina/](http://www.cityofrochester.gov/marina/)

A public hearing for the receipt of public comments on the DEIS was held on November 1, 2011. The public comment period was held open until November 18, 2011. A copy of the notice was mailed to all residents in the community of Charlotte, and to neighborhood associations throughout the City.

As stated above, this FEIS incorporates the preceding DEIS in its entirety Volumes 1, 2, 3, 4 and 5. This FEIS and the incorporated DEIS are both Site specific/generic documents prepared pursuant to Section 617.10 of the SEQR regulations (6 NYCRR Part 617). The environmental review encompassing this FEIS and the preceding DEIS is anticipated to conclude with the publication of this FEIS and the subsequent adoption of SEQR Findings by Involved Agencies.

At the same time, it is also anticipated that site-specific reviews, including those associated with the required approvals listed in Table I-1 (See Page 11), will follow the conclusion of this environmental review. The need for additional or further environmental review as more specific site reviews and other approvals progress will be determined by compliance with the conditions and thresholds found in the DEIS, this FEIS and the Findings ultimately adopted at the conclusion of this SEQR process. No further SEQR compliance will be required where a subsequent proposed action under consideration would be carried out in conformance with the conditions and thresholds established in these generic EIS's or statement of findings. Should a subsequently proposed action be found to have not been adequately addressed in the generic EIS's or findings further review would be required. Such further review would be expected to culminate in either a negative declaration regarding the absence of any significant environmental impacts or in preparation of a supplemental EIS should one or more significant environmental impacts be identified.

With respect to the comments to the DEIS, the Rochester Environmental Commission (REC) has reviewed the DEIS and the comments relative thereto received during the public comment period. The REC has subsequently made recommendations regarding responses to be included in this FGEIS and classified each comment, (Refer to Section VII, Appendix A of this FEIS).

As part on the formal SEQR public comment process, a Visual Preference Survey was conducted; the results are included in Section VII, Appendix B, of this FEIS.

This Final Environmental Impact Statement is available for public review at the following locations:

1. City Hall, Rms 300A and 300B
2. Rochester Public Library- Charlotte Branch
3. Rundel Central Library
4. City of Rochester Website - [www.cityofrochester.gov/marina/](http://www.cityofrochester.gov/marina/)

## II. Corrections/ Clarifications to DEIS

### A. Nomenclature Standardization

1. Lake Ontario Research Center - throughout the DEIS document there are references to the Lake Ontario Resource Center. Such references should be to the Lake Ontario Research Center.
2. Environmental Management Plan (EMP) – Throughout the DEIS there are references to the (EMP), the acronym for the Environmental Management Plan
3. Marina Maintenance and Operation Plan - Throughout the DEIS there are references to the Marina Maintenance Plan, the Marina Operations Plan and the Marina Maintenance and Operations Plan. These three documents are one and the same.
4. Material Safety Data Sheet (MSDS) – in the DEIS there are references to the MSDS, the acronym for the Material Safety Data Sheet.

### B. Text

#### 1. Table of Contents

Appendices (p xiv)  
Volume 2

##### A. City of Rochester LWRP ~~2010~~ 2011 Amendment

The Local Waterfront Revitalization Plan (LWRP) Amendment was adopted by the City of Rochester and New York State in December 2011.

#### 2. Executive Summary:

Project Location (p 2)

The second bullet is amended to read as follows: “South of the Port site, the Right-of-way Improvements extend across parcels at 490, 503, 527, 530 and 565 River Street.”

The third bullet is amended to read as follows: “Also, south of the Port site, the Lighthouse Trail project area includes two city-owned parcels at 4576 and 4580 Lake Avenue, and the privately owned parcel at 4554 Lake Avenue.” At this time, the project does not include improvements to 70 Lighthouse Street. Improvements at 70 Lighthouse, if required, will be determined during design of the project and the impacts associated with such improvements will be assessed as part of the required site-specific review. Exhibit 4 of the DEIS was revised to reflect this change. (Refer to FEIS Section III.)

Project Description (p 2)

The DEIS incorrectly states that the new zoning district created by the project is located within the existing Harbortown Village District. The new Marina Zoning District will replace that portion of the Harbortown Village District located east of Lake Avenue southerly approximately to the south limit of 4590 Lake Avenue. (Refer to the DEIS, Section 2 Proposed Action, Proposed Marina Zoning District map, Page 22)

3. Section I. Introduction

Project/Site Evolution (p 10)

The DEIS states that the City's amendment to the LWRP was adopted by City Council in May 2010. The Port LWRP amendment was approved by the New York State Secretary of State in December 2011 which formally concluded the review and approval process for the amendment.

Table I-1 (p16)

The Table has been amended to add the Environmental Facilities Corporation as an Involved NYS Agency. Refer to this FEIS III. 1.

(p 17)

The list of Interested Agencies is expanded to include The United States Department of the Interior related to its interest in the Charlotte Genesee Historic Lighthouse property at 70 Lighthouse Street.

4. Section II. Proposed Action

A. Project Location

The second bullet under the second paragraph is amended to read as follows: "South of the Port site, the Right-of-Way Improvements extend across parcels at 490, 503, 527, 530 and 565 River Street." (Refer to this FEIS, III. Exhibit No. 4. for changes to Project Parcel map)

The third bullet is amended to read as follows: "Also, south of the Port site, the Lighthouse Trail project area includes two city-owned parcels at 4576 and 4580 Lake Avenue, and the privately owned parcel at 4554 Lake Avenue." At this time the project does not include improvements to 70 Lighthouse Street. Improvements at 70 Lighthouse, if required will be determined during design of the project and the impacts associated with such improvements will be assessed as part of the required site specific review. (Refer to this FEIS, III. Exhibit No. 4 for changes to Project Parcel map)

Project Description (p 26)

Lighthouse Trail

The last sentence of the first paragraph is amended to read as follows: "Once off the City-owned land, the trail will extend along the perimeter of the adjacent property owned by Rochester Gas & Electric Corporation (RG&E) at 4554 Lake Avenue." At this time the project provides for the trail to begin at Lake Avenue and extend to the Lighthouse property. Improvements at the Lighthouse property, if required or desired, will be determined during design of the project and the impacts associated with such improvements will be assessed as part of the required site specific environmental review.

The last sentence of the third paragraph is amended to read as follows: "The project will also require acquisition of a permanent easement or fee title from RG&E to provide for the trail." The temporary access easement will only be required if the project is expanded to include improvements to the Lighthouse property.

5. Section III. Purpose, Public Benefits, Needs

D. 1.3 Lighthouse Trail (p 51)

The first bullet is amended to read as follows: A dedicated public access will be developed from the Lake Avenue ROW to the Lighthouse property.” The trail will be constructed to the Lighthouse property at 70 Lighthouse Street. The Lighthouse property fronts on Lighthouse Street which is improved with public sidewalks.

6. Section IV

F. Aesthetic and Visual Resources

Visual Impact Analysis (p 126-145)

Figure F2 which appeared on Pages 126 -145 of the DEIS has been revised to reflect changes in the Project Description and the proposed Marina District zoning code related to height restrictions on Private Development Parcel III, and the placement of street trees. (Refer to FEIS Section IV. A and B for a full description of the changes. Note: DEIS Figure F2 Proposed Views was revised to reflect the changes. Refer to FEIS Section III)

Impacts and Mitigation/ Waterfront Area (p 148)

Item no. 4 is amended to read as follows: “Views of the Marina will be available along the proposed public promenade as well as from Corrigan Street, Portside Drive and River Street. Additionally, at the east terminus of Portside Drive a scenic overlook will be constructed creating a designated destination point for public observation of the marina, harbor and Ontario Beach Park. These vantage points and the additional aesthetic resources are intended to mitigate the lost view sheds from Lake Avenue.”

G. Historic and Cultural Resources

Introduction (p 163)

NOTE: The additional updated information referenced in the DEIS on page 148 and copied below for your immediate reference was inadvertently omitted from the DEIS, and is included as Appendix E of this FEIS.

*“The 2000 Cultural Resource Survey, as well as additional updated information, is summarized below and included in Appendix P.”*

Impacts and Mitigation, Lighthouse Trail (p168)

The first sentence of the third paragraph is amended to read as follows: The Lighthouse trail would cross three parcels.” At this time the project does not include crossing the Lighthouse site. Improvements at 70 Lighthouse, if required will be determined during design of the project and the impacts associated with such improvements will be assessed as part of the required site specific environmental review.

H. Parks, Recreation and Open Space

Impacts and Mitigation (p 183)

The first sentence is amended to read as follows: “Phase I of the Marina is designed with the expectation that it and all future phases of the overall Port Development are integrated, yet phased over several years in realistic recognition that private development will depend on market conditions.”

I. Land Use, Zoning and Conformance with Adopted Plans

Existing Setting (p 190)

#### Existing Land Uses

The parcel at 4590 Lake was inadvertently omitted from the bulleted list, it is city owned vacant land formerly used as railroad right-of-way. Exhibit 4 of the DEIS was revised to reflect this change. (Refer to FEIS Section III)

#### Zoning (p 196)

##### Existing Zoning

This section is amended to delete the last sentence,

##### Proposed Zoning (p 198)

The paragraph is amended to read as follows: “The proposed Marina District includes an area that is smaller than the entire project. Former development Parcel IV (See Figure I-3), is not included in the new district and will remain in an Open Space (OS) District. Project areas/parcels outside the new Marina District will remain in the HV District or in the OS District.”

#### K. Transportation

##### Mitigation (p 277)

The first sentence on the page is amended to read as follows:

“A key operational component of this plan will be to coordinate forces from the police agencies including but not limited to the Greece Police, City Port and Special Events staff, County DOT, and Parks Department, and NYSDOT, to plan for and manage traffic and parking during events.”

##### Impacts and Mitigation (p 286)

The third paragraph is amended to read as follows:

Specific to the project area, the new trail alignment ~~will allow trail users to more easily cross~~ **avoids** the railroad tracks near River Street.

#### O. Solid Waste

##### Solid Waste Handling and Disposal (p 330)

This section referenced 6 NYCRR but failed to provide the applicable part which is 6 NYCRR Part 360-1.15b (8).

#### R. Environmental Justice

##### Traffic Issues (p 355)

The first sentence is amended to read as follows: “The first potential negative impact is traffic congestion, created by a desire of the greater Rochester Community to visit the new and improved public waterfront to enjoy the natural resources and entertainment opportunities and to visit the 430 new residences.

#### S. Temporary Impacts Related to Construction Activities

##### Phase 2 Public Improvements (p 368)

The third sentence is amended to read as follows:

“Regarding the Phase 2 Marina expansion, excavation of the enlarged basin will be immediately preceded by demolition of the Public Boat Launch.”

#### 4. Section V Analysis of Alternatives

##### A. No Action Alternative (p 374)

The first sentence of the third paragraph is amended to read as follows:  
“At the same time, the City’s commitment to the repositioning of the existing port of Rochester Terminal Building to a viable office/retail complex would likely remain unfulfilled in a “no action” scenario as there is no indication that such repositioning would be accomplished in the absence of a project like that being proposed.”

D. Relocation of the Ontario Beach Park Labor Operations Center

Alternative Relocation Sites

Alternative Site 1 (East side of Lake Avenue) (p 399)

This section is amended to delete the last sentence of the second paragraph,

III. Corrections/Revisions/ Additions to DEIS Tables, Exhibits & Figures

1. Tables, Section I. Introduction

Table I-1 Involved and Potentially Involved Agencies Under SEQR

<u>INVOLVED / POTENTIALLY INVOLVED AGENCIES</u>	<u>ACTION(S)</u>
City of Rochester	
Mayor/City Council	Funding Comprehensive Plan Amendment Zoning Map and Text Amendment Land Disposition/Acquisition Amendment to City County Parks Agreement Parkland Alienation/Dedication Official Map Amendment
Commissioner of Neighborhood and Business Development	Site Preparation Permit Flood Development Permit Demolition Permit
Manager of Zoning	Site Plan Review
City Planning Commission	Special Permit Subdivision
Traffic Control Board	Right-of-way parking/signalization approvals
New York State	
Dept. of Environmental Conservation	Article 15 Excavation and Fill Article 15 Docks, Moorings and Platforms 401 Water Quality Certification Mined Land Reclamation permit (Note: The NYSDEC has determined that this permit will not be required unless the project scope changes.) SPDES
Department of State	Funding
Dormitory Authority	Funding ( <i>CYAA Concessions Facility</i> )
Department of Transportation	Funding
SUNY College at Brockport	Lease Execution
Office of Parks, Recreation & Historic Preservation	Parkland Alienation
<b>Environmental Facilities Corporation</b>	<b>Funding</b>
Monroe County	
Executive/Legislature	Amendment to the City/County Parks Agreement Parkland Alienation
Pure Waters	Utility modification approvals
<u>INVOLVED / POTENTIALLY INVOLVED AGENCIES</u>	<u>ACTION(S)</u>
Town of Irondequoit	
Town Board	Potential New Boat Launch Development
Town Planning Board	Potential New Boat Launch Development
Town of Greece	
Town Planning Board	Potential Parking Facility





Exhibit 8

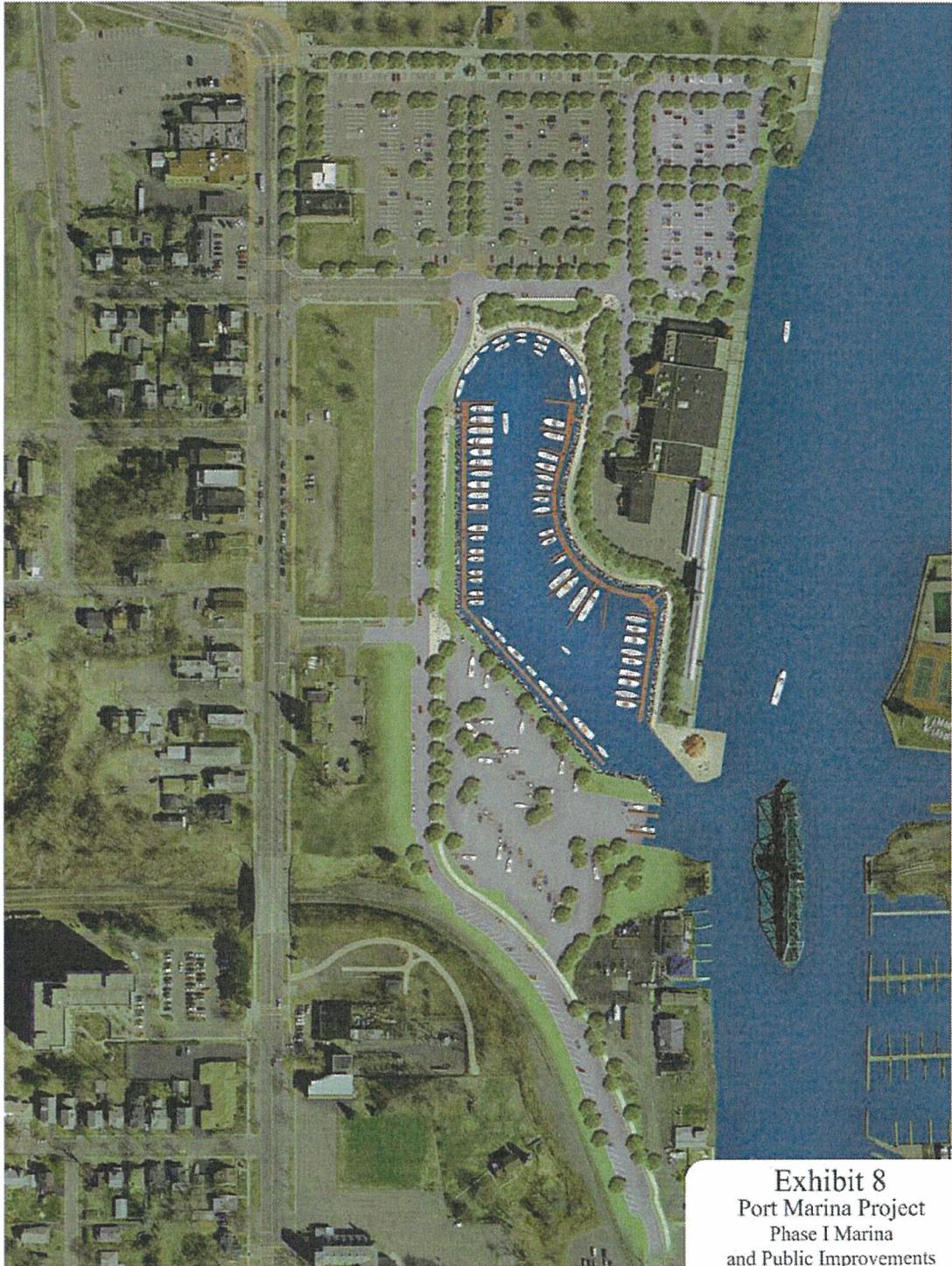


Exhibit 8  
Port Marina Project  
Phase I Marina  
and Public Improvements

Exhibit 10 (Refer to FEIS Section IV. A & B)



Exhibit 10  
Port Marina Project  
Phase I Marina and Public  
Improvements and Private  
Development on Parcel I

Exhibit 11 (Refer to FEIS Section IV. A & B)



Exhibit 11  
Port Marina Project  
Phase I Marina and Public  
Improvements and Private  
Development on Parcels I & II

Exhibit 12 (Refer to FEIS Section IV. A & B)



Exhibit 12  
Port Marina Project  
Phase I Marina and Public  
Improvements and Private  
Development on Parcels I, II & III

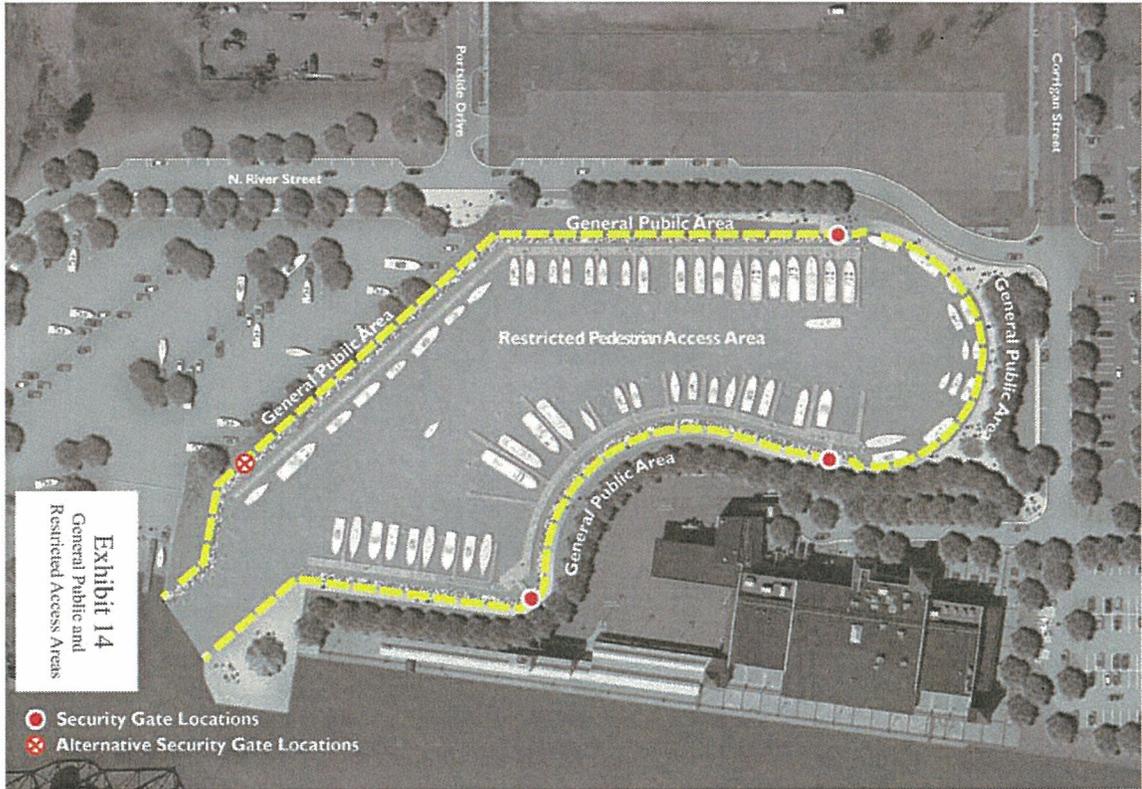
Exhibit 13 (Refer to FEIS Section IV. A & B)



Exhibit 13  
Port Marina Project  
Full Build Development  
(Phase II Marina and Private  
Development on Parcels I, II & III)

Section IV. Parks Recreation and Open Space - Exhibit 14 was created to define the general public and restricted pedestrian access areas within the marina. The floating docks and gangways will be restricted pedestrian access areas, secured by gates.

Exhibit 14



Port of Rochester Marina  
City of Rochester, New York

January 23, 2012

General Public and Restricted Access Areas

NOT TO SCALE



3. Figures

Section IV Aesthetic & Visual Resources

Figure F-2 Proposed Views

Model of Full Build Scenario for Port Project



View from adjacent high-rise apartment building (#1)



**Views from the Charlotte-Genesee Lighthouse (#2)**



**Views from proposed Lake to Lighthouse Trail Overlook (#3)**



**View from the railroad bridge on Lake Avenue looking east (#4)**



**View from just north of the railroad bridge on Lake Avenue looking northeast including the Ontario Beach Park Labor Operations Center in the foreground (#5)**



**View north on Lake Avenue approaching Portside Drive (#6)**



**View south on Lake Avenue at Portside Drive**  
**#7)**



**View east along Portside Drive from Lake Avenue facing the “link building” of the Terminal (#8)**



**View through the project site in the area of the proposed public easement (Civic Square) (#9)**



**View of River and Lake from Lake Avenue (#10)**



**View south on Lake Avenue in front of proposed parcel I (#11)**



**View from Lake Avenue looking east on Corrigan Street  
(#12)**



**View from Lake Avenue toward lake/park (northeast)  
(#13)**



**View from Pavilions in Ontario Beach Park (#14)**



**View from Ontario Beach Park (#15)**



View of project site from Carousel (#16)



View of the project site from the front of the terminal building (#17)



**View of the project site from the south end of the terminal building (#18)**



**View south along River Street in area of parcel II (#19)**





**View north along existing River Street at Portside Drive (#20)**



**View of the project site from the east side of the River (#21)**



#### **IV. Changes in Project in Response to Public Comment on the DEIS**

This section presents changes in the project based on comments received during the public comment period. DEIS Exhibits 10, 11, 12, and 13 and Figure 2 Proposed Views were revised to illustrate the changes. (Refer to FEIS Section III).

##### **A. Changes to the Proposed Marina Zoning District/Parcel 3**

The project plans and the proposed Marina District form-based code presented in the DEIS provided for development of a tall building on Parcel III, with a maximum height of 16 stories, but required the massing of the building to be divided into three stepped increments, with the lowest portion having a maximum height of three stories and the center portion having a maximum height of six stories. The tall portion of the building was required to be located farthest to the east (closest to the Genesee River). It was intended that this building massing would preserve a sufficient view of the lake and river, with the new marina in the foreground, from the lighthouse and the new lighthouse trail.

Based on comments received during review of the DEIS regarding the height allowance on Parcel III and the potential to block views of the harbor from the Lighthouse, the project plans and the proposed Marina District zoning code have been revised. The code was changed (Refer to FEIS, Appendix C for revised Marina District Zoning Code) to limit the height of a building on Parcel III to six stories with no portion of the building having an elevation greater than 320 feet above sea level. This may in reality limit the height of a building to five stories, but the topography of this parcel is irregular enough that a partially below grade, partially above grade level may be feasible, which could make the building six stories. The stipulation that no portion of the building may exceed 320 feet above sea level should be sufficient to preserve an unbroken vista of the lake from the top of the lighthouse. Also, the code no longer requires the massing of the building to be divided into stepped increments, which will allow for greater design flexibility, without impacting the vistas of the lake.

To prepare for the revisions to the code, a model was analyzed to determine at what elevation the site line from the top of the lighthouse to the lake would pass over Parcel III. Figure IV. A. 1, below illustrates how the site lines would pass over the roof of a building at a height of 322 feet above sea level. The lighthouse is at the far left side of the picture and the lake is at the far right side of the picture.

Figure IV. A 2 below presents the model view from the lighthouse tower to the harbor. The Figures (I-4, I-6 and I-7) below from the DEIS were revised to illustrate how the reduced height on parcel 3 impact the project plan

Figure FEIS IV A. 1

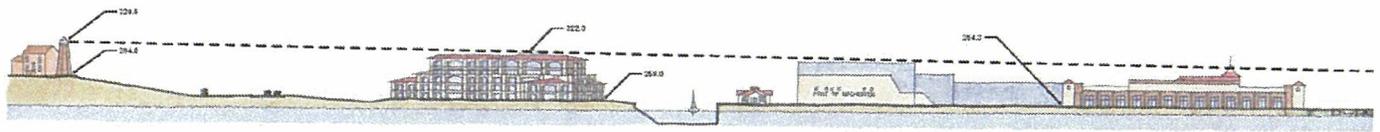


Figure FEIS IV. A. 2



Figure I-4



**Figure I-6**



**Figure I-7**



## B. Changes to the Lighthouse Trail Project

The scope of the Lighthouse Trail Project as presented in the DEIS was based on preliminary concept plans and included installation of trail improvements upon 70 Lighthouse Street, the site of the historic Charlotte Genesee Lighthouse and Lightkeeper's House. It is important to note that these improvements had not been reviewed by the parties responsible for the site or the Lighthouse including the Charlotte Genesee Historical Society (operators), the County of Monroe (landowners), and the US Secretary of the Interior (an interested party in the deed). Also important to note, the application prepared by the City for the Lighthouse Trail project did not include drawings which provided for improvements to 70 Lighthouse Street.

Subsequent to acceptance and issuance of the DEIS, City officials met with members of the Board of the Charlotte Genesee Historical Society and it was decided that the desire or need for improvements at this property would be determined during the design phase of the project which will involve interested parties review and approval. For purposes of the FEIS, the scope of the Lighthouse Trail Project has been revised to remove all references to installation of improvements upon 70 Lighthouse Street. Impacts associated with such improvements, if improvements are desired, will be assessed as part of the required site specific review. Refer to this FEIS, Section II, B for associated corrections to the DEIS document and to Section III, 2. Exhibit No. 4

Also, at that meeting, the Board discussed a concern related to the placement of street trees along River Street extension, shown in Figure F-2 of the DEIS, which when full grown could impact views of the Lighthouse from Ontario Beach Park and the marina. Some of these trees have been removed from the plans (Refer to FEIS, Section III. 3. Figure F-2). Street trees are an important element of a streetscape providing shade, a more attractive streetscape, traffic calming, a reduction in the urban heat island effect, and a reduction in greenhouse gases. With all these factors in mind, tree plantings and species will be selected and placed in such a way to minimize the impact on views to and from the historic lighthouse.

**V. Information Supplementary to the Draft GEIS**

This section presents additional information provided by the availability of new information and the need for clarification.

**A. Additional Analysis/Estimate of Anticipated Natural Gas Service Needs**

**This information was developed in response to RG&E’s request for detailed loading estimates for the private development, but was inadvertently omitted from the DEIS..**

**(Prepared September 1, 2011)**

1. Overall Project Plan

Overall site which shows the following residential units:

<u>Parcel 1 North</u>	<u>Potential Building Occupancy*</u>
• Phase I Commercial/Restaurant 1 4000 sf Townhomes 4 1800 sf Condominiums 35 Vary (1 Bedroom to 3 Bedroom)	Spring 2014
• Phase II Commercial Office 2 2000 sf Commercial/Restaurant 2 2000 sf Townhomes 10 1800 sf Condominiums 52 Vary (1 Bedroom to 3 Bedroom) Pool & Clubhouse	2014-2016
<u>Parcel 1 South</u> Commercial Office 1 4000 sf Commercial/Restaurant 1 4000 sf Townhomes 17 1800 sf Condominiums 96 Vary (1 Bedroom to 3 Bedroom) Pool & Clubhouse	2016-2018
<u>Parcel 2</u> Commercial/Office 1 2000 sf Commercial/Restaurant 1 2000 sf Townhomes 16 1800 sf Condominiums 82 Vary (1 Bedroom to 3 Bedroom) Pool & Clubhouse	2018-2024
<u>Parcel 3</u> Condominiums 121 Vary (1 Bedroom to 3 Bedroom) Pool & Clubhouse	2018-2024

\* Following completion of Parcel I, development of either Parcel 2 or 3 could occur next in sequence. While possible, it is unlikely they would be developed concurrently.

2. Overall Description of Anticipated Gas Services

Based on experience with similar projects, the following gas needs are anticipated for the residential units:

- Typical residential unit is a 2-bedroom, 1500 sf condominium with approximately half of the units serving a year-round population and the other half used seasonally. Generally, the quality of the construction is luxury/upper end with higher-end appliances. The heating systems will likely not contain a large boiler to serve the entire building. It is anticipated that each unit will have individual high-efficiency 98-99% forced-air gas / air conditioning units for each condominium. It is anticipated that 50% of the water heaters would be a water tank design and the other 50% of an on-demand hot water system (the reasoning being for second home units versus primary residences). The kitchens would typically have a four-burner gas stove with one oven, and the other appliances would be electric. Generally speaking, there are no cooking grills on the decks of the units as there may be some cooking grills using propane tanks in a common area or clubhouse location. Each unit would have one gas-fired fireplace.

Typical peak usage for a 1,500 sf condominium unit consists of the following:

- o Water Heater (40,000 BTU)
- o Furnace (115,000 BTU)
- o Dryer (20,000 BTU)
- o Range (56,000 BTU)
- o Fireplace (125,000 BTU)

**Total: 261,000 BTU**

- Each large building will also likely have a small, heated swimming pool (20' x 30' x 4' deep) with gas heater. The clubhouse typically would be 2,000 sf with a gas-fired, energy-efficient HVAC system (similar to a residential unit).

Typical peak usage for a 2,000 sf pool clubhouse and pool consists of the following:

- o 2 Water Heaters (80,000 BTU)
- o Furnace (115,000 BTU)
- o Pool Heater (300,000 BTU)

**Total: 495,000 BTU**

- Typical townhome units would be a mix of one- and three-bedroom units, averaging approximately 1,800 sf.

Typical peak usage for a 1,800 sf townhome consists of the following:

- o Water Heater (40,000 BTU)
- o Furnace (115,000 BTU)
- o Dryer (20,000 BTU)
- o Range (56,000 BTU)
- o Grill (36,000 BTU)
- o Fireplace (125,000 BTU)

**Total: 299,000 BTU**

- 4,000 sf Restaurant Spaces:

Typical 4,000 sf restaurant peak usage consist of the following:

- o 2 convection ovens (44,000 BTU per oven)
- o 2 fryers (100,000 BTU per fryer)
- o 1 char grill (120,000 BTU)
- o 1 range (99,000 BTU)
- o 1 salamander broiler (40,000 BTU)
- o 1 pizza oven (125,000 BTU)

- o 1 80-gallon water heater (130,000 BTU)
- o 1 furnace (115,000 BTU)
- o 1 convection steamer (32,000 BTU)
- Total: 949,000 BTU**

- 2,000 sf Restaurant Spaces:

Typical 4,000 sf restaurant peak usage consist of the following:

- o 2 convection ovens (44,000 BTU per oven)
- o 1 fryer (100,000 BTU per fryer)
- o 1 char grill (120,000 BTU)
- o 1 range (99,000 BTU)
- o 1 salamander broiler (40,000 BTU)
- o 1 furnace (115,000 BTU)
- o 1 50-gallon water heater (40,000 BTU)
- o 1 Convection Steamer (32,000 BTU)

**Total: 634,000 BTU**

- 4,000 sf Commercial Office Spaces:

Typical 4,000 sf restaurant peak usage consist of the following:

- o 80-gallon water heater (1300,000 BTU)
- o furnace (115,000 BTU)

**Total: 245,000 BTU**

- 2,000 sf Commercial Office Spaces:

Typical 4,000 sf restaurant peak usage consist of the following:

- o 50-gallon water heater (40,000 BTU)
- o furnace (115,000 BTU)

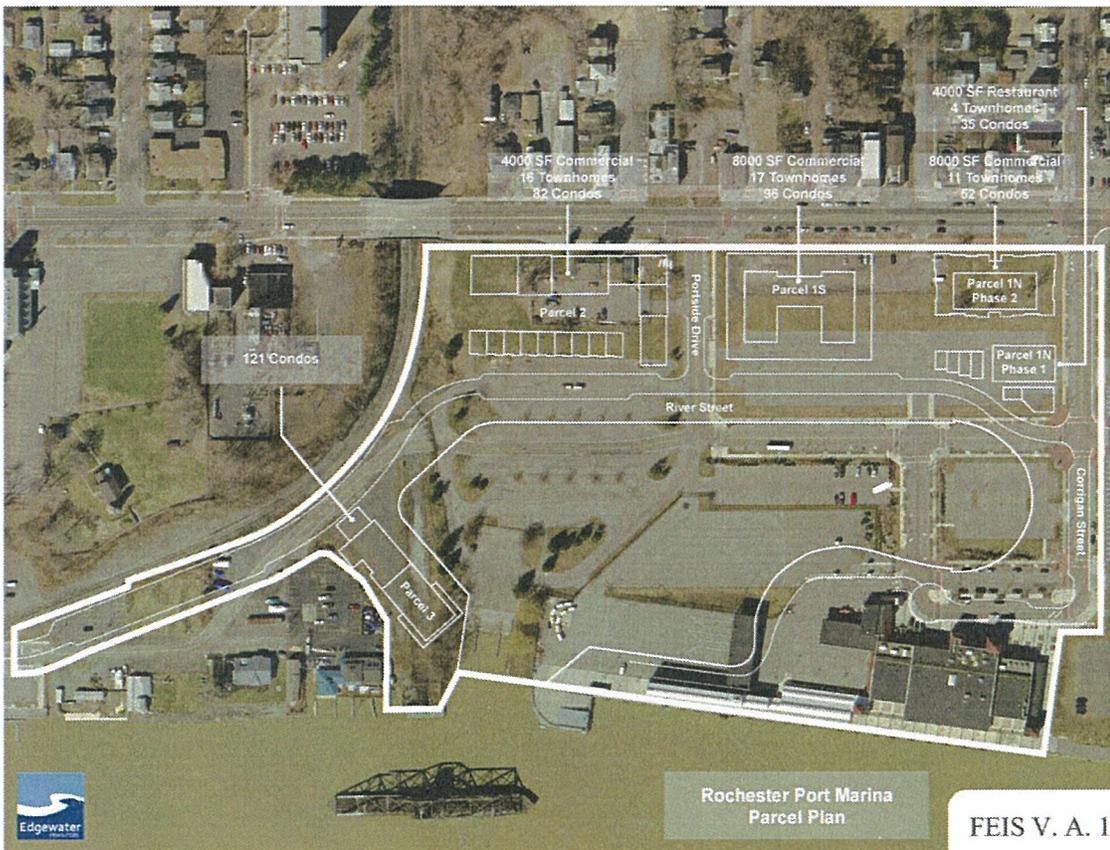
**Total: 155,000 BTU**

3. Total Anticipated Gas Peak Usage

Based on the above project plan and anticipated gas usage by unit, peak usage for each parcel/phase estimates are as follows:

- Parcel 1 North Phase I 11,280,000 BTU
- Parcel 1 North Phase 2 18,635,000 BTU
- Parcel 1 South 31,828,000 BTU
- Parcel 2 27,470,000 BTU
- Parcel 3 32,076,000 BTU

Figure V. A. 1 Additional Analysis/Estimate of Anticipated Natural Gas Service Needs



**B. Visual Preference Survey Results**

The results of the Visual Preference Survey which was made part of the DEIS have been tabulated and are available for review. (Refer to Section VII. Appendix B of this FEIS)

- C.** Related to Section IV. G of the DEIS, as requested by New York States Parks Recreation and Historic Preservation, an overlay to the 1829 Map was created. (Refer to Section VII. Appendix F of this FEIS)

**VI. Response to Public Comment**

**Following is a chart that summarizes the comments, both verbal and written, that were received during the comment period of October 7, 2011 to November 18, 2011, and provides a response to each comment. The complete comment summary and disposition recommendation report, as presented by the Rochester Environmental Commission, is attached as Appendix A**

<b><u>COMMENT CATEGORY: Proposed Action</u></b>
Comment: We strongly support and encourage a phased approach to the proposed development.
<b>RESPONSE: Refer to the DEIS, Section II Proposed Action, B. Project Time Table, Page 20 for full description of phased project implementation schedule.</b>
Comment: Boat owners will be insisting on good security for their boats, so appropriate decorative fencing should be planned now rather than later.
<b>RESPONSE: The design of the marina includes limited access points for entering the secured boater area. These access points will be controlled with fencing or other barrier. The entire perimeter will not be fenced.</b>
Comment: Is the City committing to having a replacement launch facility, or multiple if deemed necessary, with equivalent launching and parking capacity, fully operating prior to the closing of the existing ramp and commencement of Phase II?
<b>RESPONSE: Yes. The City will have an operational equivalent launch prior to closing the existing launch.</b>
Comment: While we do not have concerns with the proposed dates, the September 30, 2011 letter indicates that the reconfigured boat ramp is expected to be completed by May of 2012 whereas the DEIS suggests that this will not occur until May 2013 (Table S-1).
<b>RESPONSE: The schedule provides for completion of the launch and associated parking reconfiguration by May of 2013.</b>
Comment: Marina Operations and Maintenance Plan: In the City's September 30, 2011 letter you indicate that a Marina Operations and Maintenance Plan would be included in the DEIS. We were unable to locate the referenced plan.
<b>RESPONSE: A preliminary marina operations and maintenance plan has been prepared, portions of which will be updated by the marina manager at a later date. The preliminary plan is appended to this FEIS, Appendix D.</b>
Comment: Thank you very much for sending the draft document to us for our review and comments. The only discrepancy of note that should be corrected in the final version is the proposed name of Brockport's planned facility at the Port. The name has changed several times over the years and the correct version is "Lake Ontario Research Center." We noted that the facility is incorrectly called the "Lake Ontario Resource Center."
<b>RESPONSE: So noted. See FEIS Section II. Corrections/Clarifications to DEIS, A. Nomenclature Standardizations &amp; Corrections</b>
Comment: Right now the Charlotte beach area is free to all residents and there is abundant parking for all people. There is a great view of the beach and it is easy to get to. I am concerned that this new project will not enable people the full access that they are used to. I understand parking will be limited and there will be a parking fee. This will be a hardship for people with limited funds.

**RESPONSE:** This project does not propose any changes to Ontario Beach Park (Charlotte Beach), access to the Park, and does not propose fees for public parking. The existing designated parkland parking will not be altered and will remain in its current configuration. The project will reduce overall public parking capacity at the Port creating some deficit. Refer to the DEIS, Section IV. K Transportation, 2.4 Parking Management, Page 261 for the full disclosure, identified impacts and recommended mitigation. An initiation of a parking fee at the Port has been discussed at various times over the last decade as a way to address costs related to maintenance of the public parking facilities, but as stated above are not proposed as part of this project.. The proposed private development will alter and eliminate some of the existing views of the park, the lake and the Genesee River from Lake Avenue, but will create attractive new views of the marina, Lake Ontario and the Genesee River. Refer to the DEIS, Section IV F Aesthetic and Visual Resources, 4. Impacts and Mitigation, Page 148.

Comment: The other concern is for the present businesses, and future retail and residential development in relationship to parking and patron convenience.

**RESPONSE:** All new businesses and residential construction will be required to provide parking for patrons and tenants in accordance with the type of development as indicated in DEIS Section IV.K Transportation 2.4 Parking Management.

Comment: We are seeing less people interested in boating and less interest in larger sized boats. The size of the proposed marina should be looked at.

**RESPONSE:** Boating trends across the Great Lakes, supported by multiple market surveys completed in 2011 indicate that the highest demand for slips is for boats 35' and longer, with waiting lists longest for boats in the 40'-50' range. This follows the trend over the last decade towards larger boats, and also the fact that smaller vessels are easily trailer-able. The cost to store a boat on land or in water over the summer is much the same, therefore boaters tend to keep their boats in the marina regardless of economic conditions.

Comment: If we sell public land then the developers will have a field day and build as many units as they can simply to use up any and all available space.

**RESPONSE:** The number of units is limited by the proposed Zoning Code. Section 120-77.2G states the following:

(2) Any development proposal that will cause the total number of dwelling units in the Marina District to exceed 430 and/or cause the total amount of commercial space in the District, not including existing commercial space in the Terminal Building, to exceed 44,000 square feet, shall require a Supplemental Environmental Impact Statement pursuant to the requirements of the State Environmental Quality Review Act and Chapter 48 of the City Code.

The City owns the land and will sell it to developers through the use of land disposition agreements. These agreements set forth limitations for development. Public access easements and designated open areas will further limit the developable area.

Comment: In this economy who is going to buy/rent high priced units? I am afraid the demand is not there and these units will sit idle and partially constructed creating an eyesore for the city. I think this project needs to be viewed very closely and take in the concerns of the people and not focus on money.

**RESPONSE:** Based on recent meetings with local developers and a 2011 update by Edgewater Resources to the marina housing market study, there is a adequate demand for the planned phased housing development and other similarly scaled housing projects have been successful in the Great Lakes in places as close as Buffalo. The planned marina is an important amenity and will increase the marketability of the proposed adjacent housing units. The City is planning to issue a request for proposal for private development of development area 1 at a point when we have a firm schedule for the marina construction. This will be a competitive proposal process. It is expected that each developer will perform additional market analysis specific to its plan and will base the number of units, pricing, and square footage on the results of the analysis. Developers will be required to demonstrate financing for the project to the City. It is anticipated that given current lending requirements there will be requirements for presales of units and developers will be cautious about the number of units proposed. It is expected that private development will take place over several years allowing gradual absorption of the housing into the local market.

<p>Comment: We are concerned with the relocation, size (3 of pads) and parking facilities for the new boat launch. We request that the new (boat) launch have four pads.</p>
<p><b>RESPONSE: The relocated launch(es) will provide 4 pads.</b></p>
<p>Comment: We are also concerned with slip availability. We request that some percentage,(of boat slips) , ten percent perhaps, be made available for charter boat operators in Monroe County. If the current 85 slip marina is realized this would reserve 8 slips for charter boats. We further request that these slips be adjacent to each other. This is a common situation at ocean-side marinas. It is an attraction for people to visit the marinas when the charters return with their catches.</p>
<p><b>RESPONSE: The proposed marina does not preclude the leasing of docks for charter operations.</b></p>
<p>Comment: Concerned that w/in 2 yrs. Developers will want to develop parking lots to serve private commercial &amp; residential development</p>
<p><b>RESPONSE: The period of high demand for parking lasts only 2 – 3 months, making it less likely that a developer would construct a parking facility. The zoning code has provisions to address such a parking facility in the event it is proposed. Within the proposed Marina District, the form-based code contains a section that governs the design of interim parking facilities and requires that they be specially permitted on a temporary basis by the City Planning Commission. In the surrounding HarborTown Village District, community garages and parking lots are also a specially permitted use that must be approved by the Planning Commission. In both cases the public has the opportunity to speak before the Planning Commission at a public hearing.</b></p>
<p>Comment: The plan must detail how an effective security program for the area will be provided. This program should also specify how noise, especially music, motorcycle and muscle car noise is to be controlled. In addition, the plan should contain specifics for handling unlawful tenant and overnight without accommodations persons.</p>
<p><b>RESPONSE: This is a plan for the physical development of the port area, and does not directly address issues of law enforcement. That being said, the project will change the character of the area from one dominated by parking lots to one designed for pedestrians. The current parking lots are largely unsupervised, which provides a venue for ‘hanging out,’ especially ‘hanging out’ right next to the muscle cars, motorcycles, etc. New shops, restaurants, apartments, condos, hotel rooms, etc will increase the population of people in the area for ‘legitimate’ reasons and will increase the natural surveillance that comes with ‘eyes on the street.’ National experience has shown that an increase in ‘legitimate’ activity will displace ‘illegitimate’ activity. The existing music venues are outside the project area and are beyond the scope of the project. Outdoor entertainment venues within the proposed Marina District must be approved by the City Planning Commission through the public hearing process. Owners of new buildings within the Marina District will find it in their own self-interest to avoid antagonizing their residential tenants by not renting commercial space to businesses that are likely to become nuisances.</b></p>
<p>Comment: The Monroe County storage facility has already been discussed to be built near the Charlotte baseball fields by Corrigan and Estes. This would be a travesty for the local residents as well as the youngsters who play soccer and baseball on these fields all summer long. This is also a huge parking area for overflow that will only increase with even the Phase 1 development. This facility is not well kept and a complete eyesore. It should be kept on a main access road and not in the backyards of Charlotte residents.</p>
<p><b>RESPONSE: Investigations and analysis of alternative sites continue for relocation of the City owned County operated Park maintenance facility. The alternative site located near the Charlotte ball fields in Ontario Beach Park is one of 4 sites identified to date. Implementation of the maintenance facility relocation does require a site specific environmental review. Refer to the DEIS, Section V Analysis of Alternatives, D Relocation of Ontario Beach Park Labor Operations Center, Page 398.</b></p>
<p>Comment: Given the historic lack of development in Charlotte, I am skeptical of these plans coming to fruition. I don't know where the occupants of the housing units will come from, and why they would locate in Charlotte. I don't want to see it become necessary to subsidize the units, through tax waivers to the developers, or other means. If there hasn't been a survey specific to the proposed housing, then one needs to be done. While studies may show a market for hundreds of lake front units, I'd want to see one for this project alone. Have the two yacht clubs, or area boaters, especially those now docking in the River, been surveyed about marina use, and the desire to live in the project.</p>
<p><b>RESPONSE: Although a formal survey of local yacht club members has not been performed, the City's marina project team has investigated the level of interest in the type of housing that is planned. A marina market study was completed that also included an analysis of the potential market for marina related</b></p>

housing. Discussions have taken place with marinas, yacht club members, boaters, and developers. In addition, the City occasionally receives direct inquiries from individuals and real estate brokers regarding the timing and availability of housing and marina slips. The City does not plan to subsidize the private development and is looking at options for how the City will be compensated for the value of the land that would be sold to the developer. It is also anticipated that the developers submitting proposals to the City will perform additional project specific market analysis and will base the proposed number of units, pricing, and square footage on the results of the analysis.

Comment: Lighthouse Trail project- Why would the City spend \$ 150,000 to build a trail when the site is already accessible. Rather for smaller port related activities. The Lighthouse should be accessed by the historic route of lighthouse street.

**RESPONSE:** Access to the Lighthouse site from historic Lighthouse St will remain and is likely to be improved as part of the project. The public benefits from the project will include, but not be limited to, pedestrian access. In the upcoming design phase, opportunities to maximize the scenic, historic, and recreational assets of the project area, will be identified and , within the project scope and budget, advanced.

Comment: Private residential development should occur along Lake Avenue.

**RESPONSE:** The project proposes incremental private development at three properties owned by the City that front Lake Avenue on the east 4600, 4650 and 4752 Lake Avenue. It is anticipated and desirable that this project enhance potential for additional development/redevelopment in the immediate project area, within the adjacent neighborhoods and along both Lake Avenue and River Street. Refer to the DEIS, Section IV M Growth Inducing Impacts, Page 306.

**COMMENT CATEGORY: Alternatives**

Comment: Concentrate new residential in a tall tower at the former miniature golf course. Height is preferable to a sprawling footprint, giving the additional advantage of spectacular vistas from the apartment units. I would look to having a “residential hotel” there, with function rooms and exhibit rooms that would accommodate lighthouse functions and a site visitor’s center.

**RESPONSE:** The site of the former miniature golf course at 4576 and 4580 Lake Avenue is the location of the proposed Lighthouse trail and public overlook. This is an important feature of the project to as it provides for mitigation of the loss of existing waterfront views. It will also be a point for views of the new marina.

Comment: People do not visit the port area to visit parking lots. That function can best be accomplished in a long parking garage along Lake Avenue, as I testified at the public session. Instead of acres of asphalt, that area is best given over to expansion of that very popular park. So much macadam & so many parking spaces – Shouldn’t parking be concentrated in a structure? That would free up more land for the park. The current Parks maintenance facility site is a good spot for parking structure

**RESPONSE:** This entire project is about creating a place worth visiting, worth living in and worth caring about. Surface parking lots invariably devalue places that should be built around people, not cars. Structured parking, however, is very expensive. The national average parking garage cost is about \$15,000 per space. This would be a major investment for the city to make in an area that sees only 2 - 3 months of high demand each year. There is nothing in the plan to prevent a private developer from constructing a garage. The proposed Marina District form based code regulates the location of parking ingress/egress and requires all surface parking or parking structures to be wrapped with ‘liner buildings’ that screen parked cars from view and contain active uses along the streets. Liner buildings would also provide revenue needed to offset the cost of parking.

Comment: The plan should include more parking, not less than is currently available. This may involve a low parking garage built into the embankment at the current Monroe County park service location.

**RESPONSE:** The current parking lots are largely unused for most of the year. Unless a pattern of consistent demand for parking exists year-round, it is not financially viable for the City to construct a parking garage. Parking garages typically cost from \$15,000 to \$25,000 per parking space to construct and required significant operation and maintenance resources. Additional on-street parking will be created by the plan, but the amount of off-street parking will be reduced. Based on an extensive parking and traffic study the planned available public parking will generally be adequate to meet demands except when large

events are being held. During these events remote parking and shuttles will need to be established. Refer to the DEIS, Section IV. K Transportation, 2.4 Parking Management, Page 261 for the full disclosure, identified impacts and recommended mitigation.

Comment: An iconic boardwalk along the waterfront should be considered; preferably in the area north of the port terminal building.

**RESPONSE: All boardwalks along the waterfront will be retained. Currently there are three, the boardwalk along the beach in Ontario Beach Park which is operated and maintained by the County, the boardwalk owned by the City along the river which begins at the Terminal Building and connects to the Charlotte Pier which is under the jurisdiction of the Army Corps of Engineers, and the boardwalk located at the former Beach Avenue ROW between Lake Avenue and the river. These boardwalks will not be impacted by the project except to be enhanced with improved connections to the Genesee Riverway Trail, the public sidewalk system and the proposed new public promenade along the perimeter of the new marina replete with benches, decorative lighting, and other typical landscape treatments.**

Comment: I don't feel that the project encourages or promotes a ferry operation, which would draw people to the port, not the development of private residences. This area needs port activity, possibly light shipping. In other words, it should function as a PORT.

**RESPONSE: The project preserves the City's Port terminal buildings and deep draft docking facilities. The availability of these facilities will continue to offer the potential for a future passenger ferry service as well as increased Great Lakes cruise ship visits.**

The project does eliminate the possibility of a ferry service that would bring vehicles into and out of the Port of Rochester. Opportunities for small volume commercial cargo shipping into the Terminal facilities will be limited in the future. It is important to note that there has been very little demand in recent years for commercial shipping into or out of the Port of Rochester. It has been decades since the Port offered services typically associated with commercial shipping including cranes, rail connections, and container storage areas. Over the past thirty years City and community interest in the Port has shifted in emphasis from commercial shipping to recreational uses. This change has been reflected in the plans for the area including those for the current project.

**COMMENT CATEGORY: Community Character**

Comment: A select number of businesses should be allowed to develop but not to the point of taking away from the family businesses that have been in this area for years, who are also members of this community, including Mr. Dominic's, Abbott's, and Windjammers. There are also businesses in the Port of Rochester that have struggled to survive with the failure of the ferry and these businesses should also be given consideration. One exception would be Pier 45, this is an example of a business that I fear. the city has been running this establishment at an enormous loss each year, at tax payer costs. your plan wants to add businesses to this area where many are struggling as it is. Small, family businesses that have been in this community for years and decades should be given consideration prior to letting private developers do with our local community as they wish, and unfortunately the EIS as it stands is vague enough for developers to do just this.

**RESPONSE: The more residents, boaters, visitors in the area will only help the existing businesses thrive...**

Comment: The proposed code is not acceptable to the neighborhood, it does not correlate with the Design guidelines that the community developed and submitted to the City during the other recent port project. Villages have been working to protect & enhance village character  
Height restriction is important to protect Village character. Any building must fit into look of Village. Respect history of shipping in the Village

**RESPONSE: Section 120-161 and 120-192C. of the Zoning Code provide for the official adoption of neighborhood design guidelines through the City Planning Commission. If a neighborhood has prepared design guidelines, then they can pursue the adoption of them through the City Planning Commission. The project area is currently part of the Harbortown Village District, which contains design standards. The city-wide design guidelines and standards, found in Article XIX of the Zoning Code are also applicable in the Harbortown Village District. The proposed Marina District will be separate from Harbortown Village District, and its form-based code will be independent of Article XIX. Nevertheless, there is nothing in the proposed Marina District code that contradicts either the Harbortown Village District or the city-wide design guidelines and standards. The Marina District code utilizes the town planning principles that**

<p><b>underlie traditional villages, towns and cities and goes much further toward establishing a predictable development environment than any other section of the city's current Zoning Code.</b></p>
<p>Comment: How can noise pollution from beach concerts, nighttime partying at the marina, and additional restaurants and recreation venues be restricted or contained for the benefit of the anticipated residents in the mixed-use development?</p>
<p><b>RESPONSE: A certain amount of noise is to be expected in a mixed use environment. Addressing excessive noise is mostly a management and law enforcement issue. There is no noise barrier that could be constructed to keep noise from outside the project area (as all current noise sources are) from affecting the area. The proposed Marina District code requires the City Planning Commission to approve any new outdoor entertainment venues as is currently required in the surrounding Harbortown Village district. If such venues are approved and later prove to be a problem, it becomes a law enforcement issue. It will be in the city's best interest to more strictly control noise and similar nuisances in the area in order to protect its substantial investment. It will also be in the interest of the new building owners not to offend their residential tenants by permitting inappropriate activities.</b></p>
<p>Comment: I am very concerned with the height that your special codes would allow for. I firmly believe these should be taken out of your EIS, because leaving them in only leaves the opportunity for private developers to take away the beauty of the water that is Charlotte and what attracts people to our small community. Charlotte is not a big city, it is a small close knit local community. There does not presently exist a building higher than three stories, nor should one ever be allowed to be built.</p>
<p><b>RESPONSE: Buildings of any height will block views from Lake Avenue. It should be understood that views from Lake Avenue will be replaced by views from the new River St as the primary viewing point from which to see the river &amp; the lake. The project proposes that the new viewing experience will eliminate the views of vast barrier of parking lots between the street &amp; the river &amp; bring people into much more intimate contact with the water. The Marina District code contains provisions that are intended to both minimize the impact of height and promote the beauty of all the buildings in the district. Buildings taller than 5 stories are allowed only on Parcel I on each side of the civic square (only 2 buildings in the entire area can be taller than 5 stories). Where buildings are allowed to exceed 5 stories, the code requires that all floors above the 4<sup>th</sup> floor (5<sup>th</sup> floor on the River St side to account for the change in elevation between Lake Av &amp; River St) be stepped back 30ft from the façade of the lower floors. The quality of the ground floor frontages of the buildings where they meet the sidewalk &amp; the civic square is the primary ingredient in the creation of a quality place. The code contains many provisions to ensure a best possible pedestrian-oriented street space. The code contains requirements that the top 2 stories employ step-backs, roof forms or other techniques to create a distinctive roof profile. Local examples of buildings that use such techniques include the Kodak tower &amp; the Times Square building &amp; the code contains photos of buildings from other places that illustrate the idea of a distinctive profile. The code does not allow a flat-topped monolith.</b></p>
<p>Comment: The community needs to maintain some architectural overview on private development.</p>
<p><b>RESPONSE: This is the essence of what the proposed form-based zoning code does. The code's Regulating Plan determines where each building type is permitted. The Building Envelope Standards, for each building type, govern building placement, building frontage types, building heights and in some cases go into detail about where taller parts of the buildings are &amp; are not allowed. There is also a separate section in the proposed code of Architectural Standards &amp; Guidelines that deal with Context &amp; Architectural Character, Composition &amp; Articulation, Building Façade Walls, Wall Openings (windows, doors, etc), Roofs and Projecting Façade Elements. Each of these standards also addresses acceptable materials. The whole code is meant to work together to create an overall urban composition that places as much or even more importance to the spaces between the buildings as to the buildings themselves. It is all about creating a coherent place and not a disjointed collection of individual buildings. It is anticipated that the new code will do more to maintain better architectural overview on private development than any section we have in our existing code &amp; do it in a way that provides predictable outcomes. The proposed code will require applications for construction of any new building or addition within the Marina District that does not comply with the requirements of the proposed code to be subjected to major site plan review which brings in the involvement of the City's Project Review Committee (PRC). The PRC is composed of City Hall design professionals and managers and three outside design professionals who advise the Director of Planning and Zoning on site plan approvals.</b></p>
<p>Comment: Noise level from the Bands. Each year they get louder and louder. The sound systems are very sophisticated. Nola's outdoor bandstand, and the battle of the bands along the river at each bar. Certain types of</p>

music are worse than others. . Jazz, oldies, soft rock, folk, county or swing seem better. But the loud rock and rap only seem to amplify the noise across the river. Please remember this is still a residential area, we pay taxes, and there are sound ordinances that should be considered and respected. An overly hyped up crowd isn't always good!

**RESPONSE: All of the band venues mentioned are outside of the project area and are beyond the scope of the project. Within the project area, the proposed Marina District code lists outdoor entertainment as a specially permitted use. This means that any new outdoor entertainment venue must be approved by the City Planning Commission. This is the same requirement that currently exists in the Harbortown Village District. Since a significant amount of housing is likely to be constructed in the Marina District, there will be new neighbors who will have to be taken into account when such venues are applied for.**

Comment: There is a perception by many people that Charlotte is a party place, drug infested and after dark it only gets worse. Please work to change the dynamics in Charlotte and address crime that goes on. Camera's, policing?

**RESPONSE: Probably the greatest impact the project can have on this problem is the change of character from an area dominated by parking lots to an area designed for pedestrians. New shops, restaurants, apartments, condos, hotel rooms, etc will increase the population of people in the area for 'legitimate' reasons and will increase the natural surveillance that comes with 'eyes on the street.' National experience has shown that an increase in 'legitimate' activity will displace 'illegitimate' activity.**

**COMMENT CATEGORY: Construction**

Comment: IV.A.3.1.3 (p 58): This section acknowledges the high hydraulic conductivity of the subsurface soils and fills material. Since the groundwater depths are very shallow it will be necessary to manage a fairly high volume of groundwater during construction below the groundwater level. The DEIS contains few details on how groundwater will be managed. Has the City determined an approximately volume of water that will need to be managed during construction? Considering the potential volume of water and the potential contamination (SVOC's) of groundwater in certain areas additional details should be provided. The DEIS indicates that groundwater will be managed through a sump, pumping and possible treatment in accordance with the storm water pollution prevention plan (SWPPP). The SWPPP pertains to the discharge of stormwater and not groundwater. Groundwater needs to be managed, treated and if necessary, discharged in accordance with a point source SPDES permit. We would encourage you to contact Dixon Rollins, NYSDEC Regional Water Engineer, at 585-226-5468 if you have any questions on the applicability of SPDES permits

**RESPONSE: The City will continue to work with Dixon Rollins and will comply with NYSDEC requirements regarding the management of groundwater.**

**The two likely scenarios for construction of the marina basin include either lowering the groundwater table to facilitate dry excavation or utilizing wet excavation. Ultimately, the contractor will choose the method of basin excavation and how groundwater will be managed. Both methods will require management of groundwater and discharge will be consistently monitored to minimize release of suspended solids into the Genesee River.**

**Based on experience, it is anticipated that the contractor will actively manage groundwater during construction to facilitate as much dry excavation as possible in the creation of the marina basin. Mechanical pumping methods will likely be used to temporarily lower the groundwater table within the mass excavation area.**

**In order to lower the groundwater table, groundwater would likely be pumped to a set of tiered settlement basins that allow suspended solids to settle before water is eventually discharged to the Genesee River. Settlement basins will be sized and located by the contractor, but their anticipated location would be adjacent to the future marina entrance. The contractor will have several options available for discharging water pumped from the basin. Existing infiltration swales located adjacent to the former embarking/disembarking lot are available for use during construction until they are eliminated. These swales are further discussed below under comment IV.S.1.2. In addition to the swales, an existing Vortechs™ unit exists within the embarking/disembarking lot. During basin excavation, water can be discharged into this unit for treatment prior to discharge into the Genesee River.**

During dewatering and excavation, the existing river wall will be left in place and reinforced, as needed, creating a barrier between the construction operation and the Genesee River. Any required dredging on the river-side of the existing wall can be completed concurrently or after the wall is removed. Minimal dredging outside of the wall is anticipated for completion of the entrance channel and installation of stone scour protection. Dredging outside of the river wall will require the use of a turbidity curtain to contain any suspended solids and prevent sediment from discharging from the disturbed areas.

Wet excavation would not require dewatering the basin prior to excavation, but would require significant dewatering of excavated material on-site, prior to material transport. Water draining from the excavated material must be managed and monitored prior to discharge into the storm sewer system and eventually into the Genesee River. In addition, wet excavation would make placement of stone revetment more difficult than if the groundwater table was lowered prior to construction.

Once construction of the internal basin is complete, groundwater will be allowed to recharge the basin and equalize pressure on the existing river wall. Once water levels have equalized, the “plug” wall will be removed, creating the connection between the basin and the Genesee River.

Laboratory analysis of groundwater samples collected from the site has not detected concentrations of SVOCs that exceed their associated 6 NYCRR Part 703 Groundwater Standards. During dewatering operations, groundwater will be monitored through sight and smell for triggers such as oily sheen, floating foam, color, and odors. Should any triggers be detected, discharge to the Genesee River will be suspended and water will be tested.

If impacted groundwater is encountered during completion of the project, then groundwater handling and disposal procedures will be initiated, as follows: At least one (1) 21,000-gallon, tractor-trailer type “frac” tank will be mobilized to the Project Area and placed as close to the excavation and a sanitary sewer manhole as feasible. The impacted groundwater will be pumped as needed into the frac tank and staged on-site. The excavation water containerized in the frac tank will then be sampled and analyzed for all parameters specified by the Sewer Use Law of the County of Monroe (Sewer Use Law). Waste characterization sampling shall be conducted for each frac tank proposed to be discharged to the Monroe County Pure Waters (MCPW) sewer system. The Sewer Use Law outlines sampling procedures, the suite of laboratory analysis required as well as acceptable discharge limits for wastewater proposed to be discharge into the MCPW system. Laboratory analytical results as required by the Sewer Use Law will be submitted to Monroe County for review and approval prior to discharge. Based on the existing site-specific data, it is anticipated that impacted excavation waters generated as part of this project will ultimately be discharged to the Monroe County Pure Waters (MCPW) sewer system. A copy of Monroe County’s Initial Sewer Use Permit Application is included as Appendix XX.

If MCPW discharge criteria cannot be met, then transportation and off-site disposal of the impacted excavation waters may be needed. In the event that off-site transportation of impacted water is necessary, a valid 6 NYCRR Part 364 Waste Transporter Permit will be required. All excavation waters transported off-site will be disposed of at a NYS Permitted receiving facility in accordance with all applicable Rules and Regulations.

Best management practices will be utilized to minimize sediments during pumping of groundwater into the frac tank.

Sampling and analysis of the containerized groundwater shall be performed for MCPW approval and/or waste characterization. If the water is determined to be suitable for discharge to the sanitary sewer system, discharge will be completed with the approval of and as directed by MCPW.

If treatment of the containerized waters is deemed necessary by MCPW, then an appropriate groundwater treatment system will be chosen, based upon the contaminants of concern (e.g., carbon filtration, air-stripping, etc.). An initial pilot test will be performed to verify treatment system efficiency. A treated effluent sample will be collected and analyzed, and these sample results will be submitted to MCPW for

review and approval to discharge.

**If MCPW discharge criteria cannot be met with on-site treatment methods, then transportation and proper off-site disposal of the excavation waters would be needed. In the event that off-site transportation of impacted water is necessary, a valid 6 NYCRR Part 364 Waste Transporter Permit shall be required.**

Comment: IV.S.1.2 (p 361): This section indicates that groundwater entering excavations will be pumped to specific areas elsewhere within the project site developed to promote percolation to the local groundwater aquifer. This description of groundwater management does not appear to be consistent with other sections of the DEIS (also see comments above regarding Section IV.a.3.1.3).

**RESPONSE: Groundwater management relating to the basin excavation is explained above in the response to the comment regarding section IV.a.3.1.3.**

Utility construction will occur separately and precede the basin excavation. The methodology for addressing 'impacted' groundwater should it be encountered is outlined in the response to comment IV.A.3.1.3 above. The project site contains (4) existing infiltration swales designed to treat stormwater runoff from paved surfaces and parking lots. The swales are located east of the existing port terminal parking lot, along the west side of North River Street between Portside Drive and Corrigan Street, and within the former embarking / disembarking lot. The swales include a perforated pipe surrounded by a porous gravel filter media, vegetative cover and an overflow structure. The swales are 'normally dry'. During runoff events, runoff is directed to the swales where it is treated as it infiltrates through the vegetative cover and gravel material before it enters the perforated pipe and is directed to the local drainage system. Tops of the overflow structures are higher than the swale and the structures only become active during extreme runoff events. The construction of the Marina and roadway improvements will eventually eliminate all (4) infiltration swales.

Until they are eliminated as a result of the marina basin excavation, the infiltration swales are available for use during the construction of this project for managing non-impacted groundwater entering utility trench excavations. Groundwater can be pumped to the swales where it can settle before filtering through the vegetative cover and gravel. Appropriate erosion and sedimentation control measures, e.g silt fence, stone check dams, inlet protection, etc., will be employed to protect the swales as they are used for groundwater management purposes, and as construction activities occur around them. The stormwater pollution prevention plan (SWPPP) developed for the project will account for the elimination of these infiltration swales as stormwater practices.

Comment: Our first concern is the four pad public boat launch on the Genesee River. We request that because of the relative scarcity of access to Lake Ontario to boaters in Monroe County, that the current launch be maintained and utilized during the construction phase of the proposed marina.

**RESPONSE: Access to the existing boat launch will be maintained throughout construction.**

**COMMENT CATEGORY: Economic/Fiscal**

Comment: As noted in the DEIS, County storm sewers will need to be relocated. All costs associated with relocation of the public sewers to accommodate the layout of the proposed marina as well as the additional sewer flow from the development are the responsibility of the developer.

**RESPONSE: So Noted**

Comment: You also need a Marina that will pay for itself. We pay for so many things as taxpayers that keep Rochester functioning as one of the best medium-sized cities in the U.S., but there is only so much we can take. If you go forward with this plan, we as taxpayers have to pay to make it run smoothly. What you need to do is leave the Port the way it is. The City needs to think of a plan that makes sense in these tough economic times when people are struggling to survive and cannot afford to pay for things that are not essential. I am familiar with the Port of Rochester and have placed my boat at marinas for more than 50 years and this plan is not the right way to go. This is the wrong project to pursue.

**RESPONSE:** The marina is expected to generate adequate revenue to pay for operating costs, maintenance, dredging and a portion of the City's debt service for the entire project which includes utility work, roadway and trail construction, and public amenities in addition to the marina itself. The marina will not offer many of the more costly services such as maintenance and repair and fueling. The current Port facilities are underutilized and cost the City several hundred thousand dollars each year to maintain. The planned project is expected to increase the Ports visibility and image as a destination, especially by water, and result in private investment and property tax growth.

Comment: I am in favor of development in this area and agree that we should use our great resource in our favor, but not to the point that we would take away from the beautiful lake and river that surrounds us. I believe the Phase 1 development would be a great resource, as long as all the funding is there to actually pay for this project. I do not want the people of this city to be stuck with another huge bill based on grandiose plans that can never come to fruition. I am also in favor of limited private development with absolutely no public funding.

**RESPONSE:** Funding for the Phase 1 marina project is in place and includes \$5.93 million in state and federal grants, \$7.56 million previously allocated City capital funds, and \$6.57 in planned City capital funding in the Fiscal Year 2012-2013 which will be available July 1, 2013. These sources of funds equal the 50% design engineering estimate of the cost to construct the project. The City will issue general obligation bonds for a portion of the City capital funding which will require debt service payments. It is expected that City property tax revenues will increase as a result of the planned private development associated with the marina. The City does not plan to subsidize the planned private development and is looking at options for how the City will be compensated for the value of the land that would be sold to the developer.

Comment: I am concern about cost – the project is not yet fully funded, and where will the money come from and at what cost to other city initiatives. Where will the money come from to finance the project and will the cost as usually happens end up exceeding the current estimate of \$19.7M?

**RESPONSE:** The Phase 1 marina development is fully funded with existing and planned sources of funding. The plan includes \$5.93 million in already awarded state and federal grants, \$7.56 million previously allocated City capital funds, and \$6.57 in planned City capital funding in the Fiscal Year 2012-2013 which will be available July 1, 2013. The total of these sources of funds is slightly greater than the 50% engineering design estimate of the costs to construct the project. The City is actively considering several engineering design options that could result in construction cost savings. All construction projects, however, have the potential for unexpected costs.

**COMMENT CATEGORY: Geology**

Comment: Slag Material Clarification: Hydrogen sulfide exposure?

**RESPONSE:** The excavation of slag present on site for the marina basin, utilities, and street construction will sometimes cause sulfur odors. The release of hydrogen sulfide from the disturbances of slag wastes has previously been sampled and analyzed by the City's environmental consultant for this project. The laboratory analytical results indicate that the levels of hydrogen sulfide do not pose a health or safety hazard for the construction workers in the immediate vicinity of the excavations and require no special safety precautions. For precautionary purposes, air quality will be monitored during the excavation work by an environmental inspector following the environmental management plan for the project. The general public may notice nuisance odors. The odors will be intermittent and will dissipate.

**COMMENT CATEGORY: Historic Resources**

Comment: The Lighthouse derives historic significance in large part from its relationship to Lake Ontario; and although physically separated from it due to operational modifications made over time, the property nevertheless retains strong visual links to the great lake. These views and vistas to and from the property are among its most important contributing features and they should not be compromised. Of particular concern in this regard is the +/- 12 story building proposed immediately north of the property. The mass and scale of the structure will block dominant vistas from the lighthouse grounds, tower and keeper's house. Additionally, tree plantings proposed along the North River Street Extension and the NY Central Railroad corridor will substantially obscure these same vistas. Equally important, the building and tree plantings also will alter historic views from Ontario Beach Park,

<p>US Coast Guard Station Rochester and Auxiliary, the Genesee River and Lake Ontario to the Lighthouse – and thereby diminish its historic position within the port environs. These visual impacts must be avoided and/or mitigated.</p>
<p><b>RESPONSE: Refer to FEIS Section IV.</b></p>
<p>Comment: The specific location, alignment and details for the Lighthouse Trail must respect the historic character and features of the lighthouse property. The SHPO recently advised the Charlotte-Genesee Lighthouse Historical Society on the merits of preparing a cultural landscape report for the property, the results of which should be used in planning any future physical projects, including the proposed trail. Absent such a document, research regarding historic and existing conditions must be sufficient to ensure the contemporary trail is appropriate for the property. The SHPO will continue consultation regarding this component of the Port project upon receipt of a more detailed design concept.</p>
<p><b>RESPONSE: The City will incorporate this input in the upcoming design phase of the Lighthouse Trail Project. Consultation and review by agencies, organizations, and the public will be an intrinsic and important part of the design process.</b></p>
<p>Comment: The trail from Lake Avenue to the Charlotte-Genesee Lighthouse as currently presented in the Phase I public development should be reevaluated. The Landmark Society encourages the City of Rochester to involve the Charlotte-Genesee Lighthouse in further discussion to refine the proposed design, configuration, and location of the Lighthouse Trail. Staff and board members’ knowledge of the Lighthouse’s history, potential archeological resources, visitors’ use of the site, and future plans for use of the historic site, would better inform plans for the trail, leading to a more sensible and sensitively placed trail that best meets the needs of the public. Instead build a trail which more readily accesses Lighthouse St.</p>
<p><b>RESPONSE: The City will incorporate this input in the upcoming design phase of the Lighthouse Trail Project. Consultation and review by agencies, organizations, and the public will be an intrinsic and important part of the design process. Refer to FEIS Section IV. Changes to Project in Response to Public Comment</b></p>
<p>Comment: The DEIS states dredged/spoil materials will be stockpiled on municipal property for unnamed public works projects. However, at public meetings and press events these materials have been linked to the infill of the Inner Loop and therefore require consideration pursuant to Section 106/Section 14.09. Please provide site location plans, photographs, and narrative descriptions and/or graphics of proposed conditions related to this proposal.</p>
<p><b>RESPONSE: The Inner Loop project is only speculative and not a proposal at this time and is not at a point where an environmental assessment can occur.</b></p>
<p>Comment: The Landmark Society strongly encourages the City of Rochester to consider including the Hojack Swing Bridge as part of the redevelopment plans for the Port of Rochester. The Hojack Swing Bridge is a significant historic resource in the area, is visually prominent and a highly recognizable part of the cultural landscape in the community. If rehabilitated or stabilized, the bridge could serve as a prominent local landmark, attracting visitors and contributing to the unique identity of the Charlotte/Port community. Look at what was done at the Hyline in NYC.</p>
<p><b>RESPONSE: The Swing Bridge is outside the project limit and not within the control/jurisdiction of the City of Rochester.</b></p>
<p>Comment: I understand from the 2000 Phase I Report that portions of this parcel were low and wet (ie. 1829 map) and that one ft. to 20 ft. of fill (page 92) have been placed across the project area. However, specific information on the depth fill and where the low, wet areas (reed filled waterways) were located have not been provided. For example, I cannot determine where 1 ft. of fill exists versus 20 ft. While I realize the OPRHP signed off on the December 2000 Rochester Museum Phase I Report (00PR0502 &amp; 04PR1871) our standards for documenting previous ground disturbance have changed in the intervening years. Please provide:</p> <ul style="list-style-type: none"> <li>a) A large scale fold out map that includes (1) the project area limits, (2) soil boring locations and the corresponding depth of fill and (3) below ground utilities.</li> <li>b) A large scale fold out map that includes items 1, 2 and 3 as well as the marshy areas shown on the 1829 map. I realize that this overlay will not be 100% accurate because of the quality of the 1829 map.</li> </ul>
<p><b>RESPONSE: A single overlay to the 1829 map which included all information requested above was prepared and forwarded to NYS Department of Parks, Recreation and Historic Preservation. Refer to FEIS Appendix F.</b></p>
<p><b>COMMENT CATEGORY: Hydrologic/Coastal Management</b></p>

Comment: IV.C.2.2.2 (p 88): This section does not contain a discussion of how sediments would be managed if sediment tested results prevent the use of the open-lake placement area. A contingency plan should be discussed.

**RESPONSE: Sediments will be managed in accordance with NYSDEC TOGS 5.1.9. LaBella Associated collected samples at four locations near the Port of Rochester Terminal Building in 2010. Two of the locations sampled, SED-1 and SED-2 are near the proposed marina entrance. Both locations included a combination of Class A and Class B material. SED-1 showed Class B sediments to a depth of 226', while SED-2 indicated Class B sediments exist to a depth of 222' on the City of Rochester Vertical Datum. The proposed marina entrance includes a rip rap scour protection mat which would require dredging to a maximum depth of 227' for installation. We anticipate that less than 650 cubic yards of material will be removed and based on the 2010 sampling results, all the material will classify as either Class A or Class B.**

Dredging of the marina entrance may be completed using either hydraulic dredging or using land/berge based equipment to mechanically dredge. Although the sediments may qualify for open-lake placement, we will assume upland disposal for purposes of project design and budgeting because of the relatively small quantity of sediment removal.

The City has been exploring more extensive sediment removal, possibly to the federal navigation channel maintenance depth, in the marina entrance area. In the future should the ongoing US EPA Legacy Act sediment project lead to federal funding assistance for deeper sediment removal in the marina entrance area, the City will coordinate with the USEPA, NYSDEC, and Army Corps for modification of the City's dredging permit. For purposes of the marina FEIS and project design we have determined that dredging to the navigation channel depth is not necessary.

**COMMENT CATEGORY: Parkland**

Comment: Exhibits 7 & 8 (pp39-40): The sheet pile proposed at the south side of the new marina entrance may encroach upon the north boat launch. The potential impacts are evident in Exhibit 7 (page #39) and the graphic attached to the City's September 30, 2011 response to the Departments Notice of Incomplete Application, yet Exhibit 8 does not show any encroachment. This inconsistency should be addressed and the design should not significantly encroach upon the northern launch area during the construction of phase I of the marina.

**RESPONSE: The inconsistency has been addressed and Exhibit 7 has been updated to reflect the marina entrance sheet pile design. The marina entrance design will not encroach upon the boat launch. Refer to FEIS Section III.**

Comment: IV.H (pp 177-189): This section should include a figure/exhibit which clearly defines which areas of the marina will be accessible to the general public and which areas will be restricted access (tenants and boat owners).

**RESPONSE: An Exhibit 14 was created to define the general public and restricted pedestrian access areas within the marina. The floating docks and gangways will be restricted pedestrian access areas, secured by gates. Refer to FEIS Section III.**

Comment: IV.S.2 (Figure S-1, p 369): Figure S-1 appears to suggest that during Phase I of the project access for only 20 boats will be provided at the existing boat launch. In the City's September 30, 2011 letter submitted in response to the Department's January 19, 2010 Notice of Incomplete Application the City indicated that the reconfigured boat launch would accommodate nearly 100 vehicles and trailers during Phase 1. Please clarify. Additionally, while we do not have concerns with the proposed dates, the September 30, 2011 letter indicates that the reconfigured boat ramp is expected to be completed by May of 2012 whereas the DEIS suggests that this will not occur until May 2013 (Table S-1).

**RESPONSE: The purpose of Figure S-1, relating to the existing boat launch, is to show that during the physical reconfiguration of the boat launch, ongoing access will be provided for 20 vehicles and trailers. As Phase I construction is concluded, the reconfigured boat launch will again be able to accommodate about 100 vehicles and trailers. We plan to try to complete the reconfiguration of the launch parking during the off-season.**

**The September 30, 2011 letter contains an error in the completion date of the reconfigured boat launch. The reconfigured launch has a planned completion date of May 2013, not May 2012 as indicated in the letter. The actual construction schedule will be dependent on several factors.**

Comment: Provide more clarification on parkland alienation/exchange

**RESPONSE: A full summary of parkland alienation and replacement is presented in the DEIS on page 189 (Table H-2).**

**COMMENT CATEGORY: Solid Waste Management**

Comment: IV.O.3.2.2 (p 330): This section references 6 NYCRR but fails to provide the applicable part. We believe you are referring to 6 NYCRR Part 360-1.15b (8).

**RESPONSE: 6 NYCRR Part 360-1.15b (8) is the correct reference.**

**COMMENT CATEGORY: Traffic & Parking**

Comment: The routes to and from Sites A and B pass through residential neighborhoods in the Town. Some of these routes also include residential areas in the City (e.g., Beach Avenue, and Latta Road and Stutson Street west of Lake Avenue). Site A has been used in the past for parking and bus shuttle service to and from events at the Port, but only for one or two events per summer, and sometimes not even every year. It would be quite another thing to use Site A – not to mention the addition of Site B – for this purpose on a regular, perhaps weekly, basis. The DEIS and the Traffic and Parking Analysis address the issue solely in the context that is typical of most traffic engineering studies; that is, the language and data associated with Level of Service (“LOS”). Discussions of LOS do not consider the effects that the elevated traffic volumes will have on the residential areas through which the routes between the Port and Sites A and B will pass. If the proposed parking areas must be used, consideration must be given to limiting the routes that could be used to and from Sites A and B, so as to minimize travel on residential streets and to keep routes open for emergency responders.

**RESPONSE: Site A and B were considered as possible alternative parking areas relative to their location to the Port area the short travel distance 2.5 miles and travel time 5 minutes to the Port Area. The location of these sites coupled with expanded bus service during Level 1 events contributes to the advancement of these sites. It should also be noted that Site B is adjacent to the CSX Rail Road line and should this line be abandoned in the future, would make an excellent corridor into the Port Area. In past years Site A has been used for the Wednesday evening “Concerts by the Shore” It is envisioned that these remote sites would only be used during Level 1 events that exceed the Port area on site public parking capacity which is approximately 750 vehicles. The Traffic Management plan would direct traffic to these sites to use the Lake Ontario State Parkway exiting at Dewey Avenue and the entering Site A or turning onto Ling Road to Site B. The development in this area is primarily commercial, industrial and sporting facilities.**

Comment: The DEIS and the Traffic and Parking Analysis do not consider the condition of Ling Road, which connects between Greenleaf Road and Dewey Avenue. It is reasonable to expect Ling Road to be used for access to and between Sites A and B. Ling Road is a relatively narrow two-lane road, with less-than-optimal pavement condition, relatively narrow, unpaved shoulders (in some places, no shoulders), limited street lighting, and portions with no sidewalk or sidewalk on only one side of the road. Although the Regional Transit Service 1/1X Lake Avenue Route includes Ling Road, only about six buses per day travel over this road, and only on weekdays. Increased numbers of vehicles on this road, particularly buses, will contribute to the deterioration of the condition of this road.

**RESPONSE: Ling Road is only proposed to be used if Site B is selected and as indicated in the response above, the Traffic Management Plan would direct traffic to Dewey Avenue and the Lake Ontario State Parkway.**

**The City will certainly work with the Town of Greece relative to any necessary improvements if Site B is selected as the preferred location.**

Comment: The DEIS contains no discussion of the possibility of using the now-vacant Eastman Kodak Company (“Kodak”) parking lots in the vicinity of Lake Avenue, West Ridge Road, and Maplewood Drive. This unused

parking area is only about four miles from the Project site, and is on an existing mass transit route. Although there are many residences along Lake Avenue and in adjacent residential neighborhoods, the street is four lanes wide and already carries a considerable amount of vehicles on a daily basis, and even more when events occur at the Port. If the City consulted with and obtained the permission of Kodak to use the vacant parking lots for shuttle service to and from the Port, there would be an opportunity to reduce the traffic volumes that already occur on Lake Avenue when there are events at the Port.

**RESPONSE: Alternative sites for remote parking were certainly reviewed during the preparation of the EIS. These sites were in the locations on the now vacant Eastman Kodak Parking lots on Lake Ave at Eastman Ave. The travel distance was 4.0 miles and travel time 11 minutes as compared with the 2.5 miles and 5 minutes travel time. This difference in distance and travel time with shorter bus turnaround time leads to the advancement of Sites A and B as preferred sites for Level I events. Events larger than Level 1 will require detailed Traffic Management Plans and will require the use of alternate sites other than Site A and B. This has been done in the past for large events with success.**

Comment: The DEIS, Volume 1, p. 277, recommends coordination among agencies for traffic control, but doesn't specifically mention whether or not such coordination would or should include the Town's Police Department.

**RESPONSE: The City will coordinate with the Greece Police Department on Level I and II events. Refer to FEIS, Section II. Corrections/Clarifications to the DEIS**

Comment: We have questions regarding the proposed Intelligent Transportation System (ITS) Tools and Technologies. Who will be responsible for installing and monitoring these measures? Each measure should have a defined purpose and benefit, (ie.; highway traffic mitigation, parking lot management, security). Additional through and discussions between all involved parties to determine what ITS Tools and Technologies are necessary along with their location and how they will interact to minimize traffic impacts.

**RESPONSE: The NYSDOT Regional Traffic Operations Center Manager has been involved with development of the Concept of Operations and Design Report for the Port ITS plan. Device location, ownership, maintenance responsibility are all defined in the Design Report.**

Comment: At the Lake Avenue/LOSP signalized intersection we noted that in the 2020 build scenario of the project, during the Friday 6:30 pm-7:30 pm peak hour, this intersection is very near its capacity, with at least one movement in every direction experiencing a volume to capacity (v/c) ration of .92 or greater. This yields to a failing level-of-service for at least one movement and excessive queue lengths on almost every approach to the intersection. These queues will extend to adjacent intersections and possibly onto the Colonel Patrick O'Rorke Memorial Bridge. Due to existing physical and right-of-way constraints at Lake Avenue/LOSP intersection and the Colonel Patrick O'Rorke Memorial Bridge traffic mitigation to offset these traffic impacts is not feasible, and monitoring/optimizing the operation of the intersection may not appreciably alleviate traffic impacts.

**RESPONSE: While the intersection is projected to be operating at near capacity, it should be noted that operations appear to be acceptable based on the Synchro analysis results. These results were confirmed using the SimTraffic model. All Approach LOS's are D and the overall intersection is D. Relative to queues backing to the Colonel O'Rorke bridge, this westbound approach shows the 95% level queues backing just slightly more than half way between Lake Avenue and the bridge during the peak hour thus leaving sufficient excess storage capacity.**

Comment: I do not believe parking and traffic issues that will arise have or ever can be adequately addressed. My father was a former city Councilman and he has complained for years about the danger that exists due to the narrowing of Lake Avenue. This development only makes any emergency situation even more dangerous as egress roads will be busier and more crowded. And what would happen if one of these roads was blocked due to an emergency situation??

**RESPONSE: A complete Traffic Analysis was conducted for future conditions and showed acceptable operating conditions. Refer to the DEIS, Section IV. K Transportation. An additional access road is being constructed on River Street and North River Street which will facilitate entry of emergency vehicles.**

Comment: Provide more clarification on potential shuttle service: What is security plan? Who will provide

security/maintenance? How will it be financed? What happens to the parking lots if not used?
<b>RESPONSE: The process of evaluating possible off-site parking locations is not yet complete. Specific lots have not been identified and selected so security and maintenance arrangements have not been established or discussed in detail. Security and maintenance plans will be required for any location being used by the City for shuttle parking service, however. Various options for financing shuttle operations will be considered and could include fees, a City subsidy, and cost share arrangements with event organizers.</b>
Comment: If the plan is put into motion you will eliminate much of the parking that is necessary for people who want to enjoy Charlotte Beach throughout the year. We need every available parking spot because as it is we do not have enough.
<b>RESPONSE: All parking spaces (532) currently designated to serve Ontario Beach Park/Charlotte Beach) will be retained. Overall public parking capacity at the Port site will be reduced resulting in some parking deficits. These deficits will only occur at specific times on Saturdays and special events. Mitigation measures are under consideration and include offsite parking with bus service to the Port area, use of Intelligent Transportation System (ITS) devices and parking management. Refer to the DEIS Section IV K Transportation, 2. Parking Management, Page 261.</b>
Comment: People who live in high-rise will end up parking on the street when it is more convenient for them rather than entering their parking garage/facility, especially during the day. How will we stop that?
<b>RESPONSE: On-street parking is available to the public.</b>
<b><u>COMMENT CATEGORY: Utilities</u></b>
Comment: The issue that the DEIS does not specifically address is additional demand on the sewer facilities in the area. There is a general statement on page 308 that says “the potential growth...may impact...utilities... However, it is important to emphasize that any growth-inducing impacts will occur over a relatively long timeframe.” I would actually like to see flow rate estimates (average and peak gallons per minute) for the anticipated maximum demand on the sanitary sewer system in the area based on the maximum 430 dwellings and 44,000 square feet of commercial space.  The sewers to be constructed/relocated as part of the proposed project will need to be sized to accommodate future growth in the area. It is anticipated that the new 24” diameter pipe proposed for the project will suffice. We’d like to evaluate whether or not the proposed development will adversely the County’s Charlotte Sanitary Pump Station at the south end of the project.
<b>RESPONSE: Future sanitary domestic loadings have been estimated considering development of the former Terminal Building and Development Parcels I (N &amp; S), II, and III. Future loadings consider a maximum of 430 dwelling units and 44,000 square feet of commercial space on development parcels I through III. The average daily domestic loading is estimated to be 199,595 gallons per day (139 gpm) and the peak hourly domestic loading is estimated to be 798,380 gallons per day (554 gpm). The sanitary sewer capacity analysis also considers observed baseflow in the existing sewer system and its impact on the future hydraulic performance of the sewer. The final design of sanitary improvements will be coordinated with Monroe County Pure Waters.</b>
Comment: 2 parking lots 1-N, 1-S, East side of Lake Ave. Area is now grass. Runoff needs to be managed. Please explain
<b>RESPONSE: The relocated storm sewer along North River Street has been sized to accommodate runoff from these parcels. A runoff impervious coefficient consistent with commercial land has been considered in sizing the storm sewer. Internal drainage and stormwater management within the parcels will be the responsibility of the developer.</b>
<b><u>COMMENT CATEGORY: Vegetation &amp; Wildlife</u></b>
Comment: II.C.2.1 (p24): in the bulleted list it is unclear what is meant by measures to improve wildlife habitat.”

**RESPONSE:** The statement refers to the creation of new habitat and protecting water quality within the habitat. Creation of a 4.7 acre basin adds to the area of available fish habitat. Installing a stone revetment (approximately 47,000 square feet) creates a sanctuary for adolescent fish species and other aquatic animals below the water line and provides habitat for terrestrial animals above the water line. Other marina elements, such as dock anchors and the marina floor itself provide additional benthic habitat. The proposed water circulation pipe, connecting the marina basin to the Genesee River, will provide water circulation and protection from water quality degradation.

Comment: IV.D.2.2 (pp 105 & 107): The Department recommends that you seek further input and guidance from Dawn Dittman, U.S. Fish & Wildlife Service, regarding the use of the Genesee River by sturgeon. While it appears that you have reviewed some of the U.S. Fish & Wildlife sturgeon data, the Department questions some of you're the conclusions. In particular, we have concerns over the statement that indicates that sturgeon won't return to the Genesee River until the spring of 2020.

**RESPONSE:** The lake sturgeon, a State-listed threatened species, historically inhabited the waters of Lake Ontario, but this species has been thought to be absent since the 1930s due to poor water quality, habitat fragmentation and degradation, and intensive commercial exploitation. In June of 2003, the NYSDEC, USFWS, and the U.S. Geological Survey (USGS) began a lake sturgeon reintroduction program which includes juvenile stocking and public education. The goal of the program is to restore a self-sustaining lake sturgeon population to the area. The stocking program involved the reintroduction of about 900 and 1,000 juvenile sturgeons into the Genesee River in 2003 and 2004, respectively.

Subsequent gill net surveys indicate that half of the sturgeons are using the Genesee River while the others have moved into Lake Ontario. Lake sturgeon return to rivers to spawn, where they prefer waters between 1 and 6 meters in depth with cobble or boulder substrate. Research has supported the hypothesis that the Genesee River contains habitat suitable for growth and survival of stocked juvenile sturgeon. Based on the typical lake sturgeon life cycle, sturgeon stocked in 2003 are not expected to return to the Genesee River to spawn until the spring of 2020, when the first females are expected to reach sexual maturity.

**COMMENT CATEGORY: Visual/Aesthetics**

Comment: I would like to reiterate my concern that the view of the beach not be blocked; even from Lake Avenue and think even six-story condos may impair the view.

**RESPONSE:** It would be difficult to construct anything along Lake Avenue within the port area without obstructing or impacting views, therefore the plan attempts to balance the loss of some views with the community benefits brought about by new development. A key benefit is the creation of a great place for people to enjoy. The plan seeks to improve the site context. The new civic square, mandated by the Marina District form-based code which will connect Lake Ave to the new River Street is likely to become a very special public space. The square may be terraced into two or more levels due to the difference in elevation between the two streets. Each level would afford newly created views of the marina and the existing terminal building and river beyond. The square will be fronted by new buildings with ground floor retail spaces, which will help enliven it and make it a destination for people. New views will also be created from River Street and the promenade around the marina, locations which are currently amid a sea of asphalt. The lighthouse may become more prominent as it will be seen from the north rising above the marina. The view from Lake Ave looking toward the lake will be framed by new buildings on the east side of Lake. Undoubtedly, these buildings will obscure distant views of the lake and river, but these views are currently dominated by parking lots in the foreground. A new trail will be constructed on elevated land just south of the railroad track which will provide new views out over the marina toward the lake and river. Building heights will be controlled to maximize views from the trail. In short, while some views are lost or impacted, the plan preserves significant views, creates new views.

Comment: Construction of buildings right up to the side walk on the east side of Lake Avenue and the building heights will really impact the beautiful view of the river and lake as you drive over the bridge and approach the park and really does not preserve the historical integrity of the port or park area.

**RESPONSE:** See response above

Comment: Please don't ever block the view of the waterfront, whether it be from the historic light house or the view from Lake Ave, looking north as we drive in to park.

<b>RESPONSE</b> See Response above
<b>COMMENT CATEGORY:</b> Water Resources
Comment: IV.B.3.7.1.3 (p 84): This section indicates that Phase 1 will be treated through existing storm systems. What type of “treatment” will occur and does the existing system comply with the current MS4 (GP 0-10-002) requirements?
<b>RESPONSE:</b> Redevelopment of Parcel I will be constructed by a contractor working for a private developer and when developed, will likely discharge stormwater runoff into the storm system along the relocated North River Street which will connect to the system on Corrigan Street where water is passed through an existing water quality unit, treated and released into the river. The water quality unit was constructed in 2004 as a component of the City’s “Port Access Roads” project. It is a Vortechs™ System unit as manufactured by Vortechics, Inc., and is designed to be capable of removing 80 percent of the net annual TSS load based on a 50-micron particle size. We understand the device to be an acceptable proprietary practice under the DEC’s New York State Stormwater Management Design Manual.
Comment: The proposed marina will be a stillwater basin. How will bilge water, fuel spills, and other contaminants generated at the marina be processed and disposed of without further polluting the Genesee River and Lake Ontario?
<b>RESPONSE:</b> The marina will be operated and certified as a New York Clean Marina, which requires utilization of nationally established Best Management Practices. Since the proposed marina is a stillwater basin, attention will be made to the possibility of contamination from such sources as bilge pump out water and fuel spills. The New York Marina Environmental Best Management Practices recommends making boaters aware of bilge water dangers and encouraging proper practices. Measures to increase awareness could include signs and informational flyers. For bilge water, the New York Marina Pollution Prevention website suggests that bilge water is inspected for oil sheen prior to pumping. If sheen is present, an oil absorbent pad or pillow must be used to remove the contaminant from the bilge water. If such actions cannot remove the oily bilge water, the bilge water is be required to be removed manually in its entirety and properly disposed of. Fuel spill prevention will include making boaters aware of the marina’s refueling rules and regulations. Although the marina will not offer refueling, a fuel spill plan will be included in the final operations and maintenance manual that will include containment procedures and information regarding equipment. This plan will be applicable to oily bilge water, fuel and other possible basin contaminants.

**STATE ENVIRONMENTAL QUALITY REVIEW  
(SEQR)**

**FINAL  
SITE SPECIFIC/GENERIC ENVIRONMENTAL IMPACT  
STATEMENT  
FEIS**

**Proposed Action:  
City of Rochester Port Public Marina and Mixed Use Development Project**

**SEQR Environmental Impact Statement  
NEPA Environmental Assessment**

**LEAD AGENCY:  
Thomas S. Richards  
Mayor, City of Rochester  
City Hall, Room 307A, 30 Church Street  
Rochester, NY 14614-1290**

**Appendix A**

**Rochester Environmental Commission  
Public Comment Summary and  
Disposition Recommendations  
Report**

## LIST OF COMMENTERS

### Written Comments:

Louise Altobelli  
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Rochester, NY 14612

Douglas A. Fisher, Esq  
111 East Avenue, #937  
Rochester, NY 14604

Donald Fuffell, Chairman  
Monroe County Fisheries Advisory Board  
City Place, 50 West Main Street, Ste. 8100

Sarah Gallivan  
35 Corrigan Street  
Rochester, NY 14612

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Nancy Herter, Scientist, Archaeology  
NYS Historic Preservation Office  
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Catherine James  
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Rochester, NY 14617

Peter LePore, Charlotte Resident

Anthony Mittiga  
211 Edgerton  
Rochester, NY 14607

Patrick O'Neil  
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Rochester, NY 14612

John Osowski, PE, Director of Facilities  
SUNY/ College at Brockport  
Planning/Construction  
Office of Facilities and Planning  
Rochester, NY 14614-1225

Sean Schiano  
35 Corrigan Street  
Rochester, NY 14612

Richard Szwabczynski Swacen aka EarthGuy, NY  
228 River Street  
Rochester, NY 14612

Gary Tajkowski  
Town of Greece  
Dept. of Development Services  
1 Vince Tofany Blvd.  
Greece, NY 14612-5016

Kevin Quinn  
Monroe County Department of Environmental Services  
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Terrance J. Rice, P.E., Director of Transportation Monroe  
County Department of Transportation  
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Rochester, NY 14614-1231

Frank J. Sanza, Vice Chairman/MCFAB  
MC Fishery Advisory Board

Richard Vance  
[ucby@localnet.com](mailto:ucby@localnet.com)

### **Verbal Comments**

Fred Amato  
Genesee Charlotte Lighthouse  
70 Lighthouse Street  
Rochester, NY 14612

Douglas A. Fisher, Esq  
111 East Avenue, #937  
Rochester, NY 14604

Bill Grey  
59 Hincer Street  
Rochester, NY 14612

Virginia Kobylarz  
193 River Street  
Rochester, NY

Michael May  
Genesee Charlotte Lighthouse  
70 Lighthouse Street  
Rochester, NY 14612

Mike Parker  
3165 Lake Avenue  
Rochester, NY 14612

Andy Rau  
560 Birr St  
Rochester, NY

Kathy Strauss  
Charlotte Community Association & Team Charlotte

Richard Szwabczynski Swacen aka EarthGuy, NY  
228 River Street  
Rochester, NY 14612

### **Visual Survey Comments**

19 participants

## Comment Disposition Terminology

1. No Response Required - not a substantive issue
  - a) Comment expresses opinion and/or does not raise a substantive issue; acknowledge, but No Response Required - not a substantive issue.
  - b) Comment addresses an issue that is outside the purview of the DEIS.
  
2. Correction Required

The comment points out an omission or inaccuracy in the DEIS that needs to be corrected.
  
3. Explanation/Clarification Required

The comment raises an issue which was addressed in the environmental impact statement. The issue needs a simple explanation and reference to the section in the DEIS where it is discussed.
  
4. Additional Analysis Required

The comment raises an issue which has not been thoroughly addressed. Further analysis is believed necessary to offer a proper response.
  
5. Alternative Suggested

The comment suggests an alternative which merits evaluation.

**COMMENT SUMMARY/ DISPOSITION RECOMMENDATIONS**

<b><u>COMMENT</u></b>	<b><u>COMMENTER</u></b>	<b><u>DISPOSITION RECOMMENDATION</u></b>
<b><u>COMMENT CATEGORY: Proposed Action</u></b>		
We strongly support and encourage a phased approach to the proposed development	K. Strauss	Explanation/Clarification Required
Boat owners will be insisting on good security for their boats, so appropriate decorative fencing should be planned now rather than later.	D. Fisher, M. Parker	Explanation/Clarification Required
Is the City committing to having a replacement launch facility, or multiple if deemed necessary, with equivalent launching and parking capacity, fully operating prior to the closing of the existing ramp an commencement of Phase II?	T. Haley	Explanation/Clarification Required
While we do not have concerns with the proposed dates, the September 30, 2011 letter indicates that the reconfigured boat ramp is expected to be completed by May of 2012 whereas the DEIS suggests that this will not occur until May 2013 (Table S-1).	T. Haley	Explanation/Clarification Required
Marina Operations and Maintenance Plan: In the City's September 30, 2011 letter you indicate that a Marina Operations and Maintenance Plan would be included in the DEIS. We were unable to locate the referenced plan.	T. Haley	Correction Required
Thank you very much for sending the draft document to us for our review and comments. The only discrepancy of note that should be corrected in the final version is the proposed name of Brockport's planned facility at the Port. The name has changed several times over the years and the correct version is "Lake Ontario Research Center." We noted that the facility is incorrectly called the "Lake Ontario Resource Center."	J. Osowski	Correction Required
I feel that the current plan is now of the appropriate mix of type and scale to help leverage rather than detract from or overshadow our existing local amenities of nature and man. For these and other good reasons, I feel it is critical that some version of the Lake Ontario Resource Center be strongly encouraged and assisted.	R. Swacen	No response Required
Right now the Charlotte beach area is free to all residents and there is abundant parking for all people. There is a great view of the beach and it is easy to get to. I am concerned that this new project will not enable people the full access that they are used to. I understand parking will be limited and there will be a parking fee. This will be a hardship for people with limited funds.	F. Glaser	Explanation/Clarification Required
The other concern is for the present businesses, and future retail and residential development in relationship to parking and patron convenience.	F. Ardino	Explanation/Clarification Required
We are seeing less people interested in boating and less interest in larger sized boats. The size of the proposed marina should be looked at.	F. Ardino	Explanation/Clarification Required
If we sell public land then the developers will have a field day and build as many units as they can simply to use up any and all available space.	F. Glaser	Explanation/Clarification Required

In this economy who is going to buy/rent high priced units? I am afraid the demand is not there and these units will sit idle and partially constructed creating an eyesore for the city. I think this project needs to be viewed very closely and take in the concerns of the people and not focus on money.	F. Glaser	Explanation/Clarification Required
Converting the parking lot at Charlotte to a marina and condos will deprive thousands of area residents of a delightful place to spend a day or an hour at the harbor and beach.	R. Vance	No response Required
The Monroe County Fishery Advisory Board supports the concept of a public marina at Charlotte. We feel it will be good for the community and a boon for both sports fisherman and boaters. We hope the marina will attract many visitors and enhance business opportunities in the Charlotte area.	F. Sanza & D. Fuffell, B. Grey	No response Required
We are concerned with the relocation, size (3 of pads) and parking facilities for the new boat launch. We request that the new (boat) launch have four pads.	F. Sanza & D. Fuffell	Explanation/Clarification Required
We are also concerned with slip availability. We request that some percentage,(of boat slips) , ten percent perhaps, be made available for charter boat operators in Monroe County. If the current 85 slip marina is realized this would reserve 8 slips for charter boats. We further request that these slips be adjacent to each other. This is a common situation at ocean-side marinas. It is an attraction for people to visit the marinas when the charters return with their catches.	F. Sanza & D. Fuffell	Explanation/Clarification Required
Concerned that w/in 2 yrs. Developers will want to develop parking lots to serve private commercial & residential development	B. Grey	Explanation/Clarification Required
The plan must detail how an effective security program for the area will be provided. This program should also specify how noise, especially music, motorcycle and muscle car noise is to be controlled. In addition, the plan should contain specifics for handling unlawful tenant and overnight without accommodations persons.	P. O'Neill	Explanation/Clarification Required
The waterfront, in my opinion, belongs to the general public.	L. Altobelli	No response Required
The Monroe County storage facility has already been discussed to be built near the Charlotte baseball fields by Corrigan and Estes. This would be a travesty for the local residents as well as the youngsters who play soccer and baseball on these fields all summer long. This is also a huge parking area for overflow that will only increase with even the Phase 1 development. This facility is not well kept and a complete eyesore. It should be kept on a main access road and not in the backyards of Charlotte residents.	S. Schiano, K. Strauss, S. Gallivan, B.Grey,	Explanation/Clarification Required
Given the historic lack of development in Charlotte, I am skeptical of these plans coming to fruition. I don't know where the occupants of the housing units will come from, and why they would locate in Charlotte. I don't want to see it become necessary to subsidize the units, through tax waivers to the developers, or other means. If there hasn't been a survey specific to the proposed housing, then one needs to be done. While studies may show a market for hundreds of lake front units, I'd want to see one for this project alone. Have the two yacht clubs, or area boaters, especially those now docking in the River, been surveyed about marina use, and the desire to live in the project.	A. Mittiga	Explanation/Clarification Required
Lighthouse Trail project- Why would the City spend \$ 150,000 to build a trail when the site is already accessible. Rather for smaller port related activities. The Lighthouse should be accessed by the historic route of lighthouse street.	M. May, K. Strauss	Additional Analysis Required
Decorative lighting needs to be a part of the plan	B. Grey	No response Required
Private residential development should occur along Lake Avenue	M. May	Explanation/Clarification Required
<b><u>COMMENT CATEGORY: Alternatives</u></b>		
Concentrate new residential in a tall tower at the former miniature golf course. Height is preferable to a sprawling footprint, giving the additional advantage of spectacular vistas from the apartment units. I would look to having a 'residential	D. Fisher, P. O'Neil	Explanation/Clarification Required

hotel” there, with function rooms and exhibit rooms that would accommodate lighthouse functions and a site visitor’s center.		
People do not visit the port area to visit parking lots. That function can best be accomplished in a long parking garage along Lake Avenue, as I testified at the public session. Instead of acres of asphalt, that area is best given over to expansion of that very popular park. So much macadam & so many parking spaces – Shouldn’t parking be concentrated in a structure? That would free up more land for the park. The current Parks maintenance facility site is a good spot for parking structure	D. Fisher	Explanation/Clarification Required
The plan should include more parking, not less than is currently available. This may involve a low parking garage built into the embankment at the current Monroe County park service location.	P. O’Neil	Explanation/Clarification Required
An iconic boardwalk along the waterfront should be considered; preferably in the area north of the port terminal building,	D. Fisher	Explanation/Clarification Required
I don’t feel that the project encourages or promotes a ferry operation, which would draw people to the port, not the development of private residences. This area needs port activity, possibly light shipping. In other words, it should function as a PORT.	L. Altobelli, D. Fisher	Explanation/Clarification Required
There is great opportunity here for a small scale hotel.	D.Fisher	No response Required
<b>COMMENT CATEGORY: Community Character</b>		
I am concerned about how existing businesses & buildings would fit in with the proposed project. There are a number of existing businesses and buildings in the Charlotte neighborhood that are in desperate need of makeovers. I fear that if there are not resources to help fix up their businesses - it will take away from all the work being invested in the marina and will negatively affect economic stimulus in the area.	S. Gallivan	No response Required
A select number of businesses should be allowed to develop but not to the point of taking away from the family businesses that have been in this area for years, who are also members of this community, including Mr. Dominic's, Abbott's, and Windjammers. There are also businesses in the Port of Rochester that have struggled to survive with the failure of the ferry and these businesses should also be given consideration. One exception would be Pier 45, this is an example of a business that I fear. the city has been running this establishment at an enormous loss each year, at tax payer costs. your plan wants to add businesses to this area where many are struggling as it is. Small, family businesses that have been in this community for years and decades should be given consideration prior to letting private developers do with our local community as they wish, and unfortunately the EIS as it stands is vague enough for developers to do just this.	S. Schiano	Explanation/Clarification Required
12 story apartment like buildings will diminish community and Port character.	K. Strauss, D. Fisher, M. Parker, S. Gallivan	No response Required
The Port site is a major public waterfront attraction as is and little has to be done to attract users. Facilities developed here should be water oriented, residential can happen on surrounding lands.	D.Fisher	No response Required
The proposed code is not acceptable to the neighborhood, it does not correlate with the Design guidelines that the community developed and submitted to the City during the other recent port project. Villages have been working to protect & enhance village character Height restriction is important to protect Village character Any building must fit into look of Village. Respect history of shipping in the Village	V. Kobylarz	Explanation/Clarification Required
The history of Charlotte is based on Port and recreational activity	V. Kobylarz	No response Required

How can noise pollution from beach concerts, nighttime partying at the marina, and additional restaurants and recreation venues be restricted or contained for the benefit of the anticipated residents in the mixed-use development?	D. de Zafra Atwell	Explanation/Clarification Required
It is desirable that Charlotte retains and enhances its "Seaside Village" structure and appearance. This would include improvement and additions to the various shops, restaurants and buildings currently in existence on Lake Avenue. Residents and visitors alike would benefit from the addition of a hardware/general store, sporting goods, bakery, marine goods, bike rental etc.	P. O'Neil	No response Required
I am very concerned with the height that your special codes would allow for. I firmly believe these should be taken out of your EIS, because leaving them in only leaves the opportunity for private developers to take away the beauty of the water that is Charlotte and what attracts people to our small community. Charlotte is not a big city, it is a small close knit local community. There does not presently exist a building higher than three stories, nor should one ever be allowed to be built.	S. Schiano	Explanation/Clarification Required
The community needs to maintain some architectural overview on private development	B. Grey	Explanation/Clarification Required
Noise level from the Bands. Each year they get louder and louder. The sound systems are very sophisticated. Nola's outdoor bandstand, and the battle of the bands along the river at each bar. Certain types of music are worse than others. . Jazz, oldies, soft rock, folk, county or swing seem better . . But the loud rock and rap only seem to amplify the noise across the river. Please remember this is still a residential area, we pay taxes, and there are sound ordinances that should be considered and respected. An overly hyped up crowd isn't always good!	C. James	Explanation/Clarification Required
There is a perception by many people that Charlotte is a party place, drug infested and after dark it only gets worse. Please work to change the dynamics in Charlotte and address crime that goes on. Camera's, policing?	C. James	Explanation/Clarification Required
<b>COMMENT CATEGORY: Construction</b>		
IV.A.3.1.3 (p 58): This section acknowledges the high hydraulic conductivity of the subsurface soils and fills material. Since the groundwater depths are very shallow it will be necessary to manage a fairly high volume of groundwater during construction below the groundwater level. The DEIS contains few details on how groundwater will be managed. Has the City determined an approximately volume of water that will need to be managed during construction? Considering the potential volume of water and the potential contamination (SVOC's) of groundwater in certain areas additional details should be provided. The DEIS indicates that groundwater will be managed through a sump, pumping and possible treatment in accordance with the storm water pollution prevention plan (SWPPP). The SWPPP pertains to the discharge of storm water and not groundwater. Groundwater needs to be managed, treated and if necessary, discharged in accordance with a point source SPDES permit.	T. Haley	Additional Analysis Required
IV.S.1.2 (p 361): This section indicates that groundwater entering excavations will be pumped to specific areas elsewhere within the project site developed to promote percolation to the local groundwater aquifer. This description of groundwater management does not appear to be consistent with other sections of the DEIS (also see comments above regarding Section IV.a.3.1.3).	T. Haley	Correction Required
Our first concern is the four pad public boat launch on the Genesee River. We request that because of the relative scarcity of access to Lake Ontario to boaters in Monroe County, that the current launch be maintained and utilized during the construction phase of the proposed marina.	F. Sanza & D. Fuffell	Explanation/Clarification Required
<b>COMMENT CATEGORY: Economic/Fiscal</b>		
As noted in the DEIS, County storm sewers will need to be relocated. All costs	K. Quinn	Explanation/Clarification

associated with relocation of the public sewers to accommodate the layout of the proposed marina as well as the additional sewer flow from the development are the responsibility of the developer.		Required
You also need a Marina that will pay for itself. We pay for so many things as taxpayers that keep Rochester functioning as one of the best medium-sized cities in the U.S., but there is only so much we can take. If you go forward with this plan, we as taxpayers have to pay to make it run smoothly. What you need to do is leave the Port the way it is. The City needs to think of a plan that makes sense in these tough economic times when people are struggling to survive and cannot afford to pay for things that are not essential. I am familiar with the Port of Rochester and have placed my boat at marinas for more than 50 years and this plan is not the right way to go. This is the wrong project to pursue.	P. LePore	Explanation/Clarification Required
I am in favor of development in this area and agree that we should use our great resource in our favor, but not to the point that we would take away from the beautiful lake and river that surrounds us. I believe the Phase 1 development would be a great resource, as long as all the funding is there to actually pay for this project. I do not want the people of this city to be stuck with another huge bill based on grandiose plans that can never come to fruition. I am also in favor of limited private development with absolutely no public funding.	S. Schiano	Explanation/Clarification Required
I am concern about cost – the project is not yet fully funded, and where will the money come from and at what cost to other city initiatives. Where will the money come from to finance the project and will the cost as usually happens end up exceeding the current estimate of \$19.7M?	A. Rau	Explanation/Clarification Required
<b><u>COMMENT CATEGORY: Geology</u></b>		
Slag Material Clarification: Hydrogen sulfide exposure?	K. Strauss, M. Parker	Explanation/Clarification Required
<b><u>COMMENT CATEGORY: Historic Resources</u></b>		
The Lighthouse derives historic significance in large part from its relationship to Lake Ontario; and although physically separated from it due to operational modifications made over time, the property nevertheless retains strong visual links to the great lake. These views and vistas to and from the property are among its most important contributing features and they should not be compromised. Of particular concern in this regard is the +/- 12 story building proposed immediately north of the property. The mass and scale of the structure will block dominant vistas from the lighthouse grounds, tower and keeper's house. Additionally, tree plantings proposed along the North River Street Extension and the NY Central Railroad corridor will substantially obscure these same vistas. Equally important, the building and tree plantings also will alter historic views from Ontario Beach Park, US Coast Guard Station Rochester and Auxiliary, the Genesee River and Lake Ontario to the Lighthouse – and thereby diminish its historic position within the port environs. These visual impacts must be avoided and/or mitigated.	C. Capella-Peters, W. Goodman, M. May, K. Strauss, F. Amato, D. Fisher	Additional Analysis Required
The specific location, alignment and details for the Lighthouse Trail must respect the historic character and features of the lighthouse property. The SHPO recently advised the Charlotte-Genesee Lighthouse Historical Society on the merits of preparing a cultural landscape report for the property, the results of which should be used in planning any future physical projects, including the proposed trail. Absent such a document, research regarding historic and existing conditions must be sufficient to ensure the contemporary trail is appropriate for the property. The SHPO will continue consultation regarding this component of the Port project upon receipt of a more detailed design concept.	C. Capella-Peters	Additional Analysis Required

<p>The trail from Lake Avenue to the Charlotte-Genesee Lighthouse as currently presented in the Phase I public development should be reevaluated. The Landmark Society encourages the City of Rochester to involve the Charlotte-Genesee Lighthouse in further discussion to refine the proposed design, configuration, and location of the Lighthouse Trail. Staff and board members' knowledge of the Lighthouse's history, potential archeological resources, visitors' use of the site, and future plans for use of the historic site, would better inform plans for the trail, leading to a more sensible and sensitively placed trail that best meets the needs of the public. Instead build a trail which more readily accesses Lighthouse St.</p>	W. Goodman	Additional Analysis Required
<p>The DEIS states dredged/spoil materials will be stockpiled on municipal property for unnamed public works projects. However, at public meetings and press events these materials have been linked to the infill of the Inner Loop and therefore require consideration pursuant to Section 106/Section 14.09. Please provide site location plans, photographs, and narrative descriptions and/or graphics of proposed conditions related to this proposal.</p>	C. Capella-Peters	Explanation/Clarification
<p>The Landmark Society strongly encourages the City of Rochester to consider including the Hojack Swing Bridge as part of the redevelopment plans for the Port of Rochester. The Hojack Swing Bridge is a significant historic resource in the area, is visually prominent and a highly recognizable part of the cultural landscape in the community. If rehabilitated or stabilized, the bridge could serve as a prominent local landmark, attracting visitors and contributing to the unique identity of the Charlotte/Port community. Look at what was done at the Hyline in NYC.</p>	W. Goodman, M. May, D. Fisher	Explanation/Clarification Required
<p>I understand from the 2000 Phase I Report that portions of this parcel were low and wet (ie. 1829 map) and that one ft. to 20 ft. of fill (page 92) have been placed across the project area. However, specific information on the depth fill and where the low, wet areas (reed filled waterways) were located have not been provided. For example, I cannot determine where 1 ft. of fill exists versus 20 ft. While I realize the OPRHP signed off on the December 2000 Rochester Museum Phase I Report (00PR0502 &amp; 04PR1871) our standards for documenting previous ground disturbance have changed in the intervening years. Please provide:</p> <ul style="list-style-type: none"> <li>a) A large scale fold out map that includes (1) the project area limits, (2) soil boring locations and the corresponding depth of fill and (3) below ground utilities.</li> <li>b) A large scale fold out map that includes items 1, 2 and 3 as well as the marshy areas shown on the 1829 map. I realize that this overlay will not be 100% accurate because of the quality of the 1829 map.</li> </ul>	N. Herter, M. Parker	Additional Analysis Required
<b><u>COMMENT CATEGORY: Hydrologic/Coastal Management</u></b>		
<p>IV.C.2.2.2 (p 88): This section does not contain a discussion of how sediments would be managed if sediment tested results prevent the use of the open-lake placement area. A contingency plan should be discussed.</p>	T. Haley	Explanation/Clarification Required
<b><u>COMMENT CATEGORY: Parklands</u></b>		
<p>Exhibits 7 &amp; 8 (pp39-40): The sheet pile proposed at the south side of the new marina entrance may encroach upon the north boat launch. The potential impacts are evident in Exhibit 7 (page #39) and the graphic attached to the City's September 30, 2011 response to the Departments Notice of Incomplete Application, yet Exhibit 8 does not show any encroachment. This inconsistency should be addressed and the design should not significantly encroach upon the northern launch area during the construction of phase 1 of the marina.</p>	T. Haley	Correction Required
<p>IV.H (pp 177-189): This section should include a figure/exhibit which clearly defines which areas of the marina will be accessible to the general public and</p>	T. Haley	Correction Required

which areas will be restricted access (tenants and boat owners).		
IV.S.2 (Figure S-1, p 369): Figure S-1 appears to suggest that during Phase 1 of the project access for only 20 boats will be provided at the existing boat launch. In the City's September 30, 2011 letter submitted in response to the Department's January 19, 2010 Notice of Incomplete Application the City indicated that the reconfigured boat launch would accommodate nearly 100 vehicles and trailers during Phase 1.	T. Haley	Explanation/Clarification Required
Provide more clarification on parkland alienation/exchange	K. Strauss	Explanation/Clarification Required
<b>COMMENT CATEGORY: Solid Waste Management</b>		
IV.O.3.2.2 (p 330): This section references 6 NYCRR but fails to provide the applicable part. We believe you are referring to 6 NYCRR Part 360-1.15b (8).	T. Haley	Correction Required
<b>COMMENT CATEGORY: Traffic &amp; Parking</b>		
If the plan is put into motion you will eliminate much of the parking that is necessary for people who want to enjoy Charlotte Beach throughout the year. We need every available parking spot because as it is we do not have enough.	P. LePore	Explanation/Clarification Required
The routes to and from Sites A and B pass through residential neighborhoods in the Town. Some of these routes also include residential areas in the City (e.g., Beach Avenue, and Latta Road and Stutson Street west of Lake Avenue). Site A has been used in the past for parking and bus shuttle service to and from events at the Port, but only for one or two events per summer, and sometimes not even every year. It would be quite another thing to use Site A – not to mention the addition of Site B – for this purpose on a regular, perhaps weekly, basis. The DEIS and the Traffic and Parking Analysis address the issue solely in the context that is typical of most traffic engineering studies; that is, the language and data associated with Level of Service (“LOS”). Discussions of LOS do not consider the effects that the elevated traffic volumes will have on the residential areas through which the routes between the Port and Sites A and B will pass. If the proposed parking areas must be used, consideration must be given to limiting the routes that could be used to and from Sites A and B, so as to minimize travel on residential streets and to keep routes open for emergency responders.	G. Tajkowski	Explanation/Clarification Required
The DEIS and the Traffic and Parking Analysis do not consider the condition of Ling Road, which connects between Greenleaf Road and Dewey Avenue. It is reasonable to expect Ling Road to be used for access to and between Sites A and B. Ling Road is a relatively narrow two-lane road, with less-than-optimal pavement condition, relatively narrow, unpaved shoulders (in some places, no shoulders), limited street lighting, and portions with no sidewalk or sidewalk on only one side of the road. Although the Regional Transit Service 1/1X Lake Avenue Route includes Ling Road, only about six buses per day travel over this road, and only on weekdays. Increased numbers of vehicles on this road, particularly buses, will contribute to the deterioration of the condition of this road.	G. Tajkowski	Explanation/Clarification Required
The DEIS contains no discussion of the possibility of using the now-vacant Eastman Kodak Company (“Kodak”) parking lots in the vicinity of Lake Avenue, West Ridge Road, and Maplewood Drive. This unused parking area is only about four miles from the Project site, and is on an existing mass transit route. Although there are many residences along Lake Avenue and in adjacent residential neighborhoods, the street is four lanes wide and already carries a considerable amount of vehicles on a daily basis, and even more when events occur at the Port. If the City consulted with and obtained the permission of Kodak to use the vacant	G. Tajkowski	Explanation/Clarification Required

parking lots for shuttle service to and from the Port, there would be an opportunity to reduce the traffic volumes that already occur on Lake Avenue when there are events at the Port.		
The DEIS, Volume 1, p. 277, recommends coordination among agencies for traffic control, but doesn't specifically mention whether or not such coordination would or should include the Town's Police Department.	G. Tajkowski	Explanation/Clarification Required
We have questions regarding the proposed Intelligent Transportation System (ITS) Tools and Technologies. Who will be responsible for installing and monitoring these measures? Each measure should have a defined purpose and benefit, (ie,; highway traffic mitigation, parking lot management, security). Additional through and discussions between all involved parties to determine what ITS Tools and Technologies are necessary along with their location and how they will interact to minimize traffic impacts.	D. Goehring	Explanation/Clarification Required
At the Lake Avenue/LOSP signalized intersection we noted that in the 2020 build scenario of the project, during the Friday 6:30 pm-7:30 pm peak hour, this intersection is very near its capacity, with at least one movement in every direction experiencing a volume to capacity (v/c) ration of .92 or greater. This yields to a failing level-of-service for at least one movement and excessive queue lengths on almost every approach to the intersection. These queues will extend to adjacent intersections and possibly onto the Colonel Patrick O'Rorke Memorial Bridge. Due to existing physical and right-of-way constraints at Lake Avenue/LOSP intersection and the Colonel Patrick O'Rorke Memorial Bridge traffic mitigation to offset these traffic impacts is not feasible, and monitoring/optimizing the operation of the intersection may not appreciably alleviate traffic impacts.	D. Goehring	Explanation/Clarification Required
I do not believe parking and traffic issues that will arise have or ever can be adequately addressed. My father was a former city Councilman and he has complained for years about the danger that exists due to the narrowing of Lake Avenue. This development only makes any emergency situation even more dangerous as egress roads will be busier and more crowded. And what would happen if one of these roads was blocked due to an emergency situation??	S. Schiano	Explanation/Clarification Required
People who live in high-rise will end up parking on the street when it is more convenient for them rather than entering their parking garage/facility, especially during the day. How will we stop that?	B. Grey	Explanation/Clarification Required
Provide more clarification on potential shuttle service: What is security plan? Who will provide security/maintenance? How will it be financed? What happens to the parking lots if not used?	M. Parker, K. Strauss	Explanation/Clarification Required
I do not support imposing fees for parking	B. Grey	No response Required
We have reviewed the Draft Generic/Specific Environmental Impact Statement for the Port of Rochester Public Marina and Mixed Use Development Project and we do not have any comments.	T. Rice	No response Required
<b>COMMENT CATEGORY: Utilities</b>		
The issue that the DEIS does not specifically address is additional demand on the sewer facilities in the area. There is a general statement on page 308 that says "the potential growth...may impact...utilities... However, it is important to emphasize that any growth-inducing impacts will occur over a relatively long timeframe." I would actually like to see flow rate estimates (average and peak gallons per minute) for the anticipated maximum demand on the sanitary sewer system in the	K. Quinn	Correction Required

<p>area based on the maximum 430 dwellings and 44,000 square feet of commercial space.</p> <p>The sewers to be constructed/relocated as part of the proposed project will need to be sized to accommodate future growth in the area. It is anticipated that the new 24" diameter pipe proposed for the project will suffice. We'd like to evaluate whether or not the proposed development will adversely the County's Charlotte Sanitary Pump Station at the south end of the project.</p>		
<p>2 parking lots 1-N, 1-S, East side of Lake Ave. Area is now grass. Runoff needs to be managed. Please explain</p>	M. Parker	Explanation/Clarification Required
<b><u>COMMENT CATEGORY: Vegetation &amp; Wildlife</u></b>		
<p>II.C.2.1 (p24): in the bulleted list it is unclear what is meant by measures to improve wildlife habitat."</p>	T. Haley	Additional Analysis Required
<p>IV.D.2.2 (pp 105 &amp; 107): The Department recommends that you seek further input and guidance from Dawn Dittman, U.S. Fish &amp; Wildlife Service, regarding the use of the Genesee River by sturgeon. While it appears that you have reviewed some of the U.S. Fish &amp; Wildlife sturgeon data, the Department questions some of you're the conclusions. In particular, we have concerns over the statement that indicates that sturgeon won't return to the Genesee River until the spring of 2020.</p>	T. Haley	Correction Required
<b><u>COMMENT CATEGORY: Visual/Aesthetics</u></b>		
<p>I would like to reiterate my concern that the view of the beach not be blocked; even from Lake Avenue and think even six-story condos may impair the view.</p>	S. Dauenhauer Ciriello	Explanation/Clarification Required
<p>Construction of buildings right up to the side walk on the east side of Lake Avenue and the building heights will really impact the beautiful view of the river and lake as you drive over the bridge and approach the park and really does not preserve the historical integrity of the port or park area.</p>	F. Ardino	Explanation/Clarification Required
<p>Please don't ever block the view of the waterfront, whether it be from the historic light house or the view from Lake Ave, looking north as we drive in to park.</p>	L. Altobelli	Explanation/Clarification Required
<b><u>COMMENT CATEGORY: Water Resources</u></b>		
<p>IV.B.3.7.1.3 (p 84): This section indicates that Phase 1 will be treated through existing storm systems. What type of "treatment" will occur and does the existing system comply with the current MS4 (GP 0-10-002) requirements?</p>	T. Haley	Explanation/Clarification Required
<p>The proposed marina will be a stillwater basin. How will bilge water, fuel spills, and other contaminants generated at the marina be processed and disposed of without further polluting the Genesee River and Lake Ontario?</p>	D. de Zafra Atwell	Explanation/Clarification Required
<b><u>COMMENT CATEGORY: Other/Miscellaneous</u></b>		
<p>Among additional potential economic benefits the Rochester Port Public Marina &amp; Mixed Use Development plan will help improve and complete our River Way Trail system with its intimate walk and bike-way connections to land, water, forests, flora, fish and fauna that best immerses us in nature and allows us to appreciate and conserve the wondrous gifts that Mother Earth has given the Charlotte harbor towns area.</p>	R. Swacen	No response Required
<p>Put the restaurant in the terminal building in private hands to stop the losses, put more vendors in the building.</p>	R. Vance	No response Required
<p>Promote the banquet hall in the beach house, weddings, etc., caterers will deliver the food.</p>	R. Vance	No response Required

Promoting Charlotte as a year round destination: To the points of interest mentioned-the Riverway Trail, O'Rourke Bridge, carousel, lighthouse, and (deteriorating) railroad swing bridge – add and integrate in your planning the River Street cemetery, Charlotte walking tour of historic residential and commercial sites, and the to-be-opened Charlotte Transportation Museum.	D. deZafra Atwell	No response Required
Utilize the Port of Rochester's central location between Braddock Bay on the west and Nine-Mile Point on the east to promote outdoor naturalist education for the public, as well as research by professionals at the proposed Lake Ontario Resource Center. For example, the nearby ponds and lakeshore estuary are prime birding habitat on the migratory north/south flyway. Why not offer docent-led excursions?	D. deZafra Atwell	No response Required
Encourage and facilitate venues for indoor entertainment. Could the Charlotte High auditorium – with its unique and restored murals – be made available to continue the summer beach concert series into the other three seasons? Could community theater productions be offered there? Could an art house movie theater, perhaps with a café and meeting space, be attracted to Charlotte so that northside residents with cultural interests need not always trek to the Little Theatre or the Dryden on East Avenue?	D. deZafra Atwell	No response Required
Promoting Village Based Commerce: Definitely encourage small, Charlotte-based businesses. Boat-building and repair, bicycle sales and rentals, and Rochester-area tour operations are examples. Perhaps working studio and gallery space for craft people and artists could also be built, which would be an attraction for Canadian tourists as well as local folk if/when a revived and more realistic trans-lake ferry operation becomes a reality.	D. deZafra Atwell	No response Required
Speeding Cars, Motorcycles and especially the motorcycles referred to as the "crotch rockets" : Unfortunately this is a problem all over Monroe County, but in the summer is is more severe in Charlotte, Seabreeze, Summerville and along the Lake Ave, St. Paul corridor heading into the lake area. It is dangerous; they are loud, and completely obnoxious. Please slow the traffic down! Can you get control of this issue? Camera's, ticketing, policing?	C. James	No response Required
Develop supportive businesses for ships. Ship building should be brought back	V. Kobylarz	No response Required
Please increase policing on the waters for BWI. Over the years the boat accidents into the piers seem to have increased. Also, loud boats screaming, echoing all over there lake at all hours of the night. . is there anything you can do to address this?	C. James	No response Required

**STATE ENVIRONMENTAL QUALITY REVIEW  
(SEQR)**

**FINAL  
SITE SPECIFIC/GENERIC ENVIRONMENTAL IMPACT  
STATEMENT  
FEIS**

**Proposed Action:  
City of Rochester Port Public Marina and Mixed Use Development Project**

**SEQR Environmental Impact Statement  
NEPA Environmental Assessment**

**LEAD AGENCY:  
Thomas S. Richards  
Mayor, City of Rochester  
City Hall, Room 307A, 30 Church Street  
Rochester, NY 14614-1290**

**Appendix B**

**Visual Preference Survey Results  
Part 1 and Part 2**

# **Visual Preference Survey – Part 1**



1E rating	1E what do you like	1E how to improve
1: Poor		
3: Good		reduce parking lot
1: Poor	Lake / river view	
2: Fair		
2: Fair		more landscaping
2: Fair	It's a wonderful view, glorious vistas, lots of openness, although I wouldn't want to live in that apartment building	Build an amusement park and make it the "Coney Island of Western NY"
1: Poor	not filled with run down property or non marine type (car dealerships or shopping mall)	Try to preserve visual link to Lake and river
2: Fair		
1: Poor		
4: Great	I like the views of the river and the lake beyond the eastern breakwall.	
3: Good		
4: Great		
1: Poor		
2: Fair	Openness	Coordinate overall look, more landscaping around parking lots
1: Poor		
2: Fair	Nice because of the history in the area and it is important to preserve the history around the area.	
3: Good	Water on the horizon	a light house close to the water, more trees, lawn, and picnic areas
4: Great		
2: Fair	View of lake and river	Less surface parking, more active use
1: Poor		



2E Rating	2E What do you like	2E How would you improve
3: Good		
2: Fair	landscaping, green lawn	open up view to river
2: Fair		
2: Fair		
2: Fair		
4: Great	It shows a largely undisturbed site which could possibly contain a treasure trove of historical and archaeological items!	Leave it alone.
1: Poor	High point for views	get rid of swing bridge. Make it more people friendly
4: Great		
1: Poor		
4: Great		
1: Poor		
4: Great		
3: Good		
3: Good	Landscaping, greenery	
1: Poor		
2: Fair		
3: Good	natural	a picnic table
4: Great		
3: Good	Park like setting, swing bridge	Trim vegetation to increase views toward lake
2: Fair		

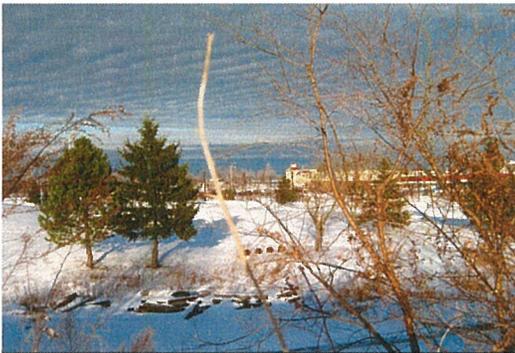


1P Rating	1P What do you like	1P How would you improve
1: Poor		
2: Fair	view of marina	reduce # and levels of multi-story buildings - looks very urban
3: Good		
4: Great		
3: Good		too many highrise buildings block the view
1: Poor	It clearly shows the dividing line between the "haves" [\$\$\$\$] and the "have-nots!" It's also very misleading because it depicts the view from above the rooftops of the proposed buildings ~ it would probably be more appropriate to show what the view would look like on the third or fourth floor of the apartment building ~ a view looking into the bedroom windows on the upscale condos!	Reduce the scale of the site plan using the guidelines proposed by the community in 2005 - 2006 and previous ~ lower buildings, smaller footprints, more open space and better vistas like you'd expect in a "quaint village setting."
4: Great	Good mix of residential and marine related activity ie basin promenade and trees	eliminate railroad tracks
3: Good		
4: Great		
1: Poor		The large (unnecessary) building in this proposed plan would ruin views from businesses on Lake Ave and cast shadow over the street. They are ostentatious and are being sold to the city as positive development but as a life long Charlotte resident... this is not Miami or even Chicago... center city is 11 miles from the proposed marina development. People may summer there but I cannot see them staying through harsh Charlotte winters. Digging out an unnatural marina will also be permanent and though a study has been done the long term impact is not known.
4: Great		

1: Poor	I think is a waste of the city money there is other things that need improving like the broken down houses but lets keep putting money into this port maybe we should get a fast ferry again	
3: Good	It's denser which will create more life around the port.	It looks like a barricade. There should be more connection between the buildings and different access points to the port.
2: Fair	Marina for visitors	Get rid of high rise and imbalanced look caused by concentration of structures on E side of street
4: Great		
3: Good		The project itself is overdue for the area. Along with the current dock space to be added (either for rental or ownership I dont know) but my suggestion is that the project should include significant more public dock space for people visiting the area. Several people i know would like to come to the river and get something to eat but are limited due to dock space. I think more people would visit the port and provide more revenue to local buisness. More boaters would rather travel by water in the summer to visit for lunch or dinner or shopping. Dock Space to the Port of Rochester definatly needs to be improved.
2: Fair	the bright blue water, the grass peir	smaller buildins
1: Poor		This project will deny our community its vista of the lake and river!
4: Great	Views to lake and river preserved, attractive buildings and new marina added	Light rail line along railroad would create great opportunity for busy transit stop at lower right of picture, would address decreased parking supply and increased parking demand
4: Great		Road Median? Bike lanes?



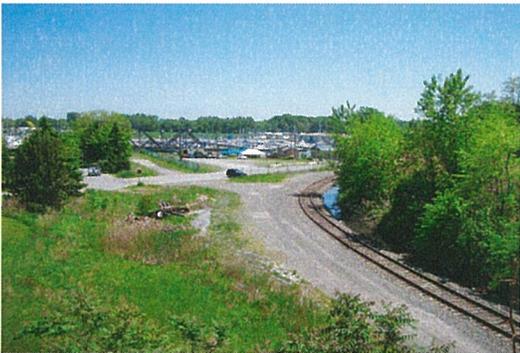
2P Rating	2P What do you like	2P How would you improve
2: Fair		
2: Fair		remove tower that blocks view
3: Good		
1: Poor		
3: Good		
1: Poor	It's too misleading to find anything good. No cars represented either traveling or parked. The once undisturbed rich archaeological site has been destroyed. A 12-story building blocks the C-G Light	Don't touch the lighthouse site. Chop off the top 8-10 stories of the high-rise. Lower the heights of the buildings on the left side of the frame.
3: Good	open spaces, tree lined road	re-consider the 12 floor high rise. get rid of RR tracks
3: Good	I think I see a walk way, near by & making the river Publicly Available.	If the footprint is small enough, then the hieght is OK. ALSO: The school nearby is built on quick sand .Engineering holes need to be explored , carefully, relative to height.
4: Great		
1: Poor		This builing is too large and will block people's view of the river. Consider something smaller and less ostentatious.
3: Good		
1: Poor		
2: Fair	Improved pedestrian and bike paths	The height of the building is too high.
1: Poor		Get rid of or reduce highrises; they do not fit style of surrounding neighborhood
3: Good		
3: Good		
1: Poor	Trees and trail	Eliminate the high rises
1: Poor		
4: Great	Sight distance greatly increased, new buildings add feel of vitality to area	Keep some plantings to minimize the clear-cut appearance of the greenspace
3: Good		



3E Rating	3E Like	3E Improve
4: Great		
3: Good		
2: Fair		
2: Fair		
3: Good		
4: Great	I love winter scenes.	Get the City to work with the community to remove the invasive plant species and all the trash, garbage and graffiti.
1: Poor	open	make easy access to waterfront
4: Great	Concept is great.	Add a Tall flag pole that exceeds the height of nearby trees:...visible from the Park.
2: Fair		
4: Great		
2: Fair		
4: Great		
3: Good		
2: Fair	Calling attention to the view	Integrate into overall look of/plan for area
2: Fair		
2: Fair		
1: Poor	the railroad looks like a nice trail or a frozen stream bed in the winter	leaves on the trees, clear skies, hiding the ferry terminal building (is that the fast ferry to the far right)
4: Great		
3: Good	Parklike, natural setting	More order to the vegetation, possibly a trail connection through to decrease sense of isolation
3: Good		



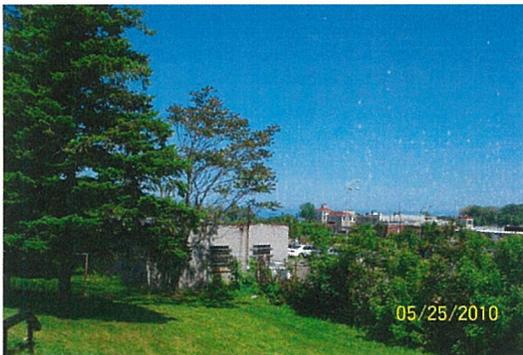
3P Rating	3P Like	3P Improve
1: Poor		
1: Poor		open up the view. too many trees and tall buildings block total view
3: Good		
3: Good		
3: Good		
1: Poor	I see more "underlook" than "overlook." Where's the scenic winter view? Where are the rows of cars traveling and parked, some parked on the grass because they can't find a convenient parking space!	Improve the view from the old "putting" golf course by removing invasive plant species, and policing the area to prevent homeless encampments under the bridge and graffiti on the bridge walls.
3: Good	looks finished	not sure of building heights
3: Good		
4: Great		
1: Poor		How will it look in the winter? After mid October Charlotte is a ghost town. There will not be enough interest in year round residence to make large buildings like this a viable option for development.
3: Good		
1: Poor		
3: Good		
2: Fair		Highrise is out of place and blocks view of lake
3: Good		
4: Great		
2: Fair	Trees and grass	get rid of the giant hotel and road, find a view point of the lake or river for the scenic overlook
1: Poor		
4: Great	Kept natural setting but added development in the distance, added trail	More plantings
3: Good		



4E Rating	4E Like	4E Improve
3: Good		
1: Poor		fill in railroad area with green space or walk way/bike trail
1: Poor		
1: Poor		
1: Poor		
2: Fair	It's a fairly clean canvas which could be transformed into a picturesque garden space.	Get the City to work with the community to remove the invasive plant species and all the trash, garbage and graffiti. Turn the green space into garden space, and native species habitat.
1: Poor	railroad has no place in waterfront	eliminate RR tracks and use for roads or walking paths
1: Poor	This is a useless track.	Emminant domain it.
2: Fair		
3: Good		
1: Poor		
4: Great		
1: Poor		
1: Poor		Obvious lack of maintenance
1: Poor		
1: Poor	it has potential, but that's about it	a trolley would be nice, general clean up. hide the marnia
3: Good		
1: Poor	View to river, marina	remove overgrown vegetation, clean up railroad, add multi-use path (rail-with-trail) or light rail station here for future service to downtown, UR
1: Poor		



4P Rating	4P Like	4P Improve
1: Poor		
2: Fair	trees and grass are nice	tall building looks very sterile and urban. this is a port community and that isnt being reflected in the building size, structure
3: Good		
2: Fair		
3: Good		
1: Poor	It's too misleading to find anything good about it.	Chop off the top 8-10 stories of the high-rise. Lower the heights of the buildings on the left side of the frame to about tree level. Turn the green space into garden space, and native species habitat.
3: Good	looks finished and not crude	eliminate RR tracks
3: Good		
4: Great		
1: Poor		This building is TOO TALL. Think about smaller builds that will be easily maintained. The city is being sold a bill of false goods by developers who do not realize that these buildings will be over priced and stand vacant entirely or for most of the year because Charlotte is not an ideal location from mid October-mid April.
3: Good		
1: Poor		
2: Fair		The building is too high.
1: Poor		Highrise is incongruous; sterile look of big space around tracks seems unrealistic
3: Good		
3: Good		
1: Poor	grass is cleaned up, new trees	eliminate the ugly building, plantings on the far bank, a trolley would be nice
1: Poor		
4: Great	Cleaned up overgrown vegetation and railroad, added vibrant buildings	Space out street trees a bit to increase chance of view to river, add light rail station for future service to downtown, UR
1: Poor		



5E Rating	5E Like	5E Improve
3: Good		
2: Fair		remove small building and open up landscaping
1: Poor		
2: Fair		
2: Fair		
2: Fair	Most of the "visual pollution" caused by the County maintenance building and equipment is hidden behind the overgrowth	Move the maintenance facility somewhere else.
1: Poor	no comment	no thought in what is there
2: Fair		
2: Fair		
3: Good		
1: Poor		
4: Great		
1: Poor		
3: Good		Get rid of ugly structure
1: Poor		
1: Poor		
1: Poor	The pine tree is nice	a fence around the maintance building
4: Great		
2: Fair	View toward lake, terminal building	Remove decrepit industrial building
3: Good		



5P Rating	5P Like	5P Improve
1: Poor		
1: Poor		extremely intrusive. too close to road. concrete walls - urban feel - doesnt reflect lakeside feel. very depressing.
2: Fair		
4: Great		
2: Fair		I don't like the buildings so close to the street
1: Poor	OMG! It looks like a prison facility.	This doesn't say "quaint village" setting to me!
4: Great	cosmopolitan, fresh	street level shops?
3: Good		
2: Fair		
1: Poor		This picture is ridiculouse and speaks for itself. Who would give an approving rating to a huge wall that obstucts views and casts large shadows on the MAJORITY of successful summer businesses in Charlotte!?!
2: Fair		
1: Poor		
2: Fair		
2: Fair		Stark contrast to buildings on other side of street. Does not have the feel of a lakeside village. Where is parking??
2: Fair		
3: Good		
1: Poor	nothing, it's a wall	seriously are we supposed to look into peoples windows
1: Poor		
2: Fair	Building up to sidewalk with tree lawn and street trees	Building looks stark, unapproachable, at street level
3: Good		



6E Rating	6E Like	6E Improve
4: Great		
2: Fair	open feel.	clean up buildings on west side of street
2: Fair		
1: Poor		
3: Good	Love the view of the lake	
2: Fair	Openness and vistas	Balance the east side of Lake Ave with buildings of the same height and set backs
2: Fair	good road condition	needs center divider, diagonal parking, New England charm
1: Poor		
1: Poor		
3: Good		
2: Fair		
4: Great		
1: Poor		
2: Fair		Would be more attractive if properties on west had an incentive to improve image
1: Poor		
2: Fair		
3: Good	I like the tree lined street, and the decorative street light, and street sign, quiet feel to the scene	crosswalks, eliminate the traffic lights, flowers in the flower pots
4: Great		
2: Fair	Nice lighting, landscaping	Add buildings on east side to increase sense of enclosure, enhance existing buildings on the west
2: Fair		Road Median? Bike lanes?



6P Rating	6P Like	6P Improve
1: Poor		
1: Poor		significantly reduce the height of the buildings on the east side. move buildings back from street as they are too close and will be oppressive.
4: Great		
4: Great		
1: Poor		buildins block the view, too congested
1: Poor	OMG! It looks like "David vs. Goliath! It reminds me of the approaches to Atlantic City!	Apply principals of Feng Shui, to create harmony, balance, peacefulness.
3: Good	Buildings help define it	needs diagonal parking,maybe a median, Niagara on the Lake plantings
2: Fair		
4: Great		
1: Poor		The shadows that would be cast on the successful businesses (who do most of their business during sunny summer months) negates any positives that may come with this development.
3: Good		
1: Poor		
3: Good		The denser development is good, but the scale from one side of the street to the other is too extreme.
1: Poor		Balance E W sides a bit more, get rid of ugly high rise. Indicate places for parking for residents and non-residents.
4: Great		
3: Good		
1: Poor	I don't like it, there is a lack of balance of the street, to the west there is an urban village feel, to the east is an urban comercial/bussiness feel, it's unwelcoming, cold	a view of the lake/river would be nice, small outdoor cafes
1: Poor		
3: Good	New, dense buildings make it feel more like a place to be	Buildings on east side dominate those on west, what happened to street lights and traffic signals?
3: Good		Road Median? Bike lanes?



7E Rating	7E Like	7E Improve
3: Good		
1: Poor		
2: Fair		
1: Poor		
3: Good		
2: Fair	i love the openness and the sky.	Seek out development in keeping with the existing buildings. Apply principals of Feng Shui, to create harmony, balance, peacefulness.
1: Poor	nothing	diagonal parking, more landscaping
2: Fair		
1: Poor		
3: Good		
2: Fair		
4: Great		
1: Poor		
2: Fair		Get rid of ugly looking structures on both sides of street
1: Poor		
2: Fair		
3: Good	decretive street sign and street lights	on street parking, and crosswalks, bike path
4: Great		
2: Fair	Street and sidewalks look nice but there doesn't seem to be much else going on	Mixed use buildings, more people
1: Poor		



7P Rating	7P Like	7P Improve
1: Poor		
1: Poor		move buildings back from streetside. reduce building height and also use building materials such as wood or brick. Reduce scale to smaller level.
4: Great		
4: Great		
2: Fair		
1: Poor	OMG! It looks like "David vs. Goliath! It reminds me of the approaches to Cancun resort.	Seek out development in keeping with the existing buildings. Apply principals of Feng Shui, to create harmony, balance, peacefulness.
4: Great	clean, no signal light	improve west side of road (to the right in the picture)
3: Good		
4: Great		
1: Poor		See #6.
3: Good		
1: Poor		
3: Good		The denser development is good, but the scale from one side of the street to the other is too extreme.
1: Poor		Balance E-W sides. Strive for an architectural form that is neither "Caribbean/Spanish" nor cutesy colonial
4: Great		
2: Fair		
1: Poor	unbalanced, unwelcoming	lake/river would be nice, small outdoor cafes, a streetcar named downtown
1: Poor		
4: Great	New buildings add sense of vitality, not too tall as to overpower rest of block	New or renovated buildings on west side of street are needed for balance
3: Good		Road Median? Bike lanes?



8E Rating	8E Like	8E Improve
3: Good		
1: Poor	ability to see the water	remove small building
1: Poor		
1: Poor		
3: Good		
2: Fair	Openness and vistas	Seek out development in keeping with the existing buildings. Apply principals of Feng Shui, to create harmony, balance, peacefulness.
1: Poor	no potholes	needs buildings or shops lining street
3: Good		
2: Fair		
3: Good		
2: Fair		
4: Great		
2: Fair		
3: Good		Get rid of storage clutter on south side of drive
1: Poor		
2: Fair		
2: Fair	decretive crosswalks and street lighting	the terminal building and parking lot is hideous to look at.
4: Great		
2: Fair	Streets and sidewalks are nice, view to port facilities	Add buildings, remove surface parking
1: Poor		



8P Rating	8P Like	8P Improve
1: Poor		
1: Poor		reduce size of buildings. add more porches, signs of life and scale for the neighborhood
4: Great		
4: Great		
2: Fair		Blocks the view of the water, don't like all the buildings
1: Poor	Nothing.	It looks like a side street in some dingy warehouse district.
4: Great	nice wide streets	make sure there are sidewalks on both sides
3: Good		
4: Great		
1: Poor		This is too much. The desire to live in Charlotte is not great enough to warrant this kind of huge development! As a life long resident of Charlotte, the place is a ghost town from mid October- mid April or May. This is not a viable development plan and it is a shame to see the city of Rochester being sold a bill of goods that will be further folly on our part.
3: Good		
1: Poor		
1: Poor		The access points looks overbearing. There should be stronger connections to the water with pedestrian access.
2: Fair		Better architecture. How is parking for visitors to the area going to be incorporated??
3: Good		
2: Fair		Restaurants, Bars and Ice cream Shops should also be put in the plans to attract more families
1: Poor	new trees to the south are nice	a park like setting on the hill overlooking the lake/river would be lovely, crosswalks/sidewalks
1: Poor		
4: Great	Looks like an urban village	sidewalks needed on south side
4: Great		



9E Rating	9E Like	9E Improve
3: Good		
1: Poor		
1: Poor		
2: Fair		
3: Good		
2: Fair	Openness.	Seek out development in keeping with the existing buildings. Apply principals of Feng Shui, to create harmony, balance, peacefulness.
1: Poor	its functional	needs improvements - no stone
3: Good		
2: Fair		
4: Great		
3: Good		
4: Great		
3: Good		
3: Good		
1: Poor		
2: Fair		
3: Good	I like the fancy sidewalk (although it's in bad shape and poorly done, rocks are nice	a park like setting would be nice
4: Great		
2: Fair	View to terminal building	Remove unkempt surface parking, out of place boulders
1: Poor		



9P Rating	9P Like	9P Improve
1: Poor		
1: Poor		increase space between buldings
4: Great		
4: Great		
3: Good		
1: Poor	Nothing. Very unappealing.	It's very claustrophobic. Seek out development in keeping with the existing buildings. Apply principals of Feng Shui, to create harmony, balance, peacefulness.
3: Good	much improved over existing	perhaps some cafes in the middle with open air seating
4: Great		
3: Good		
1: Poor		There will only be people using this space during summer months which make up less than a quarter of the year. Residents of Charlotte need smaller more modest spaces that will be sustainable and useful throughout the year, not just in summer months!
4: Great		
1: Poor		
3: Good	The addition of better defined gathering space is nice.	
3: Good		
4: Great		
3: Good		
1: Poor	I like outdoor seating/dinning	remove the buildings so people can sit outdoors and enjoy the water at the same time, this could be anywhere downtown!
1: Poor		
4: Great	Great public space with taller buildings on both sides, this is a place I would like to be	Not a thing
4: Great		



10E Rating	10E Like	10E Improve
3: Good		
2: Fair		
2: Fair		
3: Good		
3: Good		
3: Good	I love winter scenes.	Maybe needs a better job of sidewalk snow removal.
2: Fair	trees	needs to be developed
3: Good		
3: Good		
4: Great		
2: Fair		
4: Great		
3: Good		
3: Good		
2: Fair		
2: Fair		
4: Great	the parking lot looks like a park in the winter:)	Ice Skating!
4: Great		
4: Great	Park setting, view to lake	Screen the surface parking
3: Good		



10P Rating	10P Like	10P Improve
1: Poor		
1: Poor		move buildings back from road
3: Good		
3: Good		
2: Fair		too much of an urban look, not enough nature
1: Poor	It's too misleading. How are you going to have green trees in winter?	what happened to the Lake vista? Seek out development in keeping with the existing buildings. Apply principals of Feng Shui, to create harmony, balance, peacefulness.
4: Great	good use of buildings	maybe street side diagonal parking
2: Fair		
3: Good		
1: Poor		It is too big! You will never be able to fill this with residents or businesses! Think of smaller, more unique spaces that use aritecture that is more suitable to modesty of Charlotte and less like Miami Beach! Seriously! Stop trying to make a money off of rich retirees, they are not wintering here and this HUGE building will be useless for more than half the year. Small spaces that evoke nautical themes like the lighthouse and old warehouse spaces could be used. Too bad they were destroyed for the fast ferry debacle.
3: Good		
1: Poor		
1: Poor		Acts as too much of a barricade to the water views.
1: Poor	Nothing!	Get rid of highrise -- and how are you going to address attendant parking problems?!? If visitors can't park, they won't come -- esp. families in summer!
3: Good		
3: Good		
1: Poor	at least theres a sidewalk	it's a highrise, nothing special
1: Poor		
4: Great	Completely different, but also a great view	Nothing
3: Good		



11E Rating	11E Like	11E Improve
3: Good		
2: Fair		
2: Fair		
2: Fair		
3: Good		
2: Fair	Openness. Calm, peacefulness.	Seek out development for the east side of Lake Ave in keeping with the existing buildings. Apply principals of Feng Shui, to create harmony, balance, peacefulness.
2: Fair	street lights , on street parking	diagonal parking, flower beds
2: Fair		
2: Fair		
4: Great		
2: Fair		
4: Great		
2: Fair		
2: Fair		
1: Poor		
3: Good		
3: Good	decent looking street	highlight the brick median, or narrow the street, bike lanes would be nice,
4: Great		
2: Fair	Street is in good condition, quality streetscape	Fix up buildings on west side, add new buildings on east side
2: Fair		



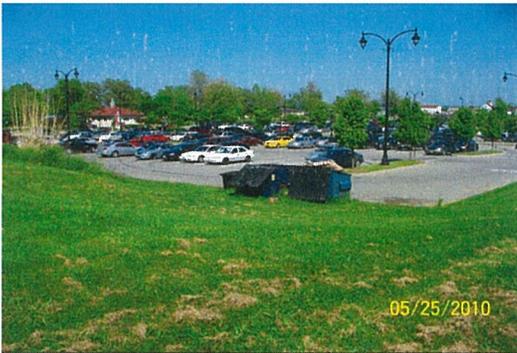
11P Rating	11P Like	11P Improve
1: Poor		
1: Poor		move buildings back from street
4: Great		
4: Great		
2: Fair		
2: Fair	What, no traffic or parking? It looks like the University Ave business district near Culver.	The buildings on the east side of Lake are still a bit too high if one expects a "quaint village setting."
3: Good	nice boulevard like road	maybe street side parking one side to west
3: Good		
3: Good		
1: Poor		See #6.
3: Good		
2: Fair		
3: Good		
2: Fair		Balance E and West sides somewhat
3: Good		
3: Good		
1: Poor	it's not very interesting to look at	lacks balance, more welcoming street
1: Poor		
3: Good	New buildings on east side add definition, vibrancy (though somewhat out of scale with west side of street)	Rehab or replace buildings on west side
3: Good		Road Median? Bike lanes?



12E Rating	12E Like	12E Improve
3: Good		
1: Poor		
2: Fair		
3: Good		
3: Good		
2: Fair	Openness but it reminds one of a drag race track.	Seek out development for the east side of Lake Ave in keeping with the existing buildings. Apply principals of Feng Shui, to create harmony, balance, peacefulness.
2: Fair	sidewalks	its plain
3: Good		
2: Fair		
4: Great		
2: Fair		
4: Great		
2: Fair		
2: Fair		Landscape parking better
2: Fair		
2: Fair		
2: Fair	I like the crosswalk, could be made a little bit better though	it's a parking lot, not a lot of curb apeal on a parking lot
4: Great		
2: Fair	Not bad for a parking lot	Screen the parking, add buildings
2: Fair		



12P Rating	12P Like	12P Improve
1: Poor		
2: Fair		
4: Great		
4: Great		
3: Good		
1: Poor	Misleading. Where is the traffic and rows of parked cars?	Seek out development for the east side of Lake Ave in keeping with the existing buildings. Apply principals of Feng Shui, to create harmony, balance, peacefulness.
3: Good	landscaping	bicycle parking, diagonal on street parking
4: Great		
3: Good		
1: Poor		
3: Good		
1: Poor		
2: Fair		
3: Good		But not if adjacent bldg. is a highrise!
3: Good		
3: Good		
3: Good	the trees are nice	sidewalks and crosswalks would be disireable.
1: Poor		
4: Great	Dense building and more trees to screen existing parking	Small building at northeast corner would continue streetscape without taking away lots of parking
3: Good		Road Median? Bike lanes?



13E Rating	13E Like	13E Improve
3: Good		
1: Poor		Seek out development for the east side of Lake Ave in keeping with the existing buildings. Apply principals of Feng Shui, to create harmony, balance, peacefulness.
1: Poor	some grass, little else	steep hill?
2: Fair		
1: Poor		
1: Poor		Seriously... you chose this picture?
1: Poor		
4: Great		
3: Good		
2: Fair		
1: Poor		
2: Fair		
1: Poor	I love dumpsters! just kidding, other than the dumpster it's a nice park/parking lot setting, looks like a nice place to go on a summer day	extend the park area so more people can picnic down by the beach
3: Good		
1: Poor	View to park, carousel	random dumpsters need to go, screen the parking
1: Poor		



13P Rating	13P Like	13P Improve
1: Poor		
4: Great	nice green space	
3: Good		
4: Great		
3: Good		
1: Poor	Misleading. Where are all the cars? Too much green space which will turn into improvised parking lots.	Seek out development for the east side of Lake Ave in keeping with the existing buildings. Apply principals of Feng Shui, to create harmony, balance, peacefulness.
4: Great	open grassy area	maybe a sidewalk
3: Good		
2: Fair		
3: Good	Of course this looks better than the previous option!	Use landscaping and smaller builds! Its that simple!
3: Good		
1: Poor		
3: Good		
2: Fair	looks the same except for the dumpster is gone	
2: Fair		
2: Fair	parking is better screened	looks empty, screen the parking better from Lake Ave to make it feel more like a park than a parking lot
2: Fair		Public Art?



14E Rating	14E Like	14E Improve
4: Great		
4: Great	beautiful open space	
2: Fair		
2: Fair		
3: Good		
3: Good	I love winter scenes.	Seek out development for the east side of Lake Ave in keeping with the existing buildings. Apply principals of Feng Shui, to create harmony, balance, peacefulness.
3: Good		
2: Fair		
2: Fair		
4: Great		
3: Good		
4: Great		
2: Fair		
3: Good		
2: Fair		
2: Fair		
4: Great	love it	perfect day for snowshoeing!
4: Great		
3: Good	Park, attractive businesses, and nice streetscape	Add new buildings to east side of street
2: Fair		



14P Rating	14P Like	14P Improve
2: Fair		
3: Good	nice green space and trees	
3: Good		
3: Good		
2: Fair		
1: Poor	Misleading. What happened to the snow?	Practice "Truth in Advertising." None of this comparing apples to oranges.
3: Good		
3: Good		
4: Great		
1: Poor		
3: Good		
1: Poor		
3: Good		
3: Good		
4: Great		
3: Good		
4: Great	still looks good	
1: Poor		
4: Great	New buildings on east side make you feel like you're somewhere important	better screen surface parking from park
3: Good		



15E Rating	15E Like	15E Improve
3: Good		
4: Great		
2: Fair		
1: Poor		
3: Good		
2: Fair	I love snow scenes.	Improve sidewalk snow removal practices for increased pedestrian use.
3: Good		
4: Great		
4: Great		
4: Great	This is the real Charlotte Beach, the one that residents of Charlotte live with for the majority of the year. No amount of architectural folly will change the climate...	
3: Good		
4: Great		
3: Good		
3: Good		
2: Fair		
3: Good		
4: Great	excellent view	Ice skating, snow shoeing, outdoor hockey tournament, curling!
4: Great		
2: Fair	View to terminal building	Screen the parking
2: Fair		



15P Rating	15P Like	15P Improve
1: Poor		
1: Poor	nice green space in foreground	remove/reduce buildings in background
3: Good		
4: Great		
2: Fair		too many buildings
1: Poor	Misleading. Where's the snow? What happened to the benches and sidewalks?	Seek out development for the east side of Lake Ave in keeping with the existing buildings. Apply principals of Feng Shui, to create harmony, balance, peacefulness.
3: Good		more landscaping
2: Fair		
4: Great		
1: Poor		It will only look like this for a few months a year.
4: Great		
1: Poor		
3: Good		
2: Fair		Lose the ugly highrise
4: Great		
3: Good		
1: Poor	I like the grass area	looks like hotels arcross from a mall parking lot
1: Poor		
4: Great	New buildings, better landscaped parking	Screen the parking more
4: Great		Public Art?



16E Rating	16E Like	16E Improve
4: Great		
4: Great		
2: Fair		
2: Fair		
3: Good		
3: Good	I love winter scenes.	Seek out development for the east side of Lake Ave in keeping with the existing buildings. Apply principals of Feng Shui, to create harmony, balance, peacefulness.
3: Good		
4: Great		
3: Good		
4: Great		
3: Good		
4: Great		
3: Good		
3: Good		
2: Fair		
3: Good		
4: Great	I like it	the tall building to the right is kind of ugly
4: Great		
2: Fair	trail to the east, terminal building in the distance	screen the parking
1: Poor		



16P Rating	16P Like	16P Improve
1: Poor		
2: Fair		reduce building size. dont use concrete bldg material.
2: Fair		
4: Great		
2: Fair		
2: Fair	Not very inviting! The Buildings in the distance make this look more like a NYC neighborhood instead of a "quaint village."	Seek out development for the east side of Lake Ave in keeping with the existing buildings. Apply principals of Feng Shui, to create harmony, balance, peacefulness.
4: Great	parking for non-residents	
2: Fair		
4: Great		
1: Poor		It will only look like this for a few months a year.
4: Great		
1: Poor		
3: Good		
2: Fair		UGLY high rise
3: Good		
2: Fair		
1: Poor	lot of trees in the parking lot	still looks like hotels across from a mall parking lot
1: Poor		
3: Good	New buildings	Appears to be new surface parking in park?
4: Great		



17E Rating	17E Like	17E Improve
4: Great		
1: Poor		re-purpose concrete parkinglot
1: Poor		
1: Poor		
3: Good		
2: Fair	I love winter scenes.	Seek out development for the east side of Lake Ave in keeping with the existing buildings. Apply principals of Feng Shui, to create harmony, balance, peacefulness.
2: Fair	snow plowing done	
3: Good		
2: Fair		
4: Great		
2: Fair		
4: Great		
2: Fair		
3: Good		
1: Poor		
3: Good		
2: Fair		it looks like a parking lot in the winter
4: Great		
1: Poor	Lots of surface parking, no active uses	Add marina, buildings
1: Poor		



17P Rating	17P Like	17P Improve
1: Poor		
2: Fair		marina looks great but need to reduce scale and soften style of buildings.
4: Great		
4: Great		
3: Good		
1: Poor	Misleading. Where's the snow? Where are the 8-foot high security fences [ which will obstruct the views ]around the marina to protect the boat owners and their sizable investments [\$800k - \$5 million for a 70-foot boat ] from theft, vandalism? The buildings in the background are way too high and imposing.	Seek out development for the east side of Lake Ave in keeping with the existing buildings. Apply principals of Feng Shui, to create harmony, balance, peacefulness. Practice "Truth in Advertising!"
4: Great	marina	buildings appear a bit too high. Think Coburg for height and design
4: Great		
4: Great		
1: Poor		There is no need to dig a new marina. It will not stimulate enough revenue to make this worth it. These buildings are too big and make no sense for Charlotte. Think smaller and more modest.
4: Great		
1: Poor		
4: Great	The building density makes the area feel more metropolitan. Having the marina make the space more pleasant.	
1: Poor		Please, please some architecture we can be proud of ... and no ugly highrise
4: Great		
4: Great		
2: Fair	the bright blue water is nice, sailboats are nice	the giant hotel shaped buildings really detract from everything
1: Poor		
4: Great	Hard to believe this could be Rochester!	Nothing
4: Great		



18E Rating	18E Like	18E Improve
4: Great		
3: Good		
1: Poor		
2: Fair		
3: Good		
2: Fair	I love snow scenes.	
1: Poor	no comment	
3: Good		
2: Fair		
3: Good		
2: Fair		
4: Great		
2: Fair		
3: Good		
3: Good		
3: Good		
4: Great	looks fantastic	
4: Great		
2: Fair	Nice trees	Better define street, reduce high security feel of fencing, lights
1: Poor		



18P Rating	18P Like	18P Improve
1: Poor		
2: Fair		love the marina, buildings must be reduced and styles need to be more inline with port/village style
4: Great		
4: Great		
3: Good		
1: Poor	Misleading. Where's the snow? Where are the 8-foot high security fences [ which will obstruct the views ]around the marina to protect the boat owners and their sizable investments [\$800k - \$5 million for a 70-foot boat ] from theft, vandalism? The buildings in the background are way too high and imposing.	Seek out development for the east side of Lake Ave in keeping with the existing buildings. Apply principals of Feng Shui, to create harmony, balance, peacefulness. Practice "Truth in Advertising!"
4: Great	marina	
3: Good		
4: Great		
1: Poor		Digging a new marina is a permanent undertaking. Remember that this will stand empty for the majority of the year once the snow birds fly back to Florida. And there is no way of knowing the long term environmental impact of something like this. And there is no reason to chance it what with the issues Charlotte Beach already faces. And those huge buildings are ugly and will cast unwanted shadow and stand EMPTY for 6-8 months out of the year. This is not Miami, Chicago or even Detroit. Our city center is MILES from our water front and modesty is key to making it a successful port. There is no need for huge garish buildings.
4: Great		
1: Poor		
4: Great	looks like the site has the potential to have more life.	
2: Fair		Too much high rise. Style feels inauthentic for this area
4: Great		
4: Great		Looks Good, hope the water is that blue!
1: Poor	the marnia looks nice, kinda of looks like what all over there	I don't like the buildings in the background
1: Poor		
4: Great	Wow	Nothing
4: Great		



19E Rating	19E Like	19E Improve
2: Fair		
1: Poor		reconfigure road and soften landscape
1: Poor	Come on?	
1: Poor		
1: Poor		
1: Poor		Seek out development for the east side of Lake Ave in keeping with the existing buildings. Apply principals of Feng Shui, to create harmony, balance, peacefulness.
1: Poor	nothing	
2: Fair		
1: Poor		
4: Great		
1: Poor		
4: Great		
2: Fair		
1: Poor		
4: Great		
1: Poor	Poor pavement, no definition, no sidewalks, lots of surface parking	Curbs, sidewalks, tree lawn, etc
1: Poor		



19P Rating	19P Like	19P Improve
3: Good		
4: Great		
4: Great		
4: Great		
3: Good		
1: Poor	Misleading. Where are the 8-foot high security fences [ which will obstruct the views ]around the marina to protect the boat owners and their sizable investments [\$800k - \$5 million for a 70-foot boat ] from theft, vandalism?	Practice "Truth in Advertising!"
4: Great	promenade	maybe an elevated promenade like Coburg around Marina
4: Great		
3: Good		
1: Poor		Digging a new marina is a permanent undertaking. Remember that this will stand empty for the majority of the year once the snow birds fly back to Florida. And there is no way of knowing the long term environmental impact of something like this. And there is no reason to chance it what with the issues Charlotte Beach already faces.
4: Great		
1: Poor		
4: Great		
3: Good		
3: Good		
4: Great		
4: Great		soooo... where are all the trucks and boat trailers that come to use the port/marnia going to park if we eliminate that?
1: Poor		
4: Great	I'm on a boat!	Fewer empty slips!
4: Great		



20E Rating	20E Like	20E Improve
3: Good		
2: Fair	trees are nice	reduce pavement
1: Poor		
2: Fair		
3: Good		
2: Fair	Openness, but it reminds me of a drag race track. Spencer Speedway!	Seek out development for the east side of Lake Ave in keeping with the existing buildings. Apply principals of Feng Shui, to create harmony, balance, peacefulness.
2: Fair	wide roads, curb cuts	
2: Fair		
1: Poor		
4: Great		
2: Fair		
4: Great		
3: Good		
3: Good		A bit more landscaping
2: Fair		
3: Good		
3: Good		
4: Great		
3: Good	Nice street, well landscaped	Needs buildings
1: Poor		



20P Rating	20P Like	20P Improve
1: Poor		
3: Good		all good except view of bulding at the end
4: Great		
4: Great		
2: Fair		too congested
1: Poor	Misleading. Where are the 8-foot high security fences [ which will obstruct the views ]around the marina to protect the boat owners and their sizable investments [\$800k - \$5 million for a 70-foot boat ] from theft, vandalism? Where are the rows of cars parked and traveling?	Practice "Truth in Advertising!"
4: Great	better flow	maybe low landscaping. add street level promenade with handrails
4: Great		
4: Great		
1: Poor		Digging a new marina is a perminent undertaking. Remember that this will stand empty for the majority of the year once the snow birds fly back to Florida. And there is no way of knowing the long term environmental impact of something like this. And there is no reason to chance it what with the issues Charlotte Beach already faces.
4: Great		
1: Poor		
3: Good		
2: Fair		Lose the ugly high rise
3: Good		
4: Great		
3: Good		crosswalks, get rid of the highrise
1: Poor		
4: Great	Attractive buildings on one side, attractive marina on the other, what's not to like?	Pave the trail/sidewalk on the east side
4: Great		



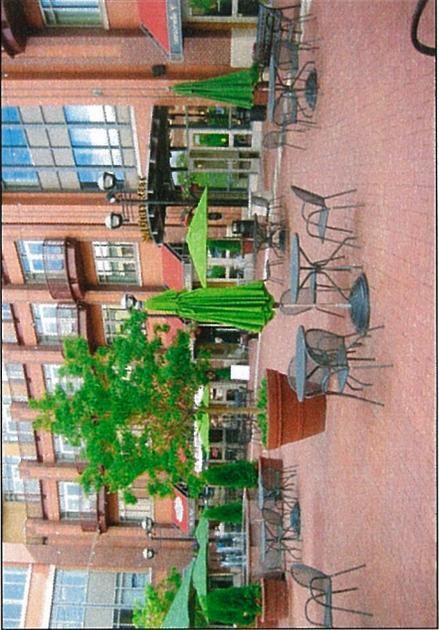
21E Rating	21E Like	21E Improve
3: Good		
2: Fair	It's winter!	
1: Poor	nothing	
3: Good		
3: Good		
3: Good		Re
3: Good		
4: Great		
2: Fair		
1: Poor		
4: Great		
3: Good	River, terminal building	More activity
1: Poor		

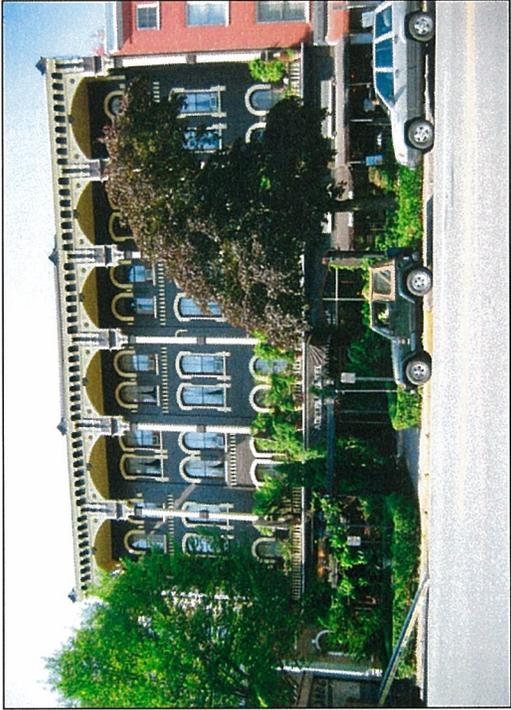


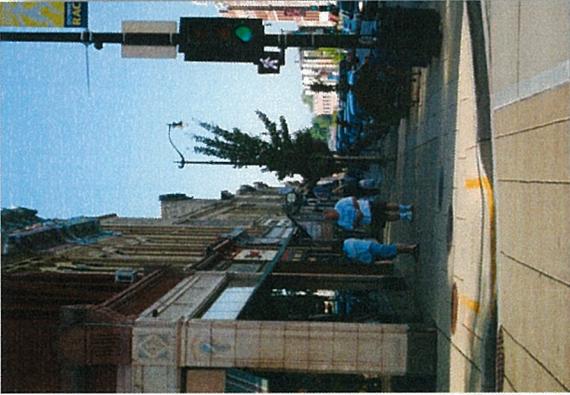
21P Rating	21P Like	21P Improve
1: Poor		
1: Poor		buildings are too tall and dont represent a harbor/port feel. very hard and depressing..
3: Good		
4: Great		
3: Good		
1: Poor	Misleading. Did that light gray building in the background suddenly get bigger? Where's the snow?	Practice "Truth in Advertising!"
3: Good		needs wharf side boat docking?
4: Great	Great, conditioned on footprint details	
2: Fair		
4: Great		
4: Great		
1: Poor		
3: Good		
1: Poor		Ugly, ugly high rises. And where is the parking?
4: Great		
3: Good		More Public Dock Space City Could make money if they put in parking meters like for cars....
1: Poor		
1: Poor		
4: Great	New buildings	Nothing
4: Great		

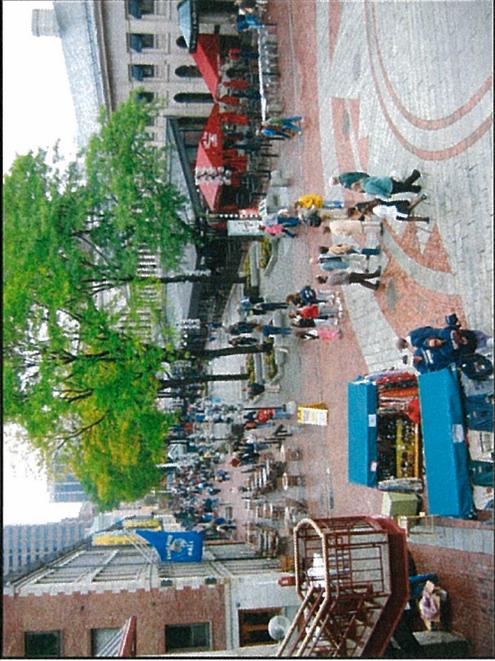
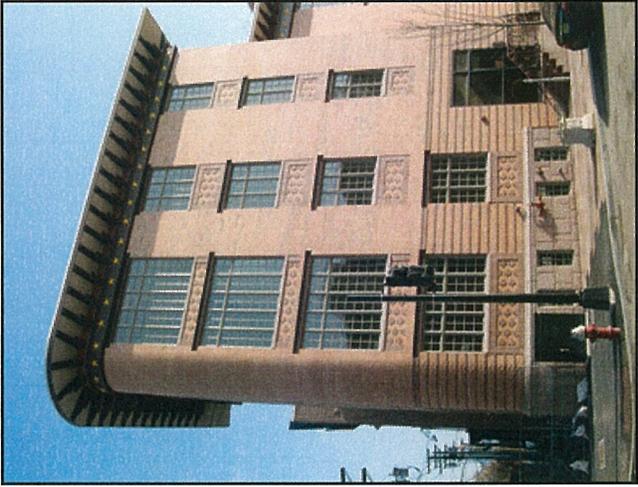
## **Visual Preference Survey – Part 2**

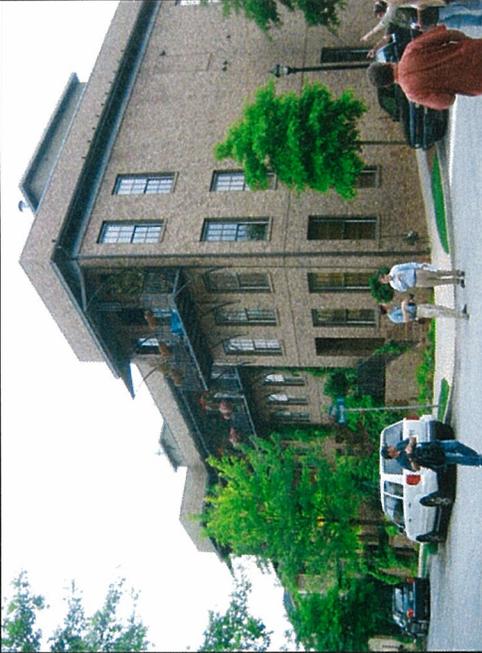
City of Rochester  
**Port Public Marina & Mixed Use Development Project**  
**Visual Preference Survey – Part 2**

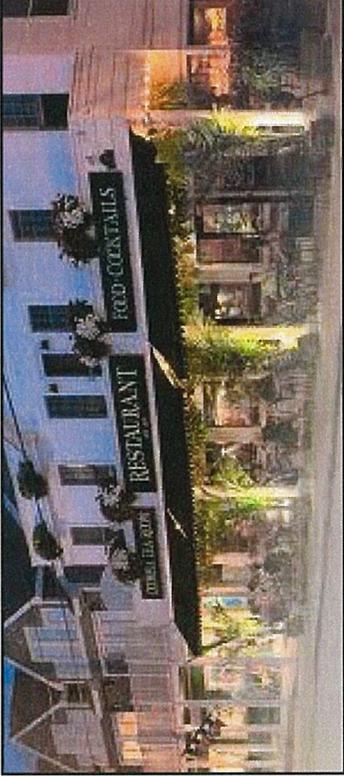
Development Example	Rating 1 Poor, 2 Fair 3 Good, 4 Great	What do you like about it?	How would you improve it?
	2.8	<ul style="list-style-type: none"> <li>• Buildings small scale</li> <li>• Low buildings, leaving vistas</li> <li>• Uniformity of sign holders</li> <li>• Shows adherence to design guidelines</li> <li>• Someplace I'd like to go</li> <li>• Pleasant for Pedestrians</li> <li>• Uniform look but w/variety</li> <li>• Like sidewalk seating</li> </ul>	<ul style="list-style-type: none"> <li>• Add more people</li> <li>• Needs better street signs</li> <li>• Treatment</li> <li>• Needs street trees</li> <li>• Wider sidewalks</li> </ul>
	2.6	<ul style="list-style-type: none"> <li>• Nothing</li> <li>• Like court yard w/trees &amp; no cars</li> <li>• Will kept courtyard w/businesses along the periphery</li> </ul>	<ul style="list-style-type: none"> <li>• Reduce height</li> <li>• Too sterile, stark, industrial</li> <li>• Where do the cars go?</li> <li>• Sterile</li> <li>• Uninteresting</li> <li>• Overdeveloped</li> </ul>

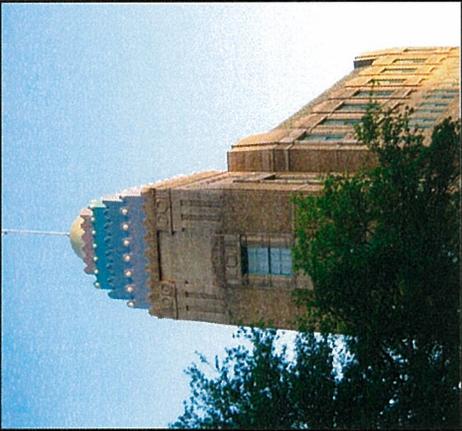
Development Example	Rating 1 Poor, 2 Fair 3 Good, 4 Great	What do you like about it?	How would you improve it?
	2.6	<ul style="list-style-type: none"> <li>• Building style in line w/Port Harbor falls</li> <li>• Yuck, Nothing , Garish</li> <li>• Varied fronts &amp; roofs</li> <li>• Nice seaside flavor</li> <li>• Harmonizes well w/historic Charlotte</li> <li>• Decent Architecture</li> </ul>	<ul style="list-style-type: none"> <li>• One level too high</li> <li>• Demolish &amp; start over</li> <li>• Very private</li> <li>• Unapproachable</li> <li>• Suburban</li> <li>• Car-oriented</li> <li>• Unwelcoming</li> </ul>
	2.4	<ul style="list-style-type: none"> <li>• Good street landscaping</li> <li>• Historic architecture</li> <li>• First floor retail</li> <li>• Dense</li> </ul>	<ul style="list-style-type: none"> <li>• Too high</li> <li>• Too ornate</li> <li>• Looks like a circus</li> <li>• Too tall</li> <li>• Architecture is inconsistent w/the area</li> <li>• Tacky imitation Moorish</li> <li>• Has nothing to do w/Rochester</li> </ul>

Development Example	Rating 1 Poor, 2 Fair 3 Good, 4 Great	What do you like about it?	How would you improve it?
	2.4	<ul style="list-style-type: none"> <li>• Smaller scale</li> <li>• Clean streets &amp; building facade</li> <li>• Dense</li> <li>• Urban</li> <li>• Attractive street scape</li> </ul>	<ul style="list-style-type: none"> <li>• Wider sidewalks</li> </ul>
	2.9	<ul style="list-style-type: none"> <li>• Inviting</li> <li>• Nice visual interest</li> <li>• Fun for kids</li> <li>• Family-friendly</li> <li>• Active space</li> </ul>	<ul style="list-style-type: none"> <li>• Put up security fence around the water feature so people don't step and fall</li> </ul>

Development Example	Rating 1 Poor, 2 Fair 3 Good, 4 Great	What do you like about it?	How would you improve it?
	2.9	<ul style="list-style-type: none"> <li>• Good mix and variety</li> <li>• Likes the shed roofed enclosure area for year-round use</li> <li>• Lively</li> <li>• No car interference</li> <li>• Pedestrian friendly</li> <li>• Active</li> <li>• Sidewalk café</li> <li>• Many businesses</li> </ul>	<ul style="list-style-type: none"> <li>• Style not in line w/Port feel</li> <li>• Add more gardens and greenery to soften brick</li> <li>• Where do cars go?</li> <li>• Might be nice w/water vistas</li> </ul>
	1.9	<ul style="list-style-type: none"> <li>• Attractive architecture</li> </ul>	<ul style="list-style-type: none"> <li>• Reduce height</li> <li>• Change materials</li> <li>• Add greenery to soften brick &amp; glass</li> <li>• Not interesting</li> <li>• Not appropriate for Charlotte- neither historic nor in ambience</li> <li>• No active 1<sup>st</sup> floor</li> <li>• Does not fit w/Port2.4</li> </ul>

Development Example	Rating 1 Poor, 2 Fair 3 Good, 4 Great	What do you like about it?	How would you improve it?
	2.4	<ul style="list-style-type: none"> <li>• Nice size</li> <li>• Inviting</li> <li>• Reduced density</li> <li>• Dense housing</li> <li>• Balcones</li> <li>• Up to the sidewalks</li> </ul>	<ul style="list-style-type: none"> <li>• Too urban</li> <li>• Wrong style for Charlotte</li> <li>• No street side activity</li> <li>• More windows on the street</li> <li>• 1<sup>st</sup> floor commercial</li> </ul>
	2.5	<ul style="list-style-type: none"> <li>• Materials</li> <li>• Porches</li> <li>• Dense</li> <li>• Attractive</li> <li>• Water-oriented</li> </ul>	<ul style="list-style-type: none"> <li>• Too high</li> <li>• Suggest 4 floors max</li> <li>• Feels like Disney</li> <li>• Seems private, are there walkways? Retail?</li> <li>• Garish</li> <li>• Horrible</li> </ul>

Development Example	Rating 1 Poor, 2 Fair 3 Good, 4 Great	What do you like about it?	How would you improve it?
	2.9	<ul style="list-style-type: none"> <li>• Appropriate size buildings</li> <li>• Tiers add separation and order</li> <li>• Public access to water</li> <li>• Nice wide promenade</li> <li>• Absence of ugly high rises</li> <li>• Street-level interest</li> <li>• Dense</li> <li>• Water-oriented</li> </ul>	<ul style="list-style-type: none"> <li>• Too urban</li> <li>• Ugly yellow trash bins</li> <li>• Railing at water's edge?</li> <li>• Can't tell if there is retail businesses facing the walkway</li> </ul>
	3.1	<ul style="list-style-type: none"> <li>• Small town</li> <li>• Harbor like</li> <li>• Homey</li> <li>• Quaint village</li> <li>• Inviting</li> <li>• Outside seating</li> <li>• Ok sidewalk width</li> <li>• Nice mixed styles</li> <li>• Historic</li> <li>• Excellent façade</li> <li>• Sidewalks café</li> <li>• Nice feel</li> </ul>	<ul style="list-style-type: none"> <li>• Needs wider sidewalks</li> </ul>

Development Example	Rating 1 Poor, 2 Fair 3 Good, 4 Great	What do you like about it?	How would you improve it?
	2.1	<ul style="list-style-type: none"> <li>• Has some visual appeal</li> <li>• Historic</li> </ul>	<ul style="list-style-type: none"> <li>• Hate the materials</li> <li>• Doesn't belong in waterfront theme</li> <li>• Dated</li> <li>• "Orientalistic"</li> <li>• Ugly</li> </ul>
	1.9	<ul style="list-style-type: none"> <li>• Nice if you can't use the waterfront &amp; don't care about blocking views</li> <li>• Unappealing</li> <li>• Dense</li> <li>• Sterile</li> </ul>	<ul style="list-style-type: none"> <li>• A reasonable look for here, but not a whole development in this style</li> <li>• Unadorned</li> <li>• Looks cheap</li> <li>• Separated from water</li> <li>• Minimal public access</li> <li>• No 1<sup>st</sup> floor retail</li> </ul>
	2.2	<ul style="list-style-type: none"> <li>• Visually interesting</li> <li>• Low building provides feeling of openness</li> <li>• The mix of different windows</li> <li>• Building materials &amp; the recessed porches give it a harmonious flair</li> <li>• Vibrant architecture</li> <li>• 1<sup>st</sup> floor retail</li> </ul>	<ul style="list-style-type: none"> <li>• Change the materials &amp; color</li> <li>• Add more greenery</li> <li>• Not a waterfront look</li> <li>• Looks like a bad strip mall</li> <li>• Could be more inviting</li> <li>• Too synthetic</li> </ul>

**STATE ENVIRONMENTAL QUALITY REVIEW  
(SEQR)**

**FINAL  
SITE SPECIFIC/GENERIC ENVIRONMENTAL IMPACT  
STATEMENT  
FEIS**

**Proposed Action:  
City of Rochester Port Public Marina and Mixed Use Development Project**

**SEQR Environmental Impact Statement  
NEPA Environmental Assessment**

**LEAD AGENCY:  
Thomas S. Richards  
Mayor, City of Rochester  
City Hall, Room 307A, 30 Church Street  
Rochester, NY 14614-1290**

**Appendix C**

**Revised Marina District Code**



**M-D Marina District**

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**Purpose**

This Section provides regulatory standards governing building form, land use and new public open spaces within the Marina District (M-D). This district was created in response to the opportunities for major improvements to the Port of Rochester area that will be made possible by the construction of a new marina, the extension of River Street and the reconfiguration and reconstruction of other existing streets and blocks. The Marina District will be the next chapter in Charlotte’s history as a lakeshore resort community. The ultimate goal of the Marina District code is to foster the creation of a district that will attract visitors because it is distinctive and memorable, and will endure because it is valued by residents and visitors alike. This Section incorporates a Form-Based Code intended to govern the development of PARCELS I, II, III and the Terminal Building site, which have been designated for private development. In the Marina District, the primary emphasis is placed upon the physical form of buildings, civic spaces and placemaking. While land uses are regulated, they are a secondary focus within this district. The Marina District code is intended to be independent of the remainder of the Zoning Code. No other section of the Zoning Code is applicable within the Marina District unless specifically referenced in this section. The principal components of the code are described herein:

**§120-77.2.010 Regulating Plan**

The REGULATING PLAN is the key to the Marina District Form-Based Code. It indicates the Building Types and Frontage Types permitted for each PARCEL and provides other specific information necessary to create the physical form and character desired for the Marina District.

**§120-77.2.020 Building Envelope Standards**

The BUILDING ENVELOPE STANDARDS establish the minimum and maximum three dimensional spatial envelope within which a building may be constructed, as well as requirements for a limited number of permitted or required building elements, such as storefronts and windows. There are general provisions applicable to all buildings as well as specific standards for each Frontage Type and Building Type. The intent of the BUILDING ENVELOPE STANDARDS is to shape public space through placement and envelope controls on buildings that frame public rights-of-way and open spaces. The Building Envelope Standards also provide for uses that are permitted on ground stories and in upper stories correlated to each Building Type and Frontage Type.

**§120-77.2.030 Building Function (Use) Standards**

The Building Function Standards establish categories of uses that are permitted anywhere within the Marina District, categories that are specially permitted and categories that are prohibited.

**§120-77.2.040 Civic Square Standards**

The CIVIC SQUARE Standards establish the basic parameters governing the required CIVIC SQUARE on PARCEL I.

**§120-77.2.050 Parking and Loading Standards**

The Parking and Loading Standards establish the basic parameters governing the placement of and access to parking facilities. The standards include requirements for permanent parking created in conjunction with new buildings, interim surface parking on undeveloped portions of PARCELS I, II and III and bicycle parking.

**§120-77.2.060 Architectural Standards**

The Architectural Standards are intended to provide a minimal level of quality and to promote a coherent character throughout the Marina District. The Architectural standards govern each building's elements, regardless of Building or Frontage Type, and set parameters for acceptable materials, configurations and techniques.

**§120-77.2.070 Review and Approval Process** The Review and Approval Process includes procedures for the approval of proposed developments that are not fully compliant with the requirements of the Marina District Form-Based Code. All proposed developments that are fully compliant will be approved administratively.

**§120-77.2.080 Definitions**

Words and/or terms used in this Section that are not defined in §120-208 or have a different definition than that shown in §120-208 are included in this Section. Such words and/or terms will appear in the text of §120-77.2 in a SMALL CAPITALS format.

.010

Regulating Plan

**M-D Marina District**

**§120-77.2.010 Regulating Plan**

	District Boundary
	Parcel Boundary
	Required Build-To Line
	Required Frontage Zone
	Parking Setback Line
	Mandatory Commercial Frontage
	Allowable Parking/Loading Ingress/Egress Location
	Building Type 'A' Permitted
	Building Type 'B' Permitted
	Building Type 'A' or 'B' Permitted
	Building Type 'B' or 'C' Permitted
	Building Type 'D' Permitted
	Terminal Building Parcel
	Required Civic Square

KEY TO PARCEL III

A= 25' Minimum Setback

B= 15' Minimum Setback from River

C= 35' Maximum

D= 2' Min., 50' Max. Setback from Marina Promenade

E= 2' Min, 135' Max. Setback

KEY TO TERMINAL BUILDING

A= 5' Minimum

B= 30' Minimum

C= 50' Maximum

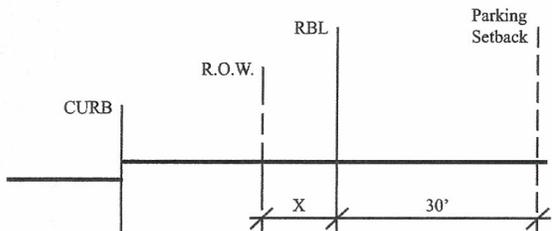
D= 2' Minimum Setback from Marina Promenade

E= Match Existing Setback from River (Minimum)

F= Southernmost Point of Portion Permitted Closer to River to Align with Straight Portion of Marina Promenade

G= Northernmost Point of Portion Permitted Closer to River to Align with Straight Portion of Setback from Marina Promenade

 Addition Permitted to be Removed



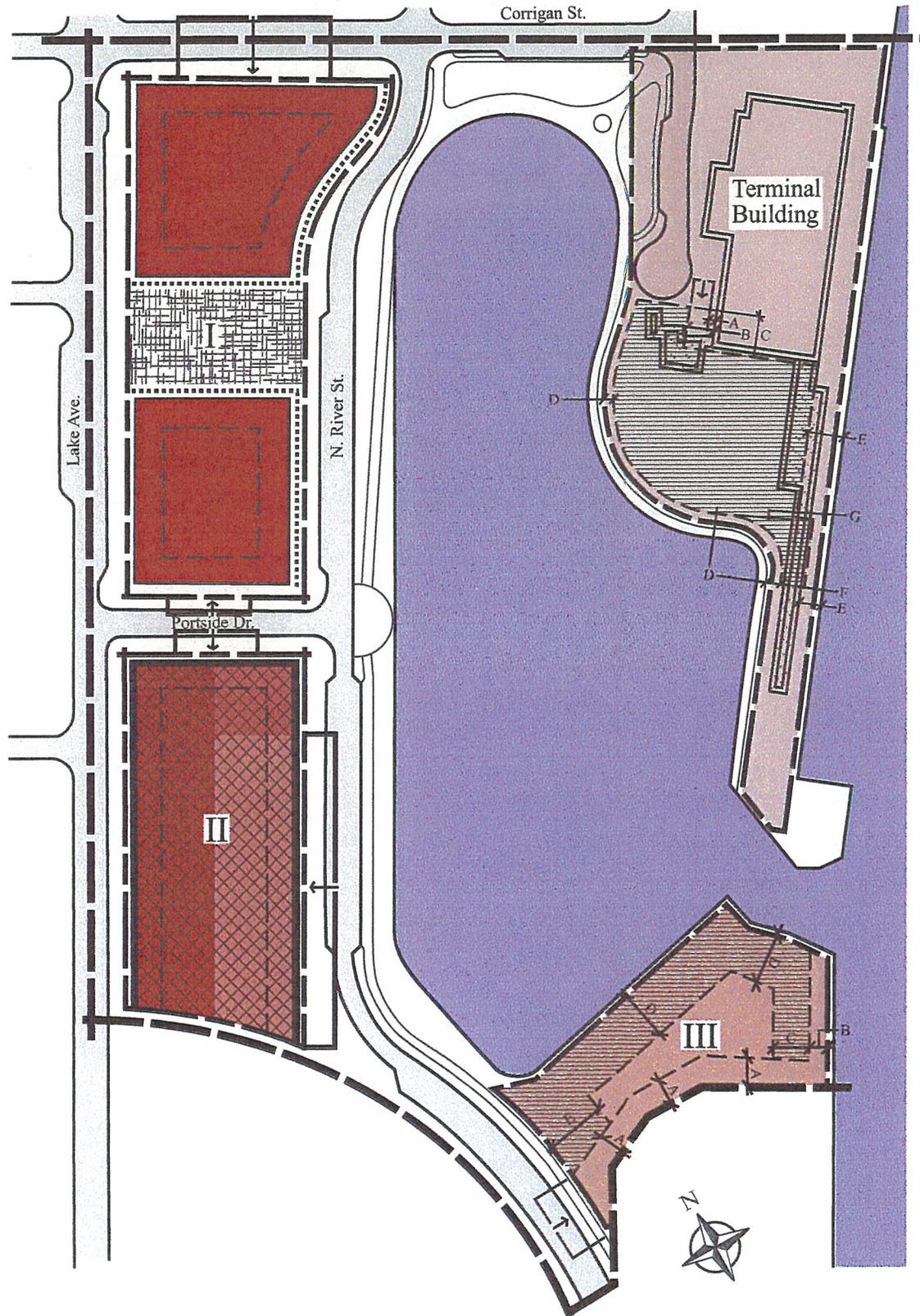
CURB      R.O.W.      RBL      Parking Setback

X      30'

X = 2' at Lake Avenue  
 2' at Corrigan Street  
 2' at Portside Drive  
 2' at North River Street South of Portside Drive  
 5' at North River Street North of Portside Drive

LOCATION OF REQUIRED BUILD-TO-LINE (RBL)  
 AND PARKING SETBACK AT PARCELS I & II

KEY TO REGULATING PLAN



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# .020

Building Envelope Standards

**M-D Marina District**

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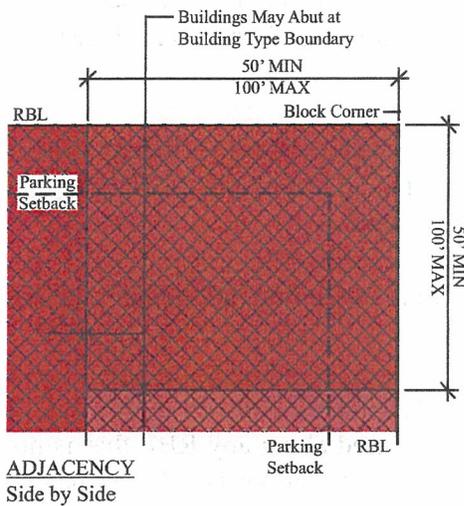
**§120-77.2.020 Building Envelope Standards**

**Intent.** The primary intent of the BUILDING ENVELOPE STANDARDS is to define and shape street and civic spaces in order to create a vital and coherent public realm. The interface of private building frontages with public thoroughfares and civic spaces shapes the public realm and is the principle focus of the BUILDING ENVELOPE STANDARDS.

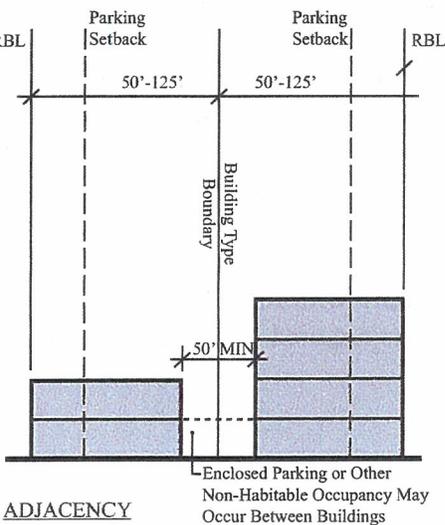
**A. General Provisions**

The following provisions are applicable to the BUILDING ENVELOPE STANDARDS for each Frontage Type and Building Type unless otherwise indicated within the standards for each Type.

**1. Adjacencies**



a. Side by Side: Where the REGULATING PLAN indicates abutting Building Types and the boundary line is perpendicular to the REQUIRED BUILD-TO LINE (RBL), the boundary between each Building Type may be located between 50 feet and 100 feet from the BLOCK CORNER. No side yard is required and buildings may abut each other with no break in the continuity of the building FAÇADES;



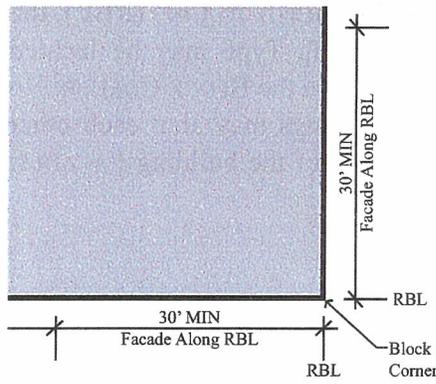
b. Back to Back: Where the REGULATING PLAN indicates abutting Building Types and the boundary line is parallel to and does not intersect with the RBL, the boundary between each Building Type may be located between 50 and 125 feet from the RBL on either side. Rear yards shall be provided on either or both sides of the Building Type boundary such that the minimum distance between rear building walls is 50 feet. This requirement is not applicable where there is structured parking or other non-habitable space located behind the PARKING SETBACK LINE, but is applicable to portions of the building located above such a structure.

**2. Height Measurement**

- a. The height of all buildings is measured in stories unless otherwise indicated in the BUILDING ENVELOPE STANDARDS for each building type;
- b. An ATTIC STORY is not included in the height measurement and may be added to the maximum height of a building; unless otherwise indicated in the BUILDING ENVELOPE STANDARDS for each building type;
- c. Mezzanines with an area greater than 1/3rd of the floor area of the story in which they are located shall be counted as a full story in the height measurement.

**3. Siting**

(Provisions of this section are applicable to PARCELS I and II only. See §120-77.2.020C(4)(a) and §120-77.2.020C(5)(c) for the siting requirements for PARCEL III and the Terminal Building).



BLOCK CORNER

a. The building FAÇADE shall be built to the REQUIRED BUILD-TO LINE (RBL) within 30 feet of a BLOCK CORNER or a chamfered or rounded corner.

b. Blank lengths of wall exceeding 20 linear feet are prohibited on all RBL's;

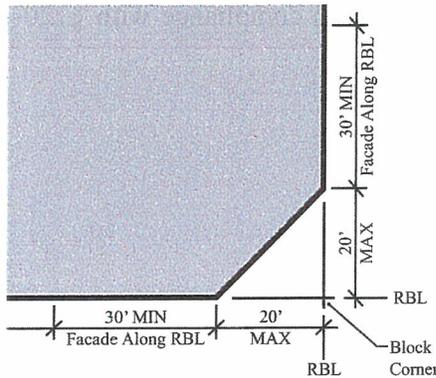
c. A STREET WALL not less than 6 feet nor more than 12 feet in height shall be required along any RBL that is not occupied by a building FAÇADE. The STREET WALL shall be located no more than 24 inches behind the RBL;

d. The PARKING SETBACK LINE is located 30 feet behind the RBL and extends vertically as a plane. In permanent parking facilities, vehicle parking shall be located behind the PARKING SETBACK LINE except where provided completely below grade;

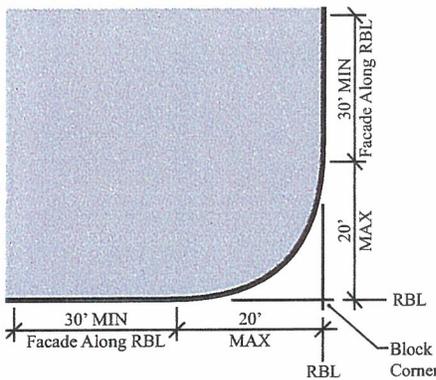
e. Corner lots and through lots shall comply with the RBL requirements for their full frontages on all streets and the required CIVIC SQUARE unless otherwise specified.

**4. Exceptions to the Required Build-To Line (RBL)**

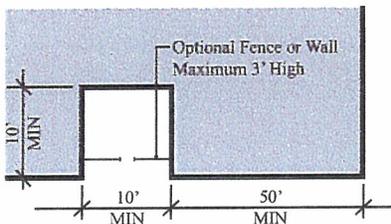
(Provisions of this section are applicable to PARCELS I and II only. See §120-77.2.020C(4)(a) and §120-77.2.020C(5)(c) for comparable requirements for PARCEL III and the Terminal Building.)



CHAMFERED CORNER



ROUNDED CORNER



FORECOURT

a. The building FAÇADE may include variations that project or recess from the RBL up to 24 inches;

b. Storefront assemblies (doors, display windows, bulkheads and associated framing) may be recessed behind or project beyond the RBL by up to 24 inches and storefront entrances may be recessed from the storefront assembly;

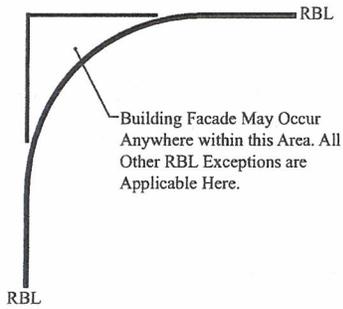
c. Within 20 feet of the BLOCK CORNER, the GROUND STORY FAÇADE may be chamfered to form a corner entry;

d. Within 20 feet of the BLOCK CORNER, the GROUND STORY FAÇADE may be curved to form a rounded corner;

e. Entry forecourts may be created by recessing the FAÇADE for a portion of the RBL. A forecourt shall be at least 10' by 10'. A fence or wall no higher than three feet, with a pedestrian opening, may be provided to define the space of the court. Only one forecourt per PARCEL is permitted to front on any street or the required CIVIC SQUARE except in Parcel I, one forecourt per street or CIVIC SQUARE is permitted north of the CIVIC SQUARE and south of the CIVIC SQUARE. Forecourts shall be located at least 50 feet from the BLOCK CORNER or a chamfered or rounded corner;

f. Upper stories may be set back from the RBL. Except where there are porches and/or BALCONIES at the BLOCK CORNER, upper story setbacks shall be located at least 50 feet from the BLOCK CORNER or chamfered or rounded corner;

g. Ground stories along non-commercial frontages may be set back from the RBL as needed to accommodate porches and/or stoops;



CURVATURE

h. Where the RBL follows the curvature of a street, the building FAÇADE may be located anywhere within the area formed by straight extensions of the RBL from each end of the curve. All other exceptions to the RBL may be utilized in this portion of the building FAÇADE;

i. Projecting FAÇADE elements in compliance with §120-77.2.060F.

**B. Frontage Types**

The Frontage Types describe the GROUND STORY condition and may be paired with any Building Type. Either Frontage Type may be located anywhere within the Marina District except that only the Commercial Frontage Type is permitted where mandatory Commercial Frontage is designated on the REGULATING PLAN.

**1. Commercial**

Commercial Frontages are specifically designed to be suitable for retail, restaurant, service and similar uses that encourage a substantial amount of pedestrian interaction with the public thoroughfare. They are characterized by substantial storefront windows with closely spaced entrances from the street. They may have projecting FAÇADE elements over the sidewalk such as awnings, canopies or GALLERIES.

a. GROUND STORY Floor Elevation:

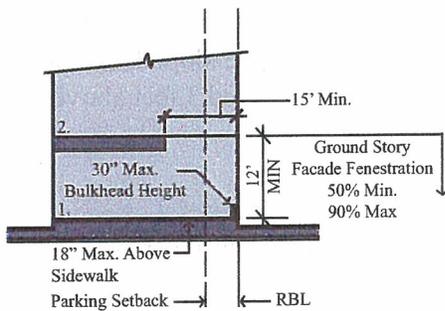
Maximum 18 inches above the average sidewalk elevation at the REQUIRED BUILD-TO LINE (RBL) or the REQUIRED FRONTAGE ZONE (RFZ); where sidewalks are sloped, elevation may exceed 18 inches but may not exceed 3 feet above the sidewalk;

b. Minimum GROUND STORY Height:

12 feet clear (floor to ceiling) contiguous to the RBL or RFZ frontage for a minimum depth of 15 feet;

c. FENESTRATION:

The primary GROUND STORY FAÇADE shall have areas of transparency equal to at least 50% but no more than 90% of the FAÇADE area of the GROUND STORY. Storefront bulkheads shall have a maximum height of 30 inches above finished floor;



COMMERCIAL FRONTAGE

d. Maximum Distance between Entrances:

Doors or entrances with public access to ground story spaces shall be provided at intervals of not less than 75 feet. Entrances shall remain unlocked and available for public use during regular business hours;

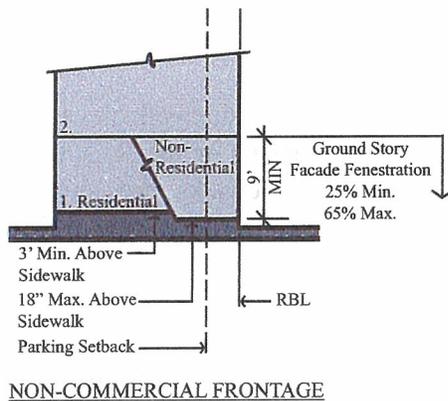
e. Sidewalk:

The space between the public right-of-way boundary and the RBL shall be paved so as to become an extension of the public sidewalk. Paving materials shall match or complement the sidewalk. Asphalt and gravel paving are not permitted;

- f. Use:  
Residential uses, including lodging, are not permitted on the GROUND STORY except for lobbies and associated common spaces.

**2. Non-Commercial**

Non-Commercial Frontages are suitable for residential, lodging, office and other uses that do not depend on a substantial amount of pedestrian interaction with the public thoroughfare. They are characterized by smaller windows and are elevated above the sidewalk level in order to reduce views into the interior spaces from the sidewalk. Entrances may be spaced farther apart.



- a. GROUND STORY Floor Elevation (where there are residential uses, including lodging):  
Minimum 3 feet above the average sidewalk elevation at the REQUIRED BUILD-TO LINE (RBL) or the REQUIRED FRONTAGE ZONE (RFZ); where sidewalks are sloped elevation may be minimum 2 feet and maximum 6 feet above the sidewalk;

- b. GROUND STORY Floor Elevation (where there are non-residential uses):  
Maximum 18 inches above the average sidewalk elevation at the RBL or RFZ; where sidewalks are sloped, elevation may exceed 18 inches but may not exceed 3 feet above the sidewalk;

- c. Minimum GROUND STORY Height:  
The GROUND STORY shall have at least 9 feet of clear interior height (floor to ceiling);

- d. FENESTRATION:  
The primary GROUND STORY FAÇADE shall have areas of transparency equal to at least 25% but no more than 50% of the total FAÇADE area of the GROUND STORY, with each FAÇADE area calculated independently;

- e. Sidewalk:  
The space between the public right-of-way boundary and the RBL may be paved so as to become an extension of the public sidewalk or may be landscaped so as to become a buffer between the building and the street. Paving materials shall match or complement the sidewalk. Asphalt and gravel paving are not permitted;

f. Use:

Although this frontage type is best suited to residential or low intensity non-commercial uses, any use allowed within the Marina District is permitted.

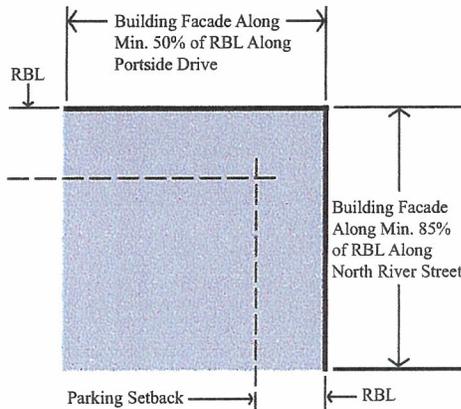
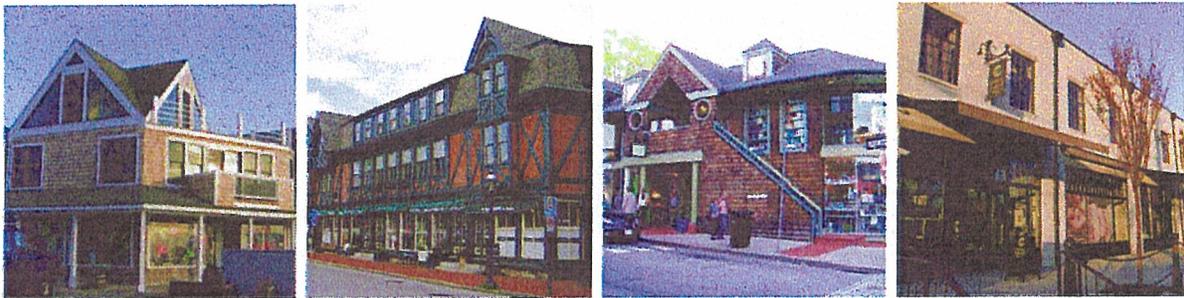
**C. Building Types**

The REGULATING PLAN indicates permitted locations for each Building Type. For each Building Type there is a set of BUILDING ENVELOPE STANDARDS that are intended to achieve the desired built form and pedestrian orientation of the Marina District.

**1. Type A, Low Rise Block**



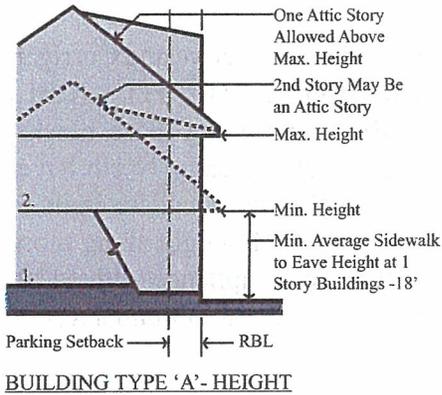
This Building Type is limited in height in order to preserve views of the harbor from the Lighthouse Trail and views of the lighthouse from the marina area. This Building Type may include attached single-family dwellings (townhouses) or other small scale buildings and may have commercial or non-commercial frontages. This Building Type is permitted on PARCEL II.



BUILDING TYPE 'A'- SITING

**a. Siting:**

Building FAÇADES fronting on Corrigan Street shall be built to the REQUIRED BUILD-TO LINE (RBL) for at least 50% of the RBL length along that block. Building FAÇADES fronting on North River Street shall be built to the RBL for at least 85% of the RBL length along that block. Exceptions to the RBL in compliance with §120-77.2.020A(4) are included within the minimum FAÇADE length;



**b. Building Height:**

Minimum one story, 18 feet from average sidewalk elevation to eave, or top of cornice or parapet; Maximum 2 stories, 1 additional story is permitted within the northernmost 50% of the area designated on the REGULATING PLAN for this building type;

**c. Upper Story FENESTRATION:**

Primary FAÇADES and FAÇADES visible from any street or open space shall have areas of transparency equal to at least 25% but no more than 50% of the total FAÇADE area, with each FAÇADE area calculated independently;

**d. Use:**

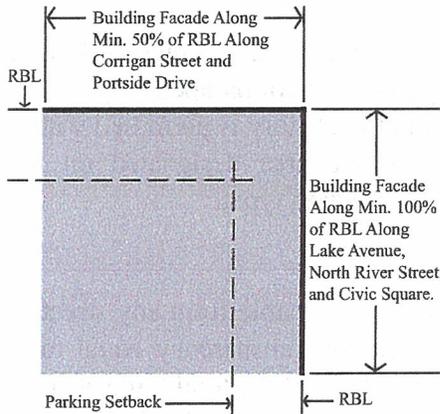
Non-residential uses are prohibited above residential uses.

**2. Type B, Mid-Rise Block**



The Mid-Rise Block is the basic background building that forms most of the fabric in neighborhood, town and village centers across the United States. The primary form is that of a two or more story building placed directly adjacent to or in very close proximity to the sidewalk. Mid-Rise Block buildings may or may not have commercial frontage on the GROUND STORY. Mid-Rise Block buildings typically house a wide range of uses and can be adapted to change their functions over time. Such buildings may be devoted to a single use or may have multiple uses. This range of uses will contribute to the vitality that is desired for the Marina District. This Building Type is permitted on PARCELS I and II.

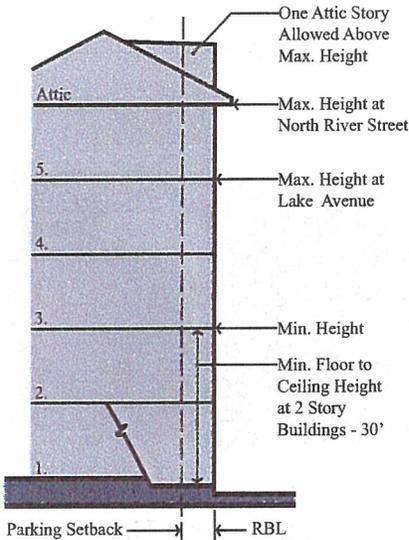




**BUILDING TYPE 'B' - SITING**

**a. Siting:**

Building FAÇADES fronting on Portside Drive and Corrigan Street shall be built to the REQUIRED BUILD-TO LINE (RBL) for at least 50% of the RBL length along those blocks. Building FAÇADES fronting on Lake Avenue, North River Street and the required CIVIC SQUARE shall be built to the RBL for at least 100% of the RBL length along those blocks. Exceptions to the RBL in compliance with §120-77.2.020A(4) are included within the minimum FAÇADE length;



**BUILDING TYPE 'B' - HEIGHT**

**b. Building Height:**

Minimum 2 stories;  
 2 story buildings shall have a minimum clear height from the GROUND STORY floor to the 2nd story ceiling of 30 feet;  
 Maximum 4 stories at Lake Avenue, 5 stories at North River Street; Up to two additional stories permitted at BLOCK CORNERS or center of building mass up to 25% of building footprint;

**c. Upper Story FENESTRATION:**

Primary FAÇADES and FAÇADES visible from any street or open space shall have areas of transparency equal to at least 25% but no more than 50% of the total FAÇADE area, with each FAÇADE area calculated independently;

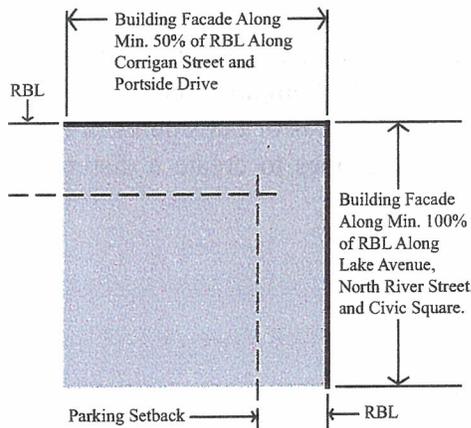
**d. Use:**

Non-residential uses are prohibited above residential uses except in buildings exceeding 3 stories in height, recreational facilities associated with the residential use may be located on the uppermost story.

**3. Type C, Tower Block**



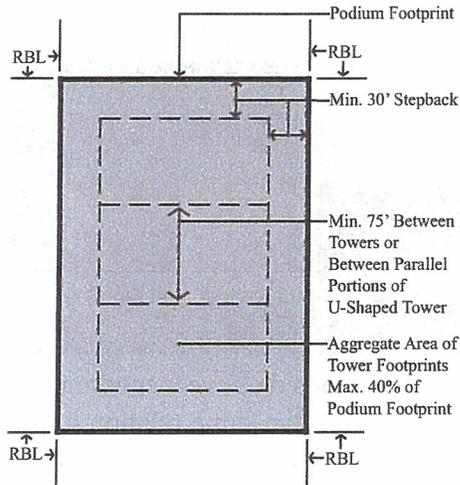
This Building Type allows taller buildings to be constructed above a ‘podium’ or base. The podium is similar to the Mid-Rise Block, with the same relation to the street. The tower above is required to be stepped back from the FAÇADE of the podium to maintain the scale of the buildings along the REQUIRED BUILD-TO LINE (RBL), increase the amount of sunlight reaching the sidewalk and minimize the apparent bulk of the tower from the street. This Building Type is permitted on PARCEL I.



**BUILDING TYPE 'C' - SITING**

**a. Podium Siting:**

Podium FAÇADES fronting on Portside Drive and Corrigan Street shall be built to the RBL for at least 50% of the RBL length along those blocks. Podium FAÇADES fronting on Lake Avenue, North River Street and the required CIVIC SQUARE shall be built to the RBL for at least 100% of the RBL length along those blocks. Exceptions to the RBL in compliance with §120-77.2.020A(4) are included within the minimum FAÇADE length;

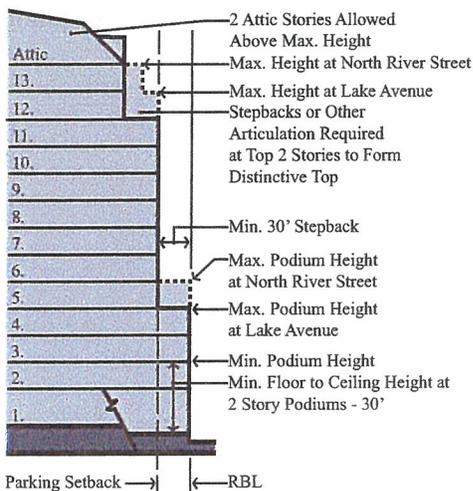


BUILDING TYPE 'C' - TOWER SITING

b. Tower Siting:

One or more towers may be constructed above a podium provided they comply with the following:

1. Towers shall be stepped back at least 30 feet from the RBL above the maximum height of the podium;
2. Tower FAÇADES shall be substantially parallel to the RBL;
3. The aggregate building footprint of the towers above the maximum height of the podium shall not exceed 40% of the building footprint of the podium;
4. Where there is more than one tower or where there is a U or H shaped tower building footprint, the minimum distance between parallel building walls shall be 75 feet;
5. The top two stories (not including stories contained within a pitched roof) shall employ step-backs, roof forms such as spires, cupolas or belvederes or other configurations or techniques to create a distinctive roof profile.



BUILDING TYPE 'C' - HEIGHT

c. Building Height:

1. Podium:
  - Minimum 2 stories;
  - 2 story podiums shall have a minimum clear height from the GROUND STORY floor to the 2nd story ceiling of 30 feet;
  - Maximum 4 stories at Lake Avenue, 5 stories at North River Street;
2. Overall Building Height:
  - Maximum 12 stories at Lake Avenue, 13 stories at North River Street; Up to two additional ATTIC STORIES permitted in a tower if contained within a single pitched roof with DORMERS;

d. Upper Story FENESTRATION:

Primary FAÇADES and FAÇADES visible from any street or open space shall have areas of transparency equal to at least 25% but no more than 50% of the total FAÇADE area, with each FAÇADE area calculated independently;

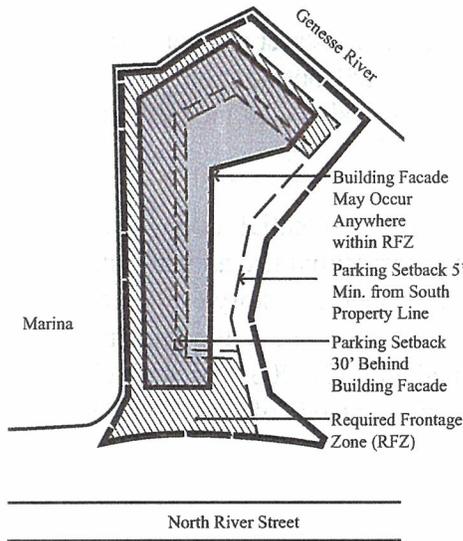
e. Use:

Non-residential uses are prohibited above residential uses except that recreational facilities associated with the residential use and/or restaurants may be located on the top two stories, the uppermost story or the first story above the podium and may utilize outdoor space above the podium. Bars, cocktail lounges and taverns as defined in §120-208 and nightclubs as defined in Section 202 of the Existing Building Code of New York State are prohibited from these locations.

**4. Type D, Edgeyard Building**



This Building Type is suited for those sites where it is less critical to the public realm that buildings be placed adjacent or close to the sidewalk. In this building type, a REQUIRED FRONTAGE ZONE (RFZ) is substituted for the REQUIRED BUILD-TO LINE (RBL). This allows greater flexibility in determining the building footprint and allows for the possibility of front and side yards. Building height is limited in order to preserve the visual relationship between the Charlotte Lighthouse and Lake Ontario. This Building Type is permitted on PARCEL III.



BUILDING TYPE 'D'- SITING

a. Siting:

The building FAÇADE of the first 2 stories shall occur entirely within the REQUIRED FRONTAGE ZONE (RFZ) indicated on the REGULATING PLAN with the following exceptions permitted: The building FAÇADE may include variations that recess behind the inside boundary or project beyond the outside boundary of the RFZ up to 24 inches;

Storefront assemblies (doors, display windows, bulkheads and associated framing) may be recessed behind the inside boundary of the RFZ or project beyond the outside boundary of the RFZ by up to 24 inches and storefront entrances may be recessed from the storefront assembly;

Projecting FAÇADE elements in compliance with §120-77.2.060F may project beyond the outside boundary of the RFZ;

b. Frontage Types:

Portions of the building facing North River Street and the public access along the marina and the Genesee River shall comply with §120-77.2.020B(1) (Commercial Frontages) and/or §120-77.2.020B(2) (Non-Commercial Frontages).

c. PARKING SETBACK LINE:

The PARKING SETBACK LINE shall be located 30 feet behind the building FAÇADE wall, except at the south property line, parking facilities shall be set back at least 5 feet;

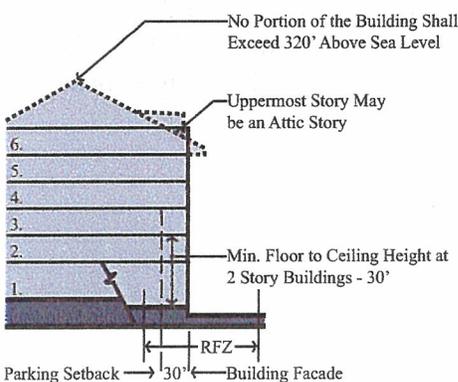
d. Building Height:

Minimum 2 stories;

2 story buildings shall have a minimum clear height from the GROUND STORY floor to the 2nd story ceiling of 30 feet; Maximum 6 stories;

The uppermost story of buildings more than two stories in height may be an ATTIC STORY;

No portion of the building shall exceed a height of 320 feet above sea level.



BUILDING TYPE 'D'- HEIGHT

e. Upper Story FENESTRATION:

Primary FAÇADES and FAÇADES visible from any street or open space shall have areas of transparency equal to at least 25% but no more than 50% of the total FACADE area, with each FAÇADE area calculated independently;

f. Use:

Non-residential uses are prohibited above residential uses except in buildings exceeding 3 stories in height, recreational facilities associated with the residential use may be located on the uppermost story.

**5. Existing Buildings**

The Terminal Building is the only existing building of significance within the Marina District. This section governs demolition, exterior alterations and additions to this structure.

a. Demolition:

Demolition of any portion of the Terminal Building except additions south of the original south building wall shall be prohibited;

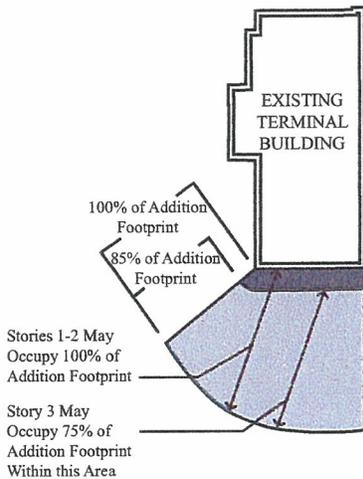
b. Exterior Alterations:

Significant architectural features including but not limited to original exterior wall finishes, the second story open veranda, the clock tower, decorative cornices, windows, doors, trim around openings, railings, storefronts and any significant decorative features shall be maintained;

c. Addition Siting:

An addition may be constructed within the area indicated on the REGULATING PLAN. The building FAÇADE of the first 2 stories shall occur entirely within the REQUIRED FRONTAGE ZONE (RFZ) indicated on the REGULATING PLAN with the following exceptions permitted:

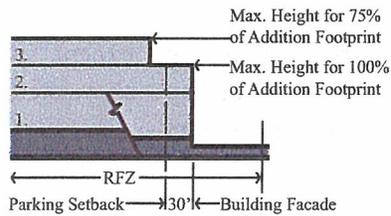
The building FAÇADE may include variations that project beyond the outside boundary of the RFZ up to 24 inches; Storefront assemblies (doors, display windows, bulkheads and associated framing) may project beyond the outside boundary of the RFZ by up to 24 inches; Awnings, canopies and GALLERIES may project beyond the outside boundary of the RFZ;



TERMINAL BUILDING ADDITION-HEIGHT

d. Addition PARKING SETBACK LINE:

The PARKING SETBACK LINE shall be located 30 feet behind the building FAÇADE wall of any addition;



TERMINAL BUILDING ADDITION-HEIGHT

e. Addition Height:

Maximum 2 stories for primary addition, 75% of building footprint may be up to 1 additional story provided this portion of the building occurs within the 85% of the building footprint farthest from the existing Terminal Building;

f. Upper Story FENESTRATION:

Primary FAÇADES and FAÇADES visible from any street or open space shall have areas of transparency equal to at least 25% but no more than 50% of the total FAÇADE area, with each FAÇADE area calculated independently;

g. The Architectural Standards in §120-77.2.060 are not applicable to the Terminal Building. The Director of Planning and Zoning shall refer all proposed exterior alterations and/or additions to the Project Review Committee for recommendations and shall approve or disapprove the proposals based on those recommendations.

.030

Building Function (Use)

**M-D Marina District**

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### **§120-77.2.030 Building Function (Use)**

**Intent.** This Section establishes permitted, specially permitted and prohibited uses within the Marina District. In order to maximize the liveliness, vitality and pedestrian character of the district, most uses are permitted as of right. Uses that are detrimental to the pedestrian character of the district are generally prohibited. The BUILDING ENVELOPE STANDARDS contain additional requirements pertaining to mixed uses within buildings.

#### **A. Permitted Uses**

All uses are permitted in fully enclosed buildings in the Marina District unless listed as specially permitted or prohibited uses in this section. In addition to any specific requirements listed below, uses shall be subject to the additional requirements for the specified uses set forth in Article XVIII of this chapter.

#### **B. Permitted Outdoor Uses**



1. Accessory outdoor seating areas without background music or entertainment;
2. Accessory display of merchandise during business hours;
3. Food vending carts and trucks may be located in interim parking facilities constructed in accordance with §120-77.2.050B, in the required CIVIC SQUARE and on the grounds of the Terminal Building;
4. Farmer markets may be located in interim parking facilities constructed in accordance with §120-77.2.050B, in the required CIVIC SQUARE and on the grounds of the Terminal Building;
5. Walk-up service windows;
6. Permanent parking constructed in accordance with §120-77.2.050A.

**C. Specially Permitted Uses**

The following uses are allowed as special permit uses in the Marina District:

1. Outdoor entertainment;
2. Interim Parking constructed in conformance with §120-77.2.050B and §120-13F. Special permits for Interim Parking shall be subject to renewal every five years. Each renewal application shall be subject to a marketability analysis demonstrating that the site cannot be developed based on the following standards:
  - a. The site is not marketable for development in accordance with the intent of the Marina District as demonstrated by at least one of the following factors:
    1. The inability to find an interested developer or buyer over an extended period of time;
    2. Physical location or locational limitations or deficiencies of the site or public infrastructure.
  - b. Standards indicating the type of information required to document conformance with each of the aforementioned standards shall be as adopted from time to time by the Planning Commission.

**D. Prohibited Uses**

The following uses are prohibited in the Marina District:

1. Homeless shelters;
2. Sexually oriented businesses;
3. Uses not in a fully enclosed building or not permitted by §120-77.2.030B;
4. Any use that would meet the definition of a manufacturing use as per §120-208 of the Zoning Code;
5. Drive-throughs;

6. Vehicle - related uses, including car washes, vehicle service stations, vehicle sales, vehicle repair including commercial vehicle repair and vehicle rental services, except vehicle rental or sharing services in which vehicles are stored in permanent parking facilities constructed in accordance with §120-77.2.050A, vehicle service takes place outside the Marina District and offices are located in a fully enclosed building;

7. Funeral homes;

8. Places of worship;

9. Warehouses;

10. Recycling centers.

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.040

Civic Square Standards

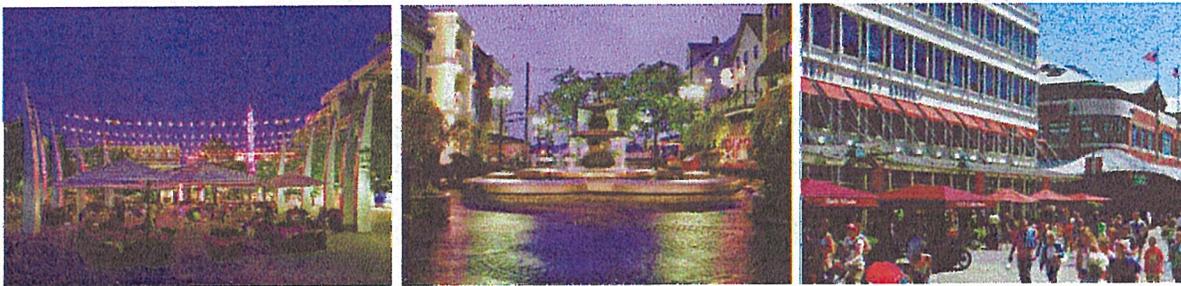
**M-D Marina District**

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**§120-77.2.040 Civic Square Standards**



**Intent.** These standards are applicable to the required CIVIC SQUARE indicated on the REGULATING PLAN on PARCEL I. The square will be the principle pedestrian connection between Lake Avenue and River Street and the marina. It will be an active pedestrian center and a major focal point within the Marina District. In order to facilitate pedestrian activity, most of the square will have paved surfaces. The use of pervious paving materials to allow oxygen for tree roots and to absorb storm water run-off is encouraged.



**A. Location and Dimensions**

The CIVIC SQUARE shall extend from the Lake Avenue public right-of-way to the River Street public right-of-way as shown on the REGULATING PLAN subject to the following requirements:

1. Minimum 150 feet south of the REQUIRED BUILD-TO LINE (RBL) fronting on Corrigan Street and 150 feet north of the RBL fronting on Portside Drive;
2. Width: 80 feet minimum, 120 feet maximum. The width of the square may vary within these parameters.

**B. Pavements**

At least 65% of the surface area of the CIVIC SQUARE shall be paved. Any type of unit pavers or concrete pavement, excluding stamped concrete, is permitted. Asphalt paving, excluding hexagonal asphalt pavers, is prohibited.

**C. Trees**

The use of trees to shade portions of the CIVIC SQUARE is encouraged. Trees shall be of deciduous species.

**D. Clear Views**

To maximize views, and to ensure public safety, there shall be a clear view zone between two and eight feet above grade. Tree trunks, street lights, kiosks, fountains, public art or monuments are permitted to be within the clear view zone. The foliage of newly planted trees may be within the clear view zone until the tree has sufficient growth to allow the removal of branches below eight feet.

**E. Slope**

The CIVIC SQUARE shall be designed in compliance with the most current ADA Standards for Accessible Design.

.050

Parking & Loading Standards  
**M-D Marina District**

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**§120-77.2.050 Parking and Loading Standards**

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**Intent.** This Section establishes standards for the placement of and access to permanent vehicular parking facilities associated with buildings and interim vehicular parking that may be located on PARCELS I, II and III until those PARCELS are developed. It also establishes standards for short and long term bicycle parking and loading areas.

**A. Permanent Parking Facilities**

1. All parking provided for users of a particular building shall be provided on the site of that building.
2. Permanent parking facilities, except for those that are completely below grade, shall be located no closer to the public right-of-way than the Required PARKING SETBACK LINE indicated on the REGULATING PLAN.
3. A maximum of one ingress and one egress lane to permanent parking facilities are permitted within each allowable area designated on the REGULATING PLAN. Ingress/Egress driveways shall be located at least 50 feet from BLOCK CORNERS. Combined In/Out driveways shall have a maximum width of 22 feet. Separate In and Out driveways shall have a maximum width of 11 feet each.
4. Openings in any Building FAÇADE for parking garage entries shall have a maximum clear height no greater than 12 feet and a clear width no greater than 12 feet for single width openings or 24 feet for double width openings;
5. The height of parking structures shall not exceed the height of adjacent liner buildings constructed between the parking facility and the REQUIRED BUILD-TO LINE. There is no minimum height for parking structures.
6. Permanent parking facilities are not required to comply with §120-173F.
7. A minimum of 15% of parking spaces provided for building residents shall be provided with facilities capable of recharging the batteries of electric and plug-in hybrid vehicles. All parking spaces provided for residents shall be capable of having recharging facilities added in the future.

**B. Interim Parking Facilities**

1. Paved parking facilities may be specially permitted to be temporarily located on undeveloped portions of PARCELS I, II and III until such time as these PARCELS are developed. Interim parking facilities shall be constructed in accordance with this section and with §120-173F. Such parking facilities shall be available for public use.

2. Interim parking facilities shall be set back at least 15 feet from the public right-of-way.
3. Ingress and egress to interim parking facilities shall be located within the allowable areas designated on the REGULATING PLAN where possible. Where the allowable access areas are not available, ingress and egress locations shall be approved by the Director of Planning and Zoning. Combined In/Out driveways shall have a maximum width of 22 feet. Separate In and Out driveways shall have a maximum width of 11 feet each.
4. Parking areas shall be drained in accordance with §40-54B. This requirement may be waived if the parking area is paved with permeable pavements and/or if bio-swales, rain gardens or similar alternative drainage techniques are utilized, provided it is shown that these techniques will provide equivalent or better performance.
5. The parking setback area shall be provided with minimal landscape treatment such as grasses or ground cover. Mulch is not an acceptable landscape treatment except when used to protect plantings. Trees are not required in the parking setback.

### C. Bicycle Parking

1. The Marina District is located at the northern terminus of the Genesee River trail system. This location and the need to provide for and encourage non-motorized transportation make the provision of facilities for bicyclists a high priority. A significant amount of bicycle parking will be provided within public rights-of-way. This section sets forth minimum requirements for short term and long term bicycle parking on private development PARCELS.
2. Short term bicycle parking shall be provided in the required CIVIC SQUARE and on the grounds of the Terminal Building. Short term bicycle parking may be provided in interim parking facilities constructed in accordance with §120-77.2.050B and in other exterior locations in PARCELS I, II and III. Short term bicycle parking facilities shall comply with the following:
  - a. Each bicycle parking space shall be accessible without moving another bicycle and its placement shall not result in a bicycle obstructing a required walkway;
  - b. Bicycle racks may be installed in the public right-of-way subject to the approval of the Department of Environmental Services;
  - c. Each bicycle parking space shall permit the locking of the bicycle frame and one wheel to the rack and shall support the bicycle in a stable position without damage to the wheels, frame or components;

- d. A minimum of 10 spaces shall be provided in the required CIVIC SQUARE;
  - e. A minimum of 2 spaces plus 1 additional space for each 5000 square feet of retail space, 10,000 square feet of assembly space or 15,000 square feet of office space shall be provided on the Terminal Building grounds in conjunction with any new development outside of the existing terminal building.
3. Long term bicycle parking facilities for residents in buildings with residential dwelling units shall be provided in compliance with the following:
- a. Facilities shall be enclosed, secure and protected from the weather;
  - b. Facilities shall be located on the GROUND STORY, between the REQUIRED BUILD-TO LINE (RBL) or REQUIRED FRONTAGE ZONE (RFZ) and the PARKING SETBACK LINE or behind the PARKING SETBACK LINE, and have direct access to a public right-of-way;
  - c. Facilities shall be internally connected to the residential dwelling units;
  - d. Each bicycle parking space shall be accessible without moving another bicycle and its placement shall not result in a bicycle obstructing a required walkway;
  - e. Each bicycle parking space shall permit the locking of the bicycle frame and one wheel to the rack and shall support the bicycle in a stable position without damage to the wheels, frame or components;
  - f. A minimum of 1 space for every 5 bedrooms shall be provided.

#### **D. Loading Facilities**

1. Access to loading areas, including dumpsters, shall be located within the allowable areas designated on the REGULATING PLAN. Except when located within a fully enclosed building, loading docks and dumpsters shall be located no closer to the public right-of-way than the Required PARKING SETBACK LINE indicated on the REGULATING PLAN. All loading docks and dumpsters shall be set back a sufficient distance from the public right-of-way to allow service vehicles to park entirely behind the REQUIRED BUILD-TO LINE.
2. Driveways shall have a maximum width of 12 feet.

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# .060

Architectural Standards

**M-D Marina District**

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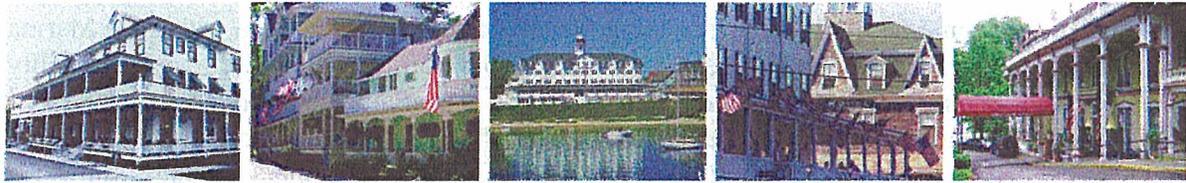
**§120-77.2.060 Architectural Standards and Guidelines**

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**Intent.** This Section establishes standards and guidelines for new construction within the Marina District to ensure that new development establishes a minimum level of architectural quality and positively contributes to the public realm. No particular architectural style is mandated or prohibited; rather the Architectural Standards and Guidelines are intended to promote harmony and help the district coalesce into a unified place. This section includes both mandatory standards and advisory guidelines. The advisory guidelines are intended to provide insight into some of the desired characteristics of the district, while the standards set the minimum requirements for architectural quality. The mandatory standards are applicable to building elements that are clearly visible from the street or any civic space. This includes all public streets and civic spaces within the Marina District and adjoining the Marina District, but does not include parking areas located in accordance with §120-77.2.050. *Advisory guidelines are printed in italics.* Mandatory standards are printed in normal font. The Architectural Standards and Guidelines are organized as follows:

- A. Context and Architectural Character
- B. Composition and Articulation
- C. Building FAÇADE Walls
- D. Wall Openings
- E. Roofs
- F. Projecting FAÇADE Elements
- G. Signage

**A. Context and Architectural Character**



*Where there is little context to relate to, the proper response is to create a new and worthy context. To achieve this goal, new buildings should establish an architectural character and pattern from which future development can take its cues. The creation of a unified, harmonious district is more important than the individual building. To this end, an “architecture of place” is preferred over an “architecture of our time.” Charlotte’s past as a resort community included fantastical amusement park structures and hotels, which helped make Charlotte a distinctive and memorable place. Charlotte’s lost historic buildings are part of a living tradition, which included buildings that spanned the vernacular to classical spectrum, from simple wood frame structures to elaborate masonry and stucco buildings rendered in exotic architectural styles. The standards and guidelines that follow are largely based upon principles that underlie this living tradition. The intent is not to replicate historic buildings, but to encourage a range of architectural expressions that will once again establish Charlotte and the Marina District as a distinctive and memorable place.*

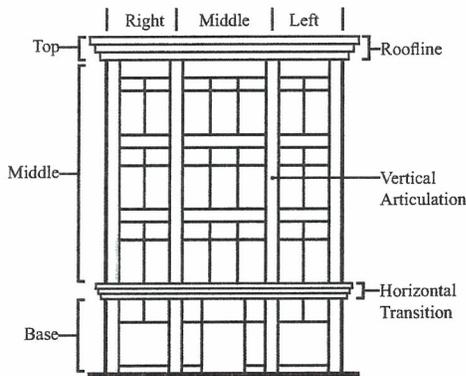


**B. Composition and Articulation**

*It is likely that most of the buildings in the Marina District will be constructed at a larger scale, both horizontally and vertically, than any previous buildings in Charlotte. Large structures can be monolithic, monotonous, and lacking in human scale. This can be avoided through principles of composition and proportion.*

- *Each composition should be conceived as a whole assembled from composite parts, which in turn are wholes composed of smaller parts;*
- *Each building mass and FAÇADE composition should have a strong focus or center. The focus need not be at the geometric center of the composition and the composition need not be symmetrical;*
- *Each composition should have a readily definable base, middle and top and left, middle and right;*
- *Parts of the composition should be joined by transitional elements that both separate and link adjacent parts;*
- *Some elements of the composition should be dominant, while others are subordinate. This establishes a hierarchy, which is a defense against the monotony of simple repetition.*

**1. Horizontal Transitions**



BUILDING FACADES

Building FAÇADES shall have Horizontal Transitions. A Horizontal Transition is an architectural element, such as a cornice, BALCONY, GALLERY or change in material that creates a distinction between the first and second stories or between the second and third stories. Horizontal Transitions are not required in buildings with Non-Commercial frontages.

**2. Roof Lines**

Building FAÇADES shall have Roof Lines. A Roof Line is an architectural element, such as a cornice, parapet or change in material, which creates a distinction between the top of the building and the lower stories.

- *Horizontal transitions and roof lines are the means to achieve a distinctive horizontal base; occupied middle; and top that complement and balance one another.*

### 3. Vertical Articulations

Building FAÇADES shall have Vertical Articulations. Vertical articulations can be produced by variations in roof lines; window groupings; applied FAÇADE elements such as piers or pilasters, BAY WINDOWS and BALCONIES; entrance stoops and porches; and subtle changes in materials and vertical planes that create shadow lines and textural differences.

- *Each building should have a clear and harmonious pattern of vertically-oriented FAÇADE elements that break up long monolithic walls and divide the composition into one or more segments, each with a discernible left, middle and right. A balance of vertical and horizontal lines should be sought, with the balance tipped slightly to the vertical;*
- *Changes in vertical planes should be part of a larger FAÇADE composition and should not be used to present a false image of individual buildings. Where separate buildings abut, the principle planes of the FAÇADES should be flush.*

**C. Building Façade Walls****1. Finish Materials**

Each building FAÇADE may have two or more finish materials. STREET WALLS shall be constructed of a material matching the adjacent building FAÇADE.

- *Materials should be separated horizontally, with “lighter” materials placed above more substantial materials, for example wood above stucco or masonry, or stucco above masonry.*

**2. Projecting Elements**

All elements that project from the building by more than 16 inches, such as BALCONIES, BAY WINDOWS, canopies and marquees shall be visibly supported from below or above by brackets, posts, columns, pilasters or similar supports that are sized proportionately to the projecting structure.

**3. Primary Materials**

The following primary materials are permitted:

- a. Masonry, including brick, stone, terra cotta, ceramic tile or similar facings;
- b. Cementitious stucco with smooth or sand finish;
- c. Fiber cement panels, siding and trim boards. Wood grain finishes are not permitted (Building Types ‘A’ and ‘B’ only);
- d. Solid PVC paintable or prefinished siding and trim boards (such as NuCedar Mills or equivalent). Wood grain finishes are not permitted (Building Types ‘A’ and ‘B’ only);
- e. Painted wood clapboards or painted, stained or natural shingles. (Buildings Types ‘A’ and ‘B’ only).

**4. Secondary Materials**

The following secondary materials are permitted for up to 10% of a building wall surface:

- a. Pre-cast masonry (for lintels, trim and cornices only);

- b. Exterior Insulation Finish System (EIFS) with smooth or sand finish. (EIFS may be utilized above the Horizontal Transition only);
- c. Metal (for beams, lintels, trim elements and ornamentation only);
- d. Polyurethane (such as Fypon or equivalent) Millwork (for lintels, trim elements and ornamentation only);
- e. Glazed, ground face or split face concrete block (used as accent trim, piers and foundation walls only);
- f. Glass block.
  - *The palette of wall materials should be kept to a minimum, preferably two (e.g. stucco and tile, brick and stone) or less. Using the same wall materials as adjacent or nearby buildings helps strengthen the district character;*
  - *Stucco and/or painted stucco surfaces should be smooth to prevent the collection of dirt and surface pollutants, and the deterioration of painted surfaces;*
  - *Sheet metal parapet cap flashings should be painted to match wall or trim color;*
  - *The following materials are not appropriate:*
    - *Curtain Wall systems except in limited areas such as connections; between buildings, entrance lobbies, etc;*
    - *Simulated finishes such as artificial stone;*
    - *Plywood siding.*

**D. Wall Openings****1. Opening Proportions**

a. Windows, doors and other openings (except at GROUND STORY commercial frontages) shall be square or vertical in proportion. Except at BAY WINDOWS, window frames shall be recessed at least 3 inches from the plane of masonry or stucco building FAÇADES;

b. Windows may be ganged horizontally if each grouping is separated by a framing element at least seven inches wide.

- *The width to height ratio of windows, doors and similar elements should typically be 1:2 or 2:3.*

**2. Permitted Finish Materials:**

a. Windows and doors may be of steel; aluminum; including clear anodized or factory finished colors; fiberglass; aluminum clad wood; painted wood; stained or natural (clear finish) wood;

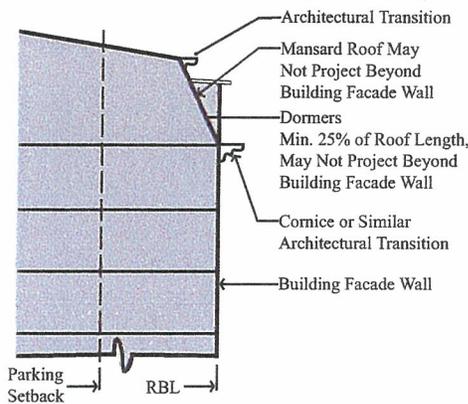
b. Glazing shall be clear or lightly tinted;

c. If muntins are provided, they shall be of the true divided or simulated divided (applied to interior and exterior with spacer bar within the glazing) types.

**E. Roofs****1. Permitted Configuration**

- a. Flat, (pitched as required for drainage) concealed by a parapet no less than 42 inches high or as required to conceal mechanical equipment. The parapet may have pitched, stepped or curved forms that help create a roof line;
  - b. Gabled, symmetrically sloped, with a minimum pitch of 5:12, with eaves and/or partially concealed by a parapet;
  - c. Hipped, symmetrically sloped, with a minimum pitch of 5:12, with eaves and/or partially concealed by a parapet;
  - d. Shed, with a minimum pitch of 2:12 where attached to a larger building mass, with eaves and/or partially concealed by a parapet;
  - e. Barrel Vaulted;
  - f. Domed
- *Roof forms should complement the building mass and match the principal building in terms of style, detailing and materials. Double-pitched roofs (such as gable, hip, pyramid), dormer windows, and chimneys can add variety and visual interest when viewed from the streets below and from a distance. Flat roofs are acceptable if a strong, attractively detailed cornice and/or parapet wall is provided. Single-pitched or “shed” roofs should not be used for the principal building. Where the BUILDING ENVELOPE STANDARDS require “a distinctive roof profile” (Building Type ‘C’), additional roof forms may be considered.*

**2. Mansard Roofs**



BUILDING FACADES

a. Mansard roofs are a type of pitched roof that are considered ATTIC STORIES and therefore not counted against the maximum story height. Mansards may be added only to buildings that are at least two stories in height or to one story Type ‘A’ buildings. They shall enclose at least one, but no more than two ATTIC STORIES of habitable space as indicated in the BUILDING ENVELOPE STANDARDS;

b. Only dormer windows are permitted in mansard roofs. Windows, balconies and other features that are recessed into the mansard are prohibited. Dormers and other architectural features shall occupy a minimum of 25% of the roof length;

c. A cornice or similar architectural feature shall form a transition between the mansard and the vertical building wall. No part of the mansard, including dormers, shall project beyond the building FAÇADE wall;

d. A transitional architectural feature shall be provided between the top of the mansard and the upper part of the roof.

- *Mansards should be used only when emulating a traditional building style that typically employs mansard roofs, e.g. Beaux Arts, Victorian, etc.*

**3. Skylights**

Skylights shall be flat to the pitch of the roof.

**4. Permitted (visible) Roofing Materials**

- Standing Seam or Five Vee metal roofs of galvanized steel, copper, aluminum or zinc-aluminum;
- Asphalt or metal “dimensional” type shingles;
- Cedar shakes (real or synthetic);

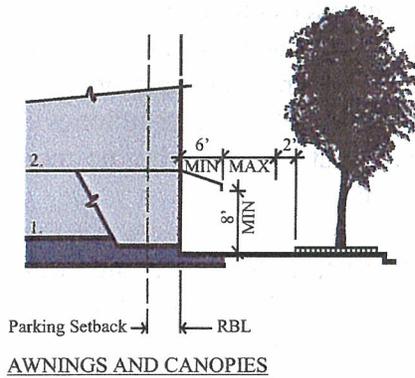
- d. Slate tiles (real or synthetic)
- e. Clay, terra cotta or concrete tiles;
- f. 'Green' roofs on flat roofs only;
- g. Roofing materials including shingles and metal panels that incorporate photovoltaic solar collectors.

**F. Projecting Façade Elements**

**1. Awnings and Canopies**



Where awnings or canopies are provided, they shall comply with the following:



- a. Minimum 8 feet clear above sidewalk, minimum 6 feet projection from building, maximum projection 2 feet from tree planters or grates, or 2 feet from curb line where no trees are present;
- b. Where encroaching into a public right-of-way, subject to Department of Environmental Services approval;
- c. Canvas cloth or equivalent (shiny or reflective materials are prohibited), metal or glass;
- d. Internal illumination is prohibited;
- e. One-quarter cylinder configurations are prohibited.

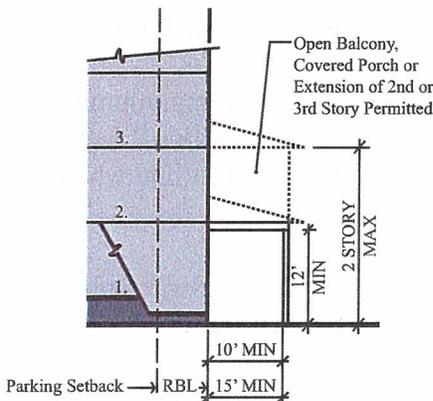


**2. Galleries**



GALLERIES are permitted along building FAÇADES where the REQUIRED BUILD-TO LINE (RBL) or REQUIRED FRONTAGE ZONE (RFZ) abuts the required CIVIC SQUARE or does not abut a public right-of-way, provided they comply with the following:

a. Minimum width of 10 feet;



GALLERIES

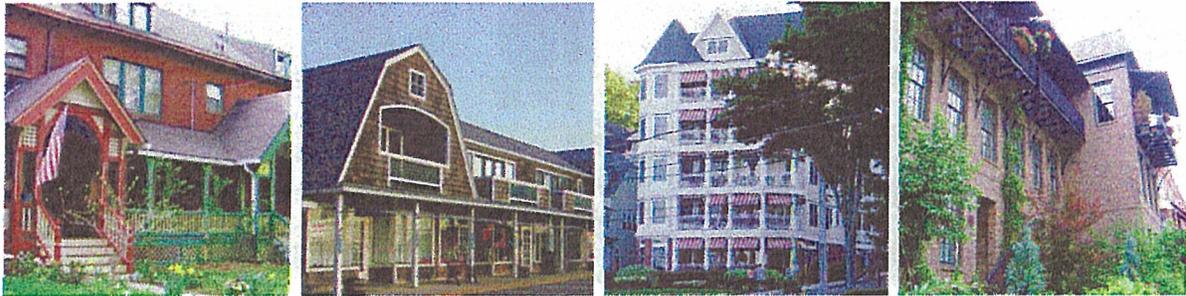
b. GALLERIES shall have a roof, an open BALCONY, a covered porch or an extension of the story above. Where there is a covered porch or an extension of an upper story, the building shall have at least one additional story above that level. Where there is an extension of an upper story, the FAÇADE of the story extension shall have fenestration with areas of transparency equal to at least 50% but no more than 90% of the FAÇADE area;

c. Minimum ceiling height 12 feet clear above sidewalk, but never less than one foot below ceiling height of adjacent GROUND STORY, 2 story maximum height;

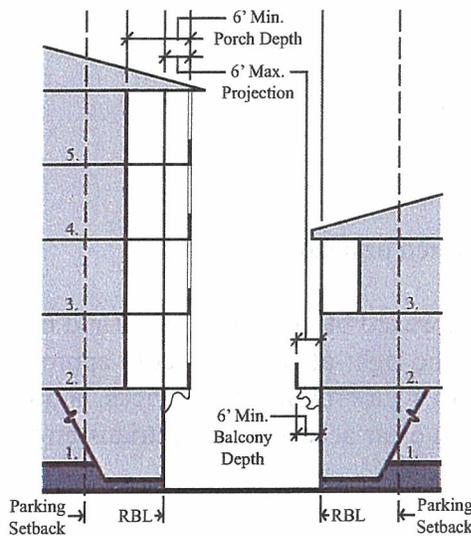
d. The distance between columns or piers shall not exceed their height. Minimum spacing is 10 feet.

- *The location of columns or piers is encouraged to correspond with storefront openings.*

**3. Porches and Balconies**



Porches and BALCONIES shall comply with the following:



PORCHES AND BALCONIES

a. Porches and BALCONIES may occur at upper stories either forward of or behind the RBL or RFZ and shall have a minimum depth of 6 feet, but shall not extend more than 6 feet beyond the RBL except at the upper level of a GALLERY;

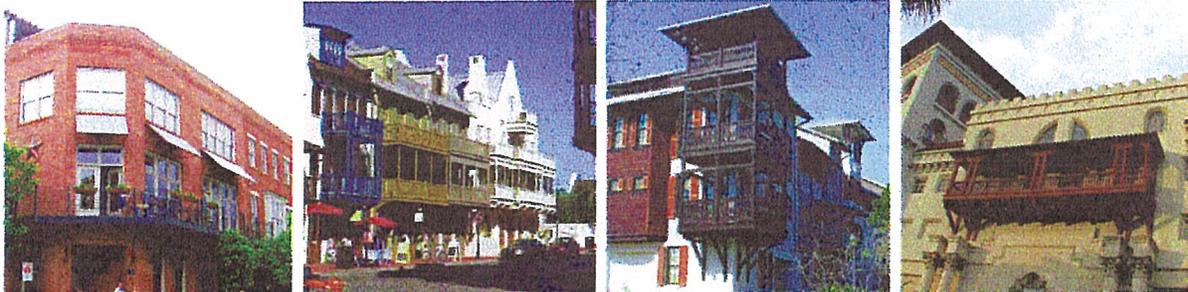
b. Porches may occur at the GROUND STORY along non-commercial frontages, either forward of or behind the RBL or RFZ and shall have a minimum depth of 8 feet, but shall not extend into the public right-of-way;

c. Porches and BALCONIES may have multi-story verandas and/or BALCONIES above;

d. Porches and BALCONIES may have roofs, but shall be open parts of buildings with no conditioned air supply. Insect screening is permitted.

e. The minimum distance between columns is 10 feet, and the distance between columns shall not exceed their height.

- *Porches and BALCONIES are encouraged at upper story locations in order to take advantage of views and breezes.*

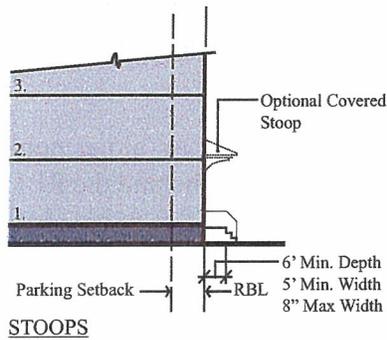


**4. Stoops**



Stoops are permitted along Non-Commercial frontages provided they comply with the following:

- a. Minimum depth 6 feet;
- b. Minimum length 5 feet;
- c. Maximum length 8 feet;
- d. Stoops may be covered or uncovered;
- e. Stoops may occur forward of the RBL or RFZ and may encroach into the public right-of-way subject to approval from the Department of Environmental Services. Stoops shall not interfere with clear access for pedestrians on the sidewalk.



**5. Finish Materials**

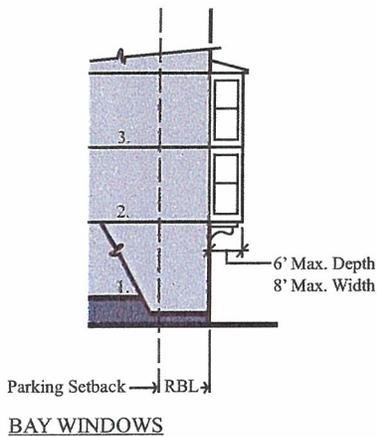
The following finish materials are permitted for porches, stoops, columns, arches, railings and balustrades:

- a. Painted finish wood;
- b. Solid PVC paintable or prefinished trim boards (such as Azek or equivalent). Wood grain finishes are not permitted;
- c. Painted or stainless steel;
- d. Cast iron;
- e. Concrete with smooth finish;
- f. Brick or stone masonry.

**6. Bay Windows**



BAY WINDOWS are permitted provided they comply with the following:



- a. Maximum depth 6 feet;
- b. Maximum length 8 feet;
- c. BAY WINDOWS shall have FENESTRATION on both front and side surfaces;
- d. BAY WINDOWS may occur forward of the RBL or RFZ and may encroach into the public right-of-way above the GROUND STORY subject to approval from the Department of Environmental Services.

**G. Signage**

**1. Alternate Sign Program**

No signs shall be approved within the Marina District unless an alternative sign program in compliance with §120-177K has been established for the building where the proposed sign will be located.

# .070

Review & Approval Process

**M-D Marina District**

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**§120-77.2.070 Review and Approval Process**

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A. Construction of any new building or addition within the Marina District that fully complies with the requirements of this Section will be approved administratively. Construction of any new building or addition that does not comply with the requirements of this Section shall be categorized as a major site plan and subject to the requirements set forth in §120-191 of this chapter. The Director of Planning and Zoning may waive the requirements of the Marina District through the site plan approval process upon a determination that a project is in substantial compliance with the overall intent and purpose of the Marina District except for the following:

1. Any deviation from the REGULATING PLAN including location of the REQUIRED BUILD-TO LINE (RBL), location of REQUIRED FRONTAGE ZONE, location of PARKING SETBACK LINE, location of allowable areas for parking/loading ingress/egress, location of mandatory commercial frontage and location of permitted building types;
2. Any deviation from the minimum or maximum dimensional requirements of the BUILDING ENVELOPE STANDARDS, or the maximum building height requirements measured in stories, that is greater than 20%;
3. Any deviation from the Siting requirements of the BUILDING ENVELOPE STANDARDS other than deviations from the percentage of the FAÇADE built to the REQUIRED BUILD-TO LINE (RBL) that are greater than 20% and deviations from the dimensional requirements for tower siting in Building Type C that are greater than 20%;
4. Any deviation from the Building Function (Use) Standards;
5. Any deviation from minimum or maximum dimensional requirements of the CIVIC SQUARE Standards that is greater than 20%;
6. Any deviation from the minimum or maximum dimensional requirements and numerical requirements of the Parking and Loading Standards that is greater than 10%;
7. Any deviation from the Architectural Standards requirement for an alternative sign program.

**B.** Any development proposal that will cause the total number of dwelling units in the Marina District to exceed 430 and/or cause the total amount of commercial space in the District, not including existing commercial space in the Terminal Building, to exceed 44,000 square feet, shall require a Supplemental Environmental Impact Statement pursuant to the requirements of the State Environmental Quality Review Act and Chapter 48 of the City Code.

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# .080

Definitions

**M-D Marina District**

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**§120-77.2.080 Definitions**

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**Attic Story.** Habitable space situated within the structure of a pitched roof and above the uppermost story. They are permitted for all Building Types and do not count against the maximum story height. Attic stories may have only DORMERS as windows on the REQUIRED BUILD-TO LINE (RBL) side of the roof-pitch.

**Balcony.** An exterior platform attached to the upper stories of the building FAÇADE.

**Bay Window.** An enclosure extending the interior space of the building beyond the exterior building FAÇADE. For the purposes of this code, a BAY WINDOW may be generally u-shaped, curved (also known as a bow window) or rectangular.

**Block Corner.** The outside corner of a block at the intersection of any two streets. Dimensions from BLOCK CORNERS are measured from the intersection of two RBL's.

**Building Envelope Standards.** The part of this Section that establishes basic parameters regulating building form, including the envelope (in three dimensions), placement and certain permitted/required building elements, such as storefronts, BALCONIES, and STREET WALLS. The BUILDING ENVELOPE STANDARDS establish both parameters and specific requirements. The applicable building envelope standard for a site is determined by the allowable building types indicated on the REGULATING PLAN. This produces a coherent street-space and allows the building owner greater latitude behind its FAÇADE.

**Civic Square.** A public open space designated on the REGULATING PLAN. The term square is generally used to describe spaces that have a predominately paved surface area. CIVIC SQUARES do not include active recreation structures such as playgrounds or game courts.

**Dormers.** Roofed ancillary structures with windows providing light and air to habitable space within the ATTIC STORY. DORMERS are permitted and the attic does not constitute a story (for height measurement purposes) so long as the DORMERS do not break the primary eave line, are individually less than 15 feet wide, and are collectively not more than 60 percent of their REQUIRED BUILD-TO LINE FAÇADE length.

**Façade (Building Face).** The building elevation facing the public right-of-way or required CIVIC SQUARE, generally coinciding with the REQUIRED BUILD-TO LINE or required step backs. Building walls facing private interior spaces are not FAÇADES.

**Fenestration.** Openings in the building wall, including windows and doors, allowing light and views between interior (private realm) and exterior (public realm). FENESTRATION is measured as glass area (including mullions and similar window frame elements) and/or as open area.

**Gallery.** A Lightweight roof structure or BALCONY supported by columns and attached to the building frontage to provide shelter to the sidewalk.

**Ground Story.** The first habitable level of a building at or above grade. For commercial frontages, at least two-thirds of the finished floor elevation within 30 feet of the required building line shall be within 18 inches of the adjacent fronting sidewalk level. When a residential use occupies the GROUND STORY, the finished floor shall be at least three feet, but never more than six feet above the fronting sidewalk elevation. The next story above the GROUND STORY is the second story.

**Parcel.** Where this term is used in §120-77.2, it refers exclusively to PARCELS I, II and III as indicated on the REGULATING PLAN. PARCELS may be subdivided into separate building lots in accordance with Chapter 128. Subdivisions do not affect any provisions of this section relating to PARCELS.

**Parking Setback Line.** A line or plane indicated on the REGULATING PLAN which extends vertically (unless otherwise noted) and is generally parallel to the REQUIRED BUILD-TO LINE (RBL) or REQUIRED FRONTAGE ZONE (RFZ). The parking setback is typically 30 feet behind the RBL, or the actual FAÇADE of the building where there is an RFZ, unless otherwise designated on the REGULATING PLAN. All parking shall be situated behind this line, except where it is entirely below grade. The PARKING SETBACK LINE is a permissive minimum and parking may be placed anywhere within the lot behind this line.

**Regulating Plan.** The implementing plan for the development of the Marina District. The REGULATING PLAN indicates the allowable building types, allocates space for the required CIVIC SQUARE and provides specific information for vehicular access to each PARCEL.

**Required Build-To Line (RBL).** A line or vertical plane indicated on the REGULATING PLAN, defining the street frontage which extends vertically and generally parallel to the street, at which the building shall be placed. The FAÇADE shall occur on the REQUIRED BUILD-TO LINE - this is a requirement, not a permissive minimum. The minimum length and height of frontage that is required at the RBL is shown on the appropriate Building Envelope Standard.

**Required Frontage Zone (RFZ).** A horizontal plane indicated on the REGULATING PLAN, defining the area within which the building FAÇADE must be placed. The FAÇADE shall occur within the REQUIRED FRONTAGE ZONE – this is a requirement that allows for a greater range of options on those PARCELS where the RFZ is instituted in place of the RBL. These are PARCELS where definition and enclosure of the street space are of lesser importance, therefore wider latitude is given.

**Street Wall.** A masonry wall set back not more than 24 inches from the required building line which assists in the definition of the street-space in the absence of a building. See the General Provisions of the BUILDING ENVELOPE STANDARDS for height specifications.

**STATE ENVIRONMENTAL QUALITY REVIEW  
(SEQR)**

**FINAL  
SITE SPECIFIC/GENERIC ENVIRONMENTAL IMPACT  
STATEMENT  
FEIS**

**Proposed Action:  
City of Rochester Port Public Marina and Mixed Use Development Project**

**SEQR Environmental Impact Statement  
NEPA Environmental Assessment**

**LEAD AGENCY:  
Thomas S. Richards  
Mayor, City of Rochester  
City Hall, Room 307A, 30 Church Street  
Rochester, NY 14614-1290**

**Appendix D**

**Marina Operations and Maintenance Plan**

## **Port of Rochester Marina Operations and Maintenance Plan**

### **1. Operations**

#### 1.1 General

##### 1.1.1 NOAA Clean Marinas Program

As a general objective, the Port of Rochester Marina will adopt the measures outlined within the Clean Marina Program to prevent and reduce pollution from the marina and the recreational boating community, protect habitat, enhance public image, save money, and create an aesthetically pleasing facility. Upon completion, the Port of Rochester Marina will apply to be recognized as a clean marina.

#### 1.2 Staffing & Responsibilities

##### 1.2.1 Manager

The Marina Manager is a seasonal position, being full-time for approximately 4-6 months and part-time for approximately 6-8 months. Responsibilities include ensuring that day to day operations of the marina comply with the Marina Operations and Maintenance Plan. The Marina Manager will also serve as the primary contact person and the primary enforcer of marina rules & regulations.

##### 1.2.2 Seasonal/Part-Time Personnel

Seasonal/part-time personnel will be hired to provide assistance during the peak boating season. Seasonal/part-time personnel will serve as "concierge" service for boaters while performing tasks such as billing, collection, coordination of transient slips, marina cleaning, debris removal, monitoring circulation tubes, assisting boaters with the sanitary pumpout, providing general assistance, and updating boaters with local information such as weather and fishing.

#### 1.3 Personnel Orientation

The Marina Manager will specify a schedule and requirements for orientation of personnel.

#### 1.4 Use and Operation of Marina Equipment

1.4.1 Marina equipment (i.e. pumpout, dock utilities) shall be used only after permission has been received from the marina manager.

#### 1.5 Berth and Mooring

##### 1.5.1 Slip Tracking

An electronic system is planned for tracking transient slip reservations and availability. The Marina Manager will be responsible for berth allocation and the system will be linked to surrounding ports and harbors, if possible.

##### 1.5.2 Billing/Collection

The marina manager will oversee billing/collection.

#### 1.6 Hours of Operation by Season

During the peak boating season (Memorial Day – Labor Day), the marina hours will likely be 8am – 6pm and during off-peak boating season, the marina hours will likely be 8am – 5pm for Monday thru Friday and 8am-12pm for Saturday/Sunday.

#### 1.7 Parking and Loading

There are over 800 public parking spaces are located within 900' of the marina which are used by beach-goers, tourists, and event attendees and which are intended for boaters, as well. Some parking spots may be linked to surrounding residential developments which will provide private parking as part of the development.

#### 1.8 Security and Surveillance

Most slips will be isolated from the public promenade by architectural security gates and ramps to the docks. Surveillance may be added at these security points, if needed.

#### 1.9 Marina User Regulations

Marina user regulations will be developed by the City of Rochester and the Marina Manager upon marina completion to address the following non-inclusive list:

- 1.9.1 Vessel Identification
- 1.9.2 Compliance with Applicable Laws
- 1.9.3 Insurance
- 1.9.4 Dock Use
- 1.9.5 Living Aboard
- 1.9.6 Operation of the Boat
- 1.9.7 Exchange or Subdivision of Berths
- 1.9.8 Boat Tie Up
- 1.9.9 Safety of Children and Guests
- 1.9.10 Fire Prevention
- 1.9.11 Electrical Safety
- 1.9.12 Sanitary Facilities
- 1.9.13 Garbage and Trash
- 1.9.14 Waste Oil
- 1.9.15 Boat Appearance
- 1.9.16 Dock Lockers and Steps
- 1.9.17 Dinghies
- 1.9.18 Pets
- 1.9.19 Noise
- 1.9.20 Soliciting
- 1.9.21 Disorderly Conduct
- 1.9.22 Dock Housekeeping
- 1.9.23 Outside Contractors and Vendors

- 1.9.24 Owner Work
- 1.9.25 Unoccupied Berths
- 1.9.26 Removal of Personal Property at Contract Expiration
- 1.9.27 Storm Conditions
- 1.9.28 Automobile Parking

#### 1.10 Sanitary Pumpout Procedures

The marina manager will oversee the sanitary pumpout facility operation and seasonal/part-time personnel will provide assistance to boaters using the facility.

#### 1.11 Procedure for Arrival of Foreign Vessels

Boaters should follow U.S. Coast Guard Regulations and marina personnel will cooperate with U.S. Coast Guard personnel as needed.

#### 1.12 Emergency Procedures

The marina will comply with U.S. Coast Guard and City of Rochester/Monroe County guidelines in providing for the following:

- 1.12.1 Storm Management Plan
- 1.12.2 Emergency Contact Information
- 1.12.3 Accident Procedures
- 1.12.4 Medical Emergency
- 1.12.5 Fire Prevention and Control
- 1.12.6 Bomb Threats
- 1.12.7 Fuel Spill – Since there are no fueling services provided and thus no fuel storage on site, an official Spill, Prevention, Control and Countermeasure (SPCC) Plan is not required for this marina by the EPA. However, a similar facility wide plan will be created by the marina manager to prevent the spill of fuel and to outline a contingency plan in the event an occurrence to isolate and minimize the area affected. The plan will also address the reporting requirements of such an event. A spill must be reported to the National Response Center per the Clean Water Act if:
  - The spill is to navigable waters or the adjoining shoreline, or
  - Water quality standards could be violated, or
  - The spill causes a sheen or discoloration, or
  - The spill causes a sludge or emulsion.

#### 1.13 Procurement of Supplies and Equipment

Procurement of supplies and equipment will be per City of Rochester policies.

#### 1.14 Marine Radio Procedure

The marina manager is responsible for monitoring the marine radio.

## **2. Maintenance**

### 2.1 General

All marina personnel should be aware of maintenance procedures and are required and encouraged to report deficiencies to the marina manager.

### 2.2 Upland Elements

2.2.1 Roofing, siding, doors, insulation, structural condition, electrical equipment, HVAC, utilities, telephones, laundry, toilets, security, fire protection, landscaping, drainage, lighting, fencing, erosion, trash receptacles, gates, and grinder lift station.

### 2.3 Piers, Gangways, and docks

2.3.1 The marina manager is responsible for periodic inspection of all piers, gangways, and docks. Inspection should include all appurtenances including, but not limited to decking, railings, connection hardware, floatation, freeboard, list and trim, cleats, bollards, fendering, utility pedestals, hatch covers, utility metering, utility conduits and signage.

2.3.2 If any deficiencies are observed, the marina manager will log the deficiency and recommend action to the marina owner at the earliest possible time. If recommended action is beyond the abilities of the marina manager, cost estimates will be solicited from service providers.

2.3.3 The marina manager shall oversee and ensure that adequate measures are taken to ensure all piers, gangways, and docks are winterized and secure before freezing conditions occur annually.

### 2.4 Sanitary Pump Out Facility

2.4.1 The marina manager is responsible for ensuring periodic inspection of the sanitary pump out facility.

### 2.5 Wave Attenuation Structures

2.5.1 During the boating season, the marina manager is responsible for ensuring periodic inspection of the wave attenuation structures including, but not limited to armor stone, rip rap, floating attenuators, baffle walls, and wood barriers.

### 2.6 Navigational Aids

2.6.1 The marina manager is responsible for ensuring periodic inspection of all navigational aids within the marina entrance and within the marina including lights, signs, piles, and signage.

### 2.7 Safety and Housekeeping

2.7.1 The marina manager is responsible for ensuring the safe condition and housekeeping of the marina on a periodic basis. In particular, the following items must be evaluated and cleaned:

- 2.7.1.1 Stairs and walkways clear of obstructions
- 2.7.1.2 Trash in designated areas and receptacles emptied in to dumpsters
- 2.7.1.3 Unauthorized areas secure
- 2.7.1.4 Railings and handholds secure
- 2.7.1.5 Decks clear of debris and trash
- 2.7.1.6 Chemicals and flammables secured and appropriate signage in place
- 2.7.1.7 Safety lighting operable
- 2.7.1.8 Dangerous areas barricaded and posted
- 2.7.1.9 Electrical lines and fixtures safe
- 2.7.1.10 Slippery areas surfaced with nonskid material
- 2.7.1.11 Bathroom and shower facilities must be checked/cleaned periodically.

## 2.8 Dredging

### 2.8.1 Marina Entrance/Basin

Semiannually or annually, depth soundings must be checked to evaluate silting of entrance and basin and to ensure ample depths for navigation. Based on the estimated sediment loading of approximately one foot per year or less, it is anticipated that the marina will need to be dredged every three to four years. Sediment dredging will be performed under state and federal permit conditions established to protect water quality and marine life.

## 2.9 Basin

### 2.9.1 Floating Vegetation and Debris Removal

The Marina Manager is responsible for ensuring that debris and floating vegetation is removed on a daily basis during the boating season and on a weekly basis during the off-season.

### 2.9.2 Submerged Aquatic Vegetation Management

Although submerged vegetation is not anticipated, the marina manager will choose an appropriate remedy if submerged vegetation becomes an issue. Possible remedies include mechanical solutions, such as underwater weed harvesters, and chemical solutions (herbicides). The remedy must be permitted by the New York State Department of Environmental Conservation before implementation.

### 2.9.3 Water Circulation

During the boating season, the marina manager will inspect the passive water circulation system weekly to ensure water is circulating through the pipes. A pump can be installed, if needed, to supplement passive water circulation.



**STATE ENVIRONMENTAL QUALITY REVIEW  
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City Hall, Room 307A, 30 Church Street  
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**Appendix E**

**Supplement to 2000 Cultural Resource Inventory**



## The Historic Preservation Review Process in New York State

In order to insure that historic preservation is carefully considered in publicly-funded or permitted undertakings\*, there are laws at each level of government that require projects to be reviewed for their potential impact/effect on historic properties. At the federal level, Section 106 of the National Historic Preservation Act of 1966 (NHPA) directs the review of federally funded, licensed or permitted projects. At the state level, Section 14.09 of the New York State Parks, Recreation and Historic Preservation Law of 1980 performs a comparable function. Local environmental review for municipalities is carried out under the State Environmental Quality Review Act (SEQRA) of 1978.

regulations on line at:

<http://nysparks.state.ny.us> then select HISTORIC PRESERVATION then select Environmental Review

Project review is conducted in two stages. First, the Field Services Bureau assesses affected properties to determine whether or not they are listed or eligible for listing in the New York State or National Registers of Historic Places. If so, it is deemed "historic" and worthy of protection and the second stage of review is undertaken. The project is reviewed to evaluate its impact on the properties, significant materials and character. Where adverse effects are identified, alternatives are explored to avoid, or reduce project impacts; where this is unsuccessful, mitigation measures are developed and formal agreement documents are prepared stipulating these measures.

ALL PROJECTS SUBMITTED FOR REVIEW SHOULD INCLUDE THE FOLLOWING MATERIAL(S).

### Project Description

Attach a full description of the nature and extent of the work to be undertaken as part of this project. Relevant portions of the project applications or environmental statements may be submitted.

### Maps Locating Project

Include a map locating the project in the community. The map must clearly show street and road names surrounding the project area as well as the location of all portions of the project. Appropriate maps include tax maps, Sanborn Insurance maps, and/or USGS quadrangle maps.

### Photographs

Photographs may be black and white prints, color prints, or color laser/photo copies; standard (black and white) photocopies are NOT acceptable.

*-If the project involves rehabilitation, include photographs of the building(s) involved. Label each exterior view to a site map and label all interior views.*

*-If the project involves new construction, include photographs of the surrounding area looking out from the project site. Include photographs of any buildings (more than 50 years old) that are located on the project property or on adjoining property.*

**NOTE: Projects submissions will not be accepted via facsimile or e-mail.**

\*Undertaking is defined as an agency's purchase, lease or sale of a property, assistance through grants, loans or guarantees, issuing of licenses, permits or approvals, and work performed pursuant to delegation or mandate.

**Table I-1 Involved and Potentially Involved Agencies Under SEQR**

<u>INVOLVED / POTENTIALLY INVOLVED AGENCIES</u>	<u>ACTION(S)</u>
<b>City of Rochester</b>	
Mayor/City Council	Funding Comprehensive Plan Amendment Zoning Map and Text Amendment Land Disposition/Acquisition Amendment to City County Parks Agreement Parkland Alienation/Dedication Official Map Amendment
Commissioner of Neighborhood and Business Development	Site Preparation Permit Flood Development Permit Demolition Permit
Manager of Zoning	Site Plan Review
City Planning Commission	Special Permit Subdivision
Traffic Control Board	Right-of-way parking/signalization approvals
<b>New York State</b>	
Dept. of Environmental Conservation	Article 15 Excavation and Fill Article 15 Docks, Moorings and Platforms 401 Water Quality Certification Mined Land Reclamation permit SPDES
Department of State	Funding
Dormitory Authority	Funding ( <i>CYAA Concessions Facility</i> )
Department of Transportation	Funding
SUNY College at Brockport	Lease Execution
Office of Parks, Recreation & Historic Preservation	Parkland Alienation Funding (US Fish and Wildlife BIG grant)
<b>Monroe County</b>	
Executive/Legislature	Amendment to the City/County Parks Agreement Land Acquisition/Disposition/Lease Agreements Parkland Alienation
Pure Waters	Utility modification approvals
<b>Town of Irondequoit</b>	
Town Board	Potential New Boat Launch Development
Town Planning Board	Potential New Boat Launch Development
<b>Town of Greece</b>	
Town Planning Board	Potential Parking Facility



# City of Rochester

Neighborhood and Business Development  
City Hall Room 125B, 30 Church Street  
Rochester, New York 14614-1290  
www.cityofrochester.gov



Bureau of Planning  
and Zoning

October 14, 2011

Marie Sarchiapone  
New York State OPRHP  
Peebles Island  
POB 189  
Waterford, NY 12188

Dear Ms. ~~S~~<sup>Marie</sup> Sarchiapone,

The City of Rochester has just issued a draft environmental impact statement for the development of the Port of Rochester where the Genesee River flows in to Lake Ontario. We sent the full DEIS to your office under separate cover, but for ease of your review I am sending the historic resource section distinctly. I ask that you affirm our belief that the development proposal will not have an adverse impact on historic resources.

The Port of Rochester is a +/-22 acre site that has provided minimal public benefit for decades, serving primarily as a vast parking lot and a park maintenance facility. Although the site is open to the public, there are few amenities beyond parking. There is no public green space within the site, no dedicated pedestrian access from the south, no marine services, and only few retail businesses and eateries.

For more than 20 years, the redevelopment of the site has been considered in various planning studies. The Monroe County Waterfront Recreation Opportunities Study (1990) suggested ways to enhance Ontario Beach Park to serve the community and to attract tourists, including improvements to pedestrian circulation, parking access, and public transit linkages. Recommendations to expand the City's public marina facilities were made in the Local Waterfront Revitalization Program (1990, 2010), in a market analysis (2006), and in the Marina Engineering Report and Feasibility Study (2009).

In 1998, efforts were begun to initiate high-speed ferry service between Rochester and Toronto. The port site was chosen for a new marine terminal, sparking creation of a larger development proposal that became the Port of Rochester Harbor and Public Improvement Project. Based on this plan and on environmental and cultural investigations, and in consultation with the OPRHP, several changes were made to the port site:

1. Modification of the North Warehouse into a ferry terminal building, with the addition of an embarkation building, customs stations and related site work
2. Demolition of the South Warehouse
3. Installation of streets, sidewalks, parking lots and utilities
4. Reconstruction of the river wall
5. Improvement to the Lake Avenue public right-of-way

High-speed ferry service began in 2004 but soon proved financially impractical and was suspended in early 2006. The ferry was sold, and the terminal building has been underused since. While there has been some interest in starting a new ferry service, no concrete plans have surfaced.



In 2006, we issued a comprehensive master plan for the port area, prepared by Sasaki Associates. The plan envisioned a diverse mix of uses around the ferry terminal, including up to 700 residential units, 80000SF of commercial space, 27000SF of educational space, 6000SF of office space, a marina, parking and recreational areas. For various reasons the plan was not adopted, but its research and findings remain valid and form the basis of our current planning effort.

Today, building upon the past two decades of discussion, investigation, planning and consultation, we are proposing a new plan to transform the port site into a year-round, recreationally-oriented area that would complement other public resources nearby, including Ontario Beach Park, the Terminal Building, the lake pier and the Charlotte Genesee Lighthouse. Parking lots that currently consume prime waterfront land would be replaced with a mix of buildings, a public marina, a public promenade and new streets, trails and sidewalks.

The project is being planned consistent with the following goals set forth in the LWRP:

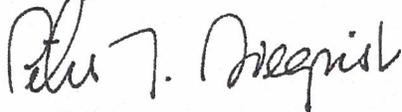
- Preserve and enhance the village character of Charlotte;
- Create a family-oriented, four-season development;
- Maintain and enhance visual and physical access to the water;
- Improve access into and out of the port area;
- Enhance economic development and business activity within Charlotte;
- Improve pedestrian circulation and safety in the area;
- Protect and enhance the environmental, historic and cultural resources of the area; and,
- Develop a mixed-use project that balances public uses and needs with appropriate private development that expands the tax base.

A Generic Environmental Impact Statement prepared by the City in 2001 for the earlier changes to the port included a Phase 1A and 1B Cultural Resource Survey (completed December 2000). This survey helped guide the conversion of the North Warehouse into the ferry terminal and the demolition of the South Warehouse. The Survey covered the area of the current master plan, except for the site of the Charlotte Genesee Lighthouse and two properties to its immediate north.

Due to the correlation between project boundaries, along with the accuracy and completeness of the 2000 Cultural Resource Survey and our findings that the resources identified in the survey remain largely unchanged, we propose to resubmit the Cultural Resource Survey to describe the existing setting, with supplemental updates and additional information regarding the lighthouse site and the two properties to the north. We are providing current photographs of the resources, and have keyed them to a map.

Please feel free to contact me for clarifications.

Sincerely,

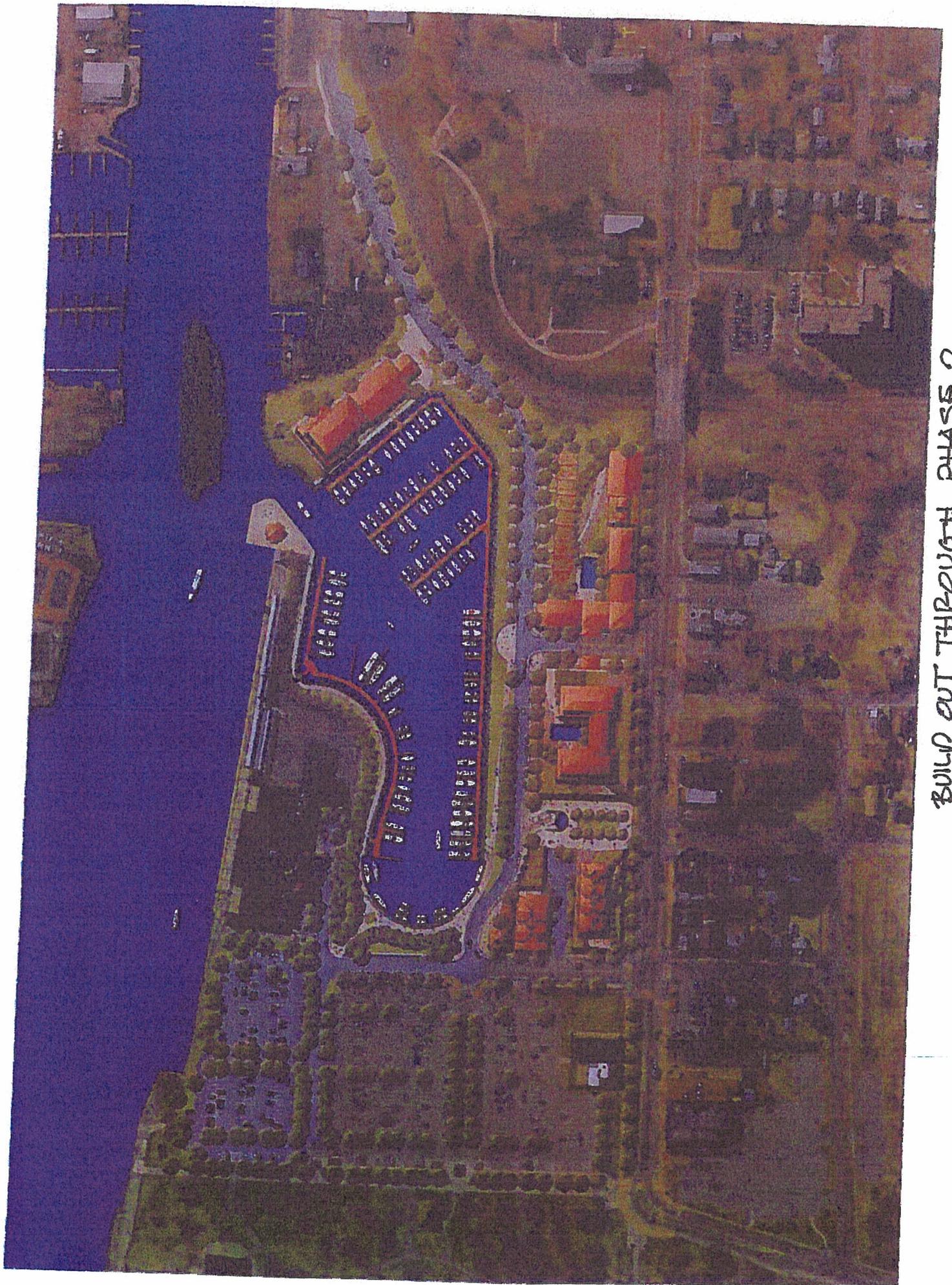


Peter Siegrist, AIA, LEED AP

Preservation Planner

(585)428-7238

[Peter.siegrist@cityofrochester.gov](mailto:Peter.siegrist@cityofrochester.gov)



BUILD OUT THROUGH PHASE 2



City Hall Park

4776  
4768

North Warehouse/  
Terminal Building

Fast Ferry

Swing Bridge

County Operations  
Center

Railroad

Warehouse

## Port of Rochester Existing Setting Analysis

The project limits lay mostly within the area addressed in a Cultural Resource Survey that was conducted for an earlier harbor improvement project, portions of which were implemented. The limits of the current project fall within those of the previous project, which extended farther south along the river to Petten Street. The sole exception is that the current project includes the site of the Genesee Lighthouse and Keeper's House.

The implemented portions of the earlier plan that lay within or adjacent to the current project limits include:

1. Modification of the North Warehouse into a ferry terminal building, with the addition of an embarkation building, customs stations and related site work
2. Demolition of the South Warehouse
3. Installation of streets, sidewalks, parking lots and utilities
4. Reconstruction of the river wall
5. Improvement to the Lake Avenue public right-of-way

**Due to the correlation between project limits, the accuracy and completeness of the earlier Cultural Resource Survey, and the findings of the current project sponsor that the resources identified in the survey remain largely unchanged, the project sponsor proposes to resubmit the Cultural Resource Survey to describe the existing setting, with the following updates and additions.**

Phase 1A and B Cultural Resource Investigations for the earlier project were conducted in 2000 by the Regional Heritage Preservation Program of the Department of Collections and Research of the Rochester Museum & Science Center (RMSC). The Phase 1A investigations examined the environmental, archaeological and historical literature prepared in the 15 years since the RMSC had conducted a Cultural Resources Inventory for the City's Local Waterfront Revitalization Program in 1986. The Phase 1B field investigations included an architectural survey of any buildings or structures not inventoried earlier, and subsurface shovel testing in suitable areas.

Within the limits of the current project, the report of the Cultural Resources Investigations [p. 80] stated the following:

Despite the number of prehistoric and historic archaeological sites documented within and surrounding the project area, substantial previous disturbance associated with filling throughout much of the project area as well as building demolition and road construction, has left little of the project area suitable for subsurface testing. A map of the harbor conditions at Charlotte in 1829 shows much of the project area as "reed-filled waterways". Most of the area north of the lighthouse and east of Lake Avenue [*the current project site (notation added)*] and along the western bank of the river to below Latta Road had to be filled before any construction or development could occur.

Within the limits of the current project, the investigations found only six existing buildings, as follows. Recent photographs are attached.

1. 70 Lighthouse Street (Genesee Lighthouse and Keeper's House) [appendix B, p.45]  
Listed in the SRHP/NRHP (90NR1478) and designated a Rochester City landmark in 1974. The report of the Cultural Resources Investigations includes the SRHP/NRHP nomination form.
2. 4650 Lake Avenue [p. 85]  
County operations building <50 years old and determined not to be eligible for SRHP/NRHP listing
3. 4768 Lake Avenue [pp. 87 and 117]  
The Cultural Resource Investigations report [p.117] states "This one-story frame commercial structure was built prior to 1918. Its present exterior – a combination of stucco, brick, and mock-mansard roof—masks any original exterior elements. This building does not possess the distinctive characteristics of a particular style or period nor is it the work of a master, and it does not possess high artistic value". The building was determined not to be eligible for SRHP/NRHP listing.
4. 4776 Lake Avenue [p. 87]  
A commercial structure <50 years old and determined not to be eligible for SRHP/NRHP listing
5. North Warehouse [p.66]  
This building was determined individually eligible for SRHP/NRHP listing, and the inventory form is included in the report of the Cultural Resource Investigations [p.106]. As part of the earlier project, the building was altered into a ferry terminal, with the addition of an embarkation building, customs stations and related site work
6. South Warehouse [p.66]  
Since demolished during the earlier project, this building was determined not to be eligible for SRHP/NRHP listing

Immediately north of the project site is Ontario Beach Park. [p. 69] The Park and eleven park buildings have been determined to be eligible for SRHP/NRHP listing as a group. One of the eleven, the Ontario Beach Carousel, was designated a City of Rochester landmark in 1980.

One structure adjacent to the project limits but outside the jurisdiction of the City of Rochester was found to be individually eligible for inclusion in the SRHP/NRHP. The Hojack Swing Bridge stands in the middle of the Genesee River, about 4500 feet upstream from where the river meets Lake Ontario. It is a rotating bridge that rests on a central pier at midstream, and is now stalled in an "open" position parallel to and about 120' from both shorelines. Although both shorelines are within the City of Rochester, the navigational channel is under federal jurisdiction. An effort in 2003 to nominate the bridge as a city landmark failed when it was ruled that the bridge is outside municipal jurisdiction, even though the bridge abutments (where the bridge would rest when "closed"), are within municipal boundaries. The inventory form is included in the Cultural Resource Investigations report. [p. 102]

The conclusion of the report of the Cultural Resource Investigations [p. 92] stated the following:

Based on the extent of previous disturbance documented through geological and geotechnical investigations of the proposed project, especially that portion of the project area located north of the CXT track and east of Lake Avenue [*the current project site (notation added)*], historic map evidence and the on-site inspection, the project area was assigned an overall sensitivity estimate of low with regard to historic and prehistoric archaeological resources. However, in areas exhibiting less disturbance (the Genesee Lighthouse Site), this sensitivity estimate was modified to high for historic and prehistoric archaeological sites.

As part of the previous project, no Phase 1B subsurface testing was recommended for the lighthouse site because no ground-disturbing activities were proposed. The report recommended that if any such activities are proposed, the SHPO should be consulted and a qualified archeologist conduct investigations.

END

**Port Existing Settings Photographs**  
Numbers keyed to site plan

1. Lighthouse and Keeper's House from southeast
2. Keeper's House from west
3. Lighthouse from east
4. Holy Cross Church from lighthouse
5. Swing bridge from lighthouse
6. Close up of swing bridge
7. Port area from lighthouse
8. North Warehouse/Ferry Terminal from south
9. 4550 Lake Avenue (Islamic Center) from southwest
10. 4550 Lake Avenue (left) and 4554 Lake Avenue (RGE substation at right) from lighthouse
11. 4554 Lake Avenue (RGE substation) from west
12. 4560 Lake Avenue (Suss vehicle repair station) from southwest
13. 4554 Lake Avenue (RGE substation) from lighthouse
14. 4554 Lake Avenue empty land north of lighthouse
15. 4650 Lake Avenue (Monroe County Operations Center) from southeast
16. 4580 Lake Avenue from west
17. North River Street toward Ontario Beach Park
18. North River Street toward Ontario Beach Park
19. Ontario Beach Park walkway from west toward river
20. Denzel Carousel from south
21. 4776 Lake Avenue from northwest
22. 4768 Lake Avenue from southwest



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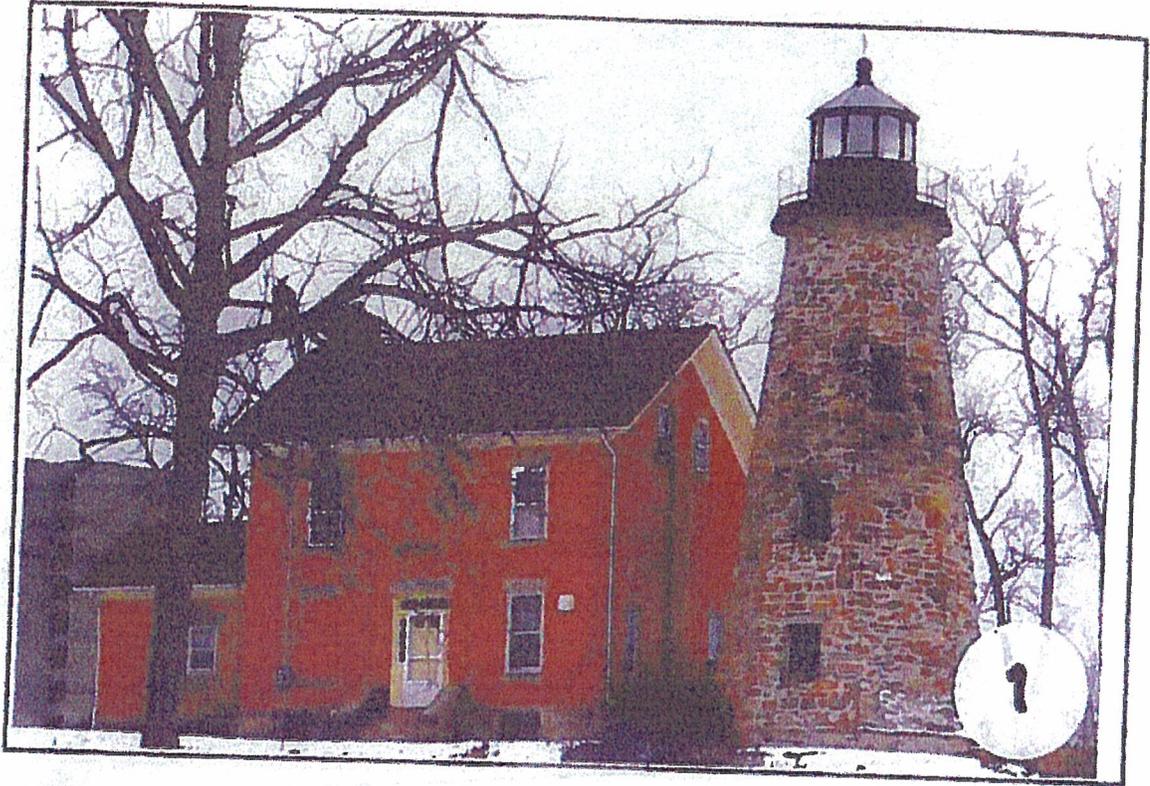
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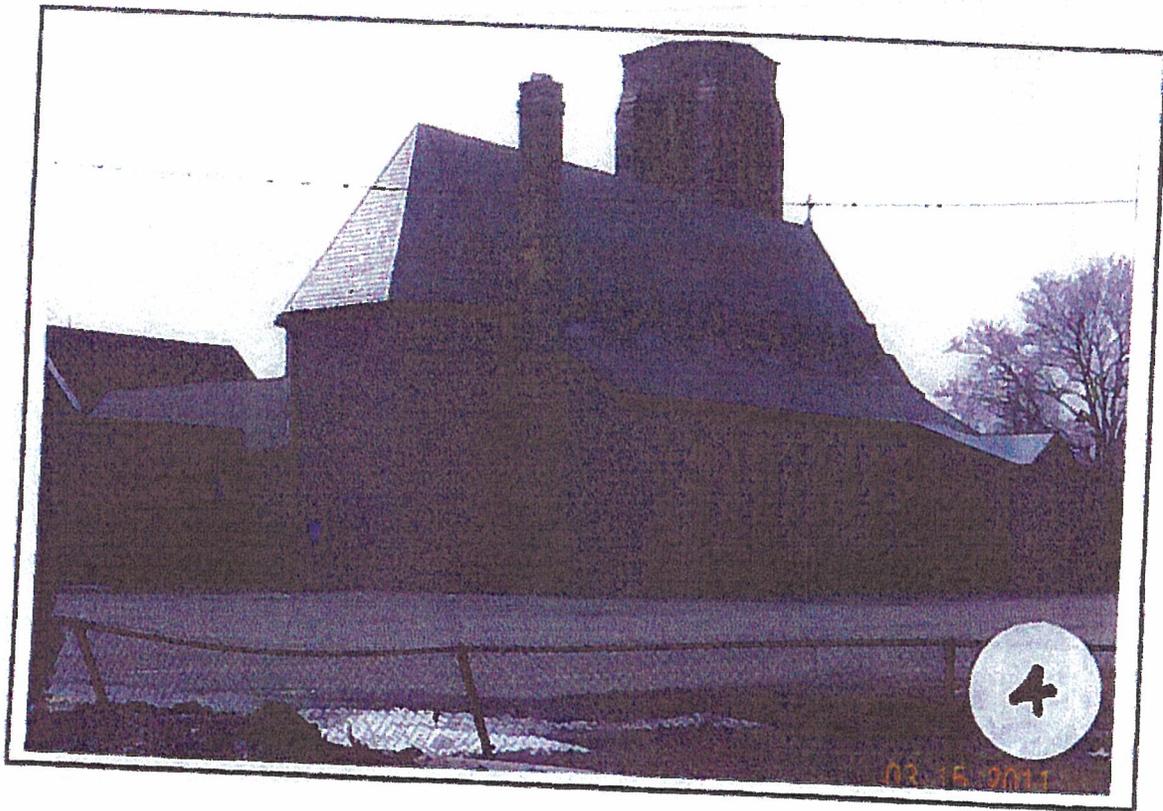
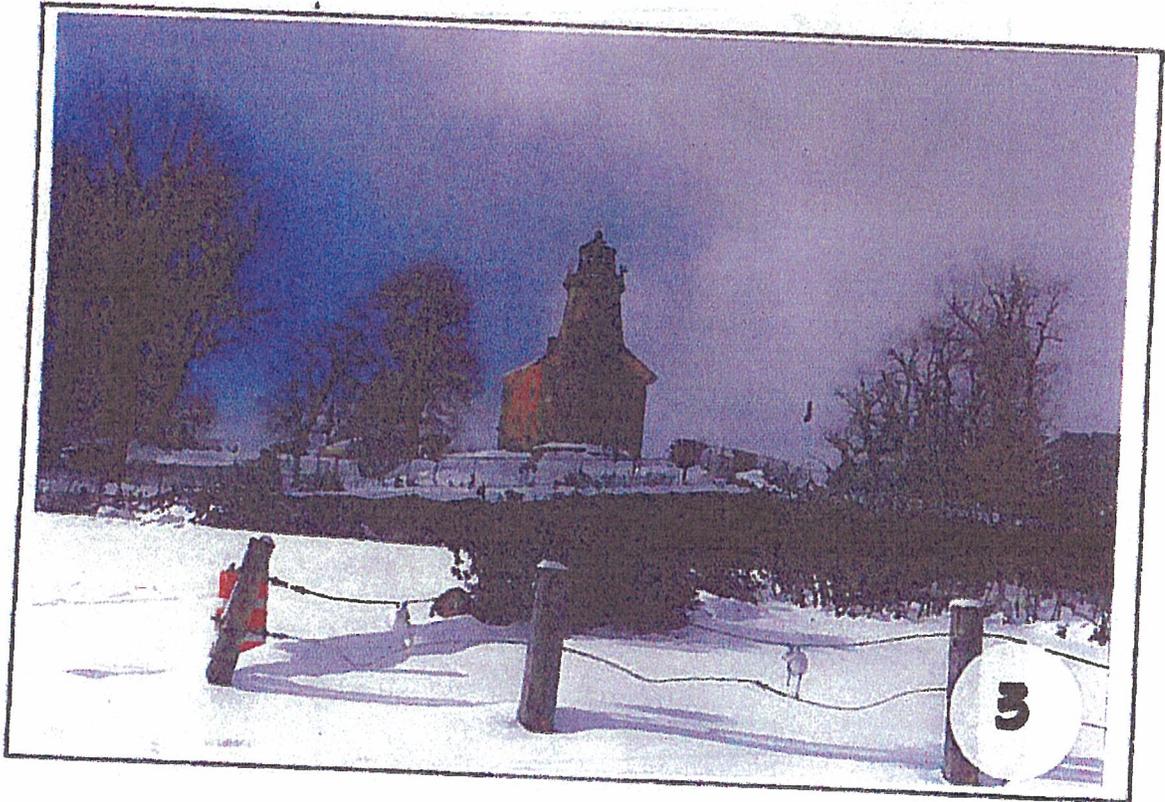
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GENESEE RIVER

Port of Rochester, NY  
Application to the State Historic Preservation Office  
March 28, 2011



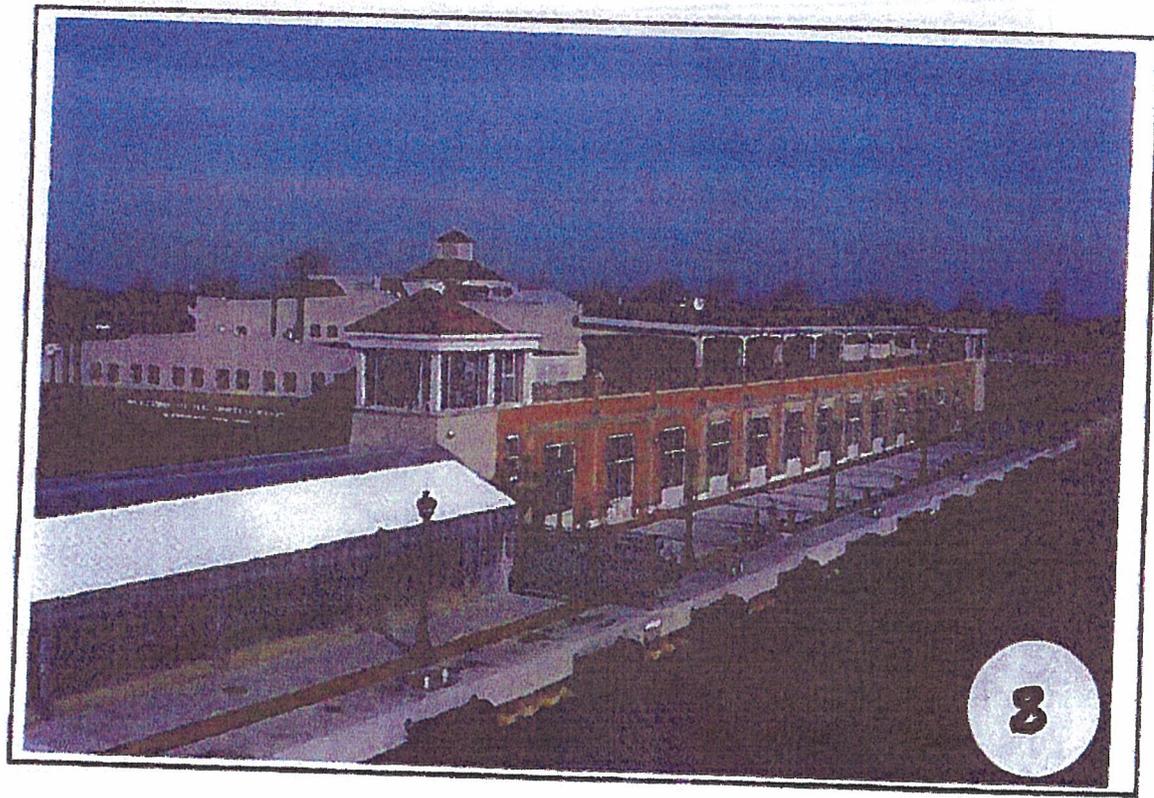
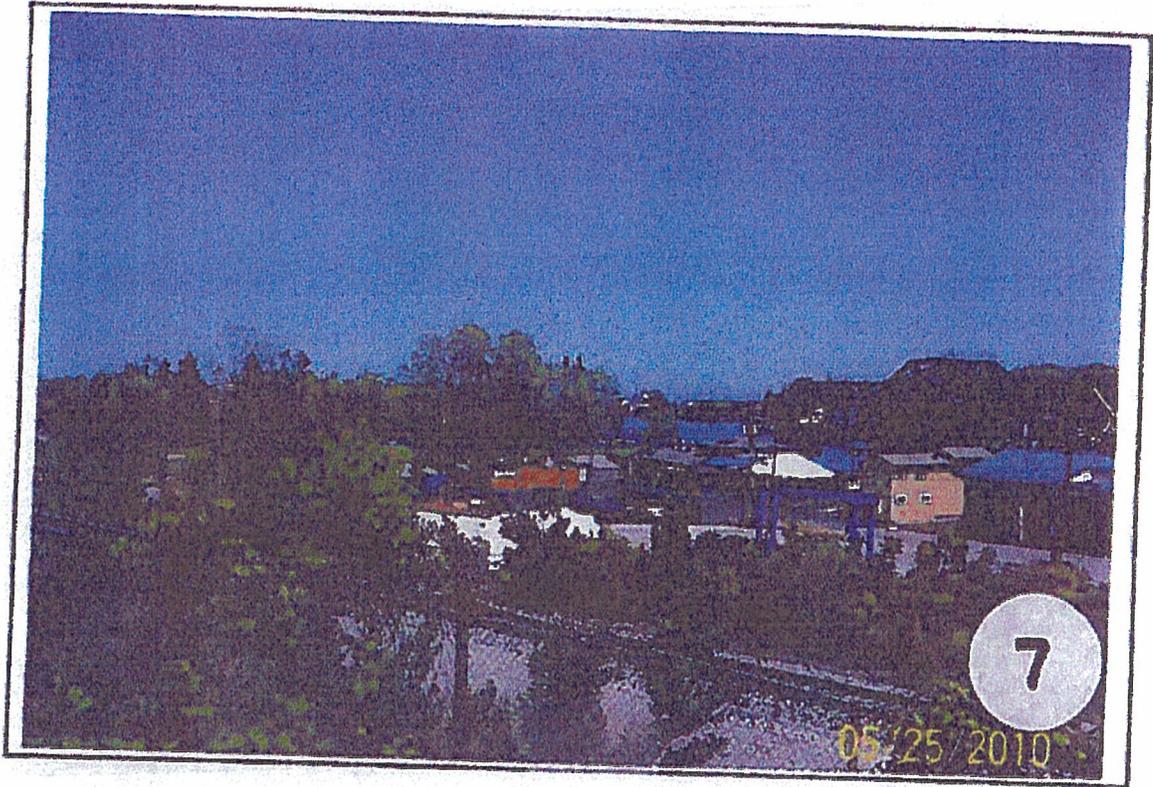
Port of Rochester, NY  
Application to the State Historic Preservation Office  
March 28, 2011



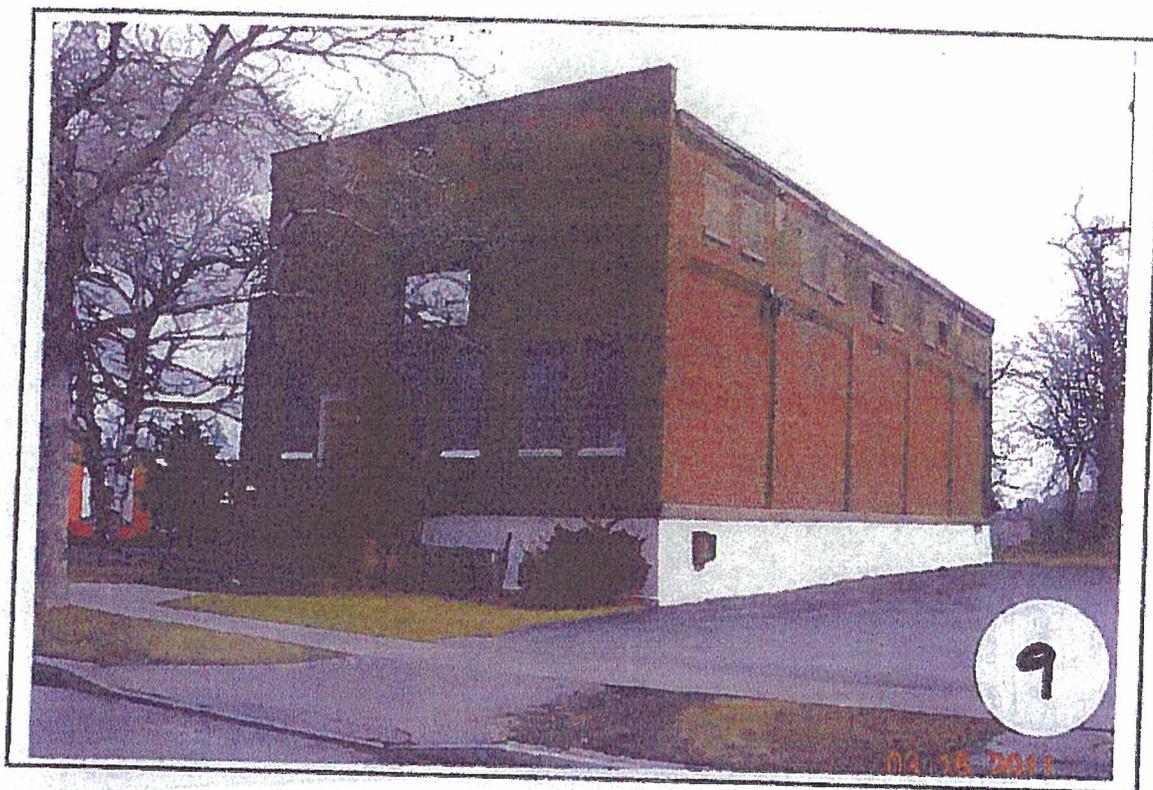
Port of Rochester, NY  
Application to the State Historic Preservation Office  
March 28, 2011



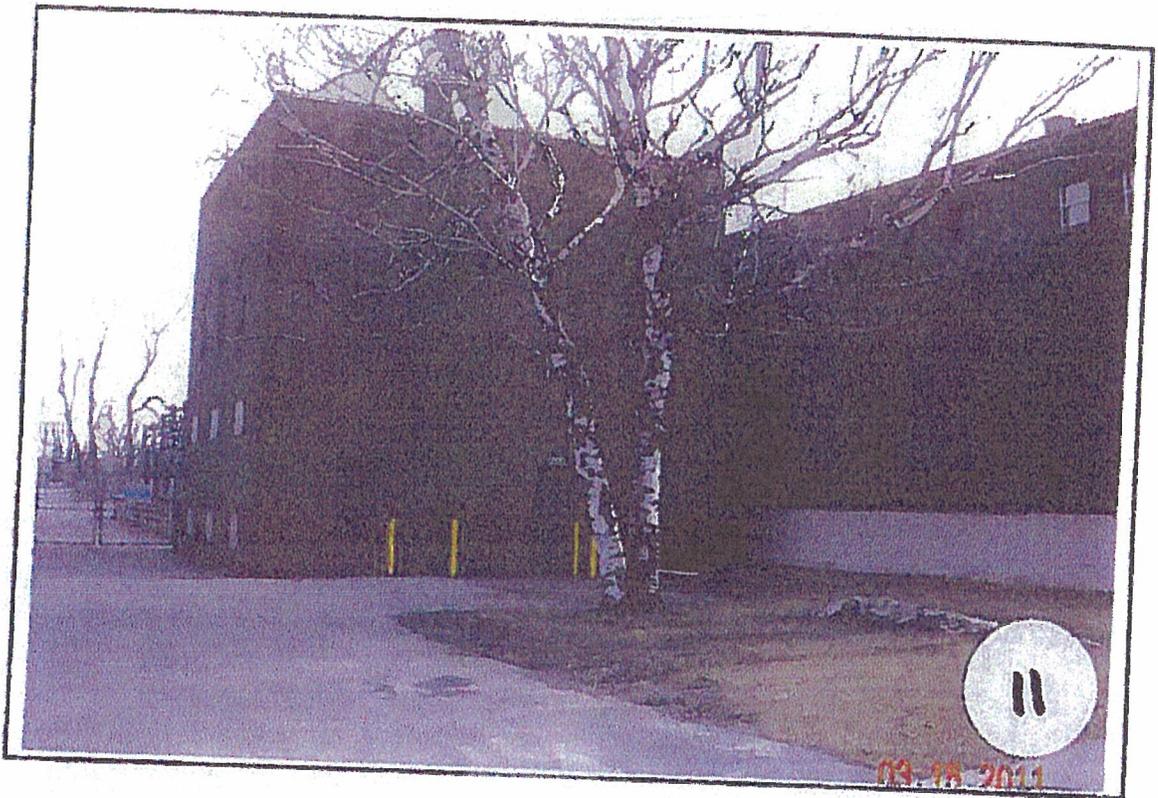
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March 28, 2011



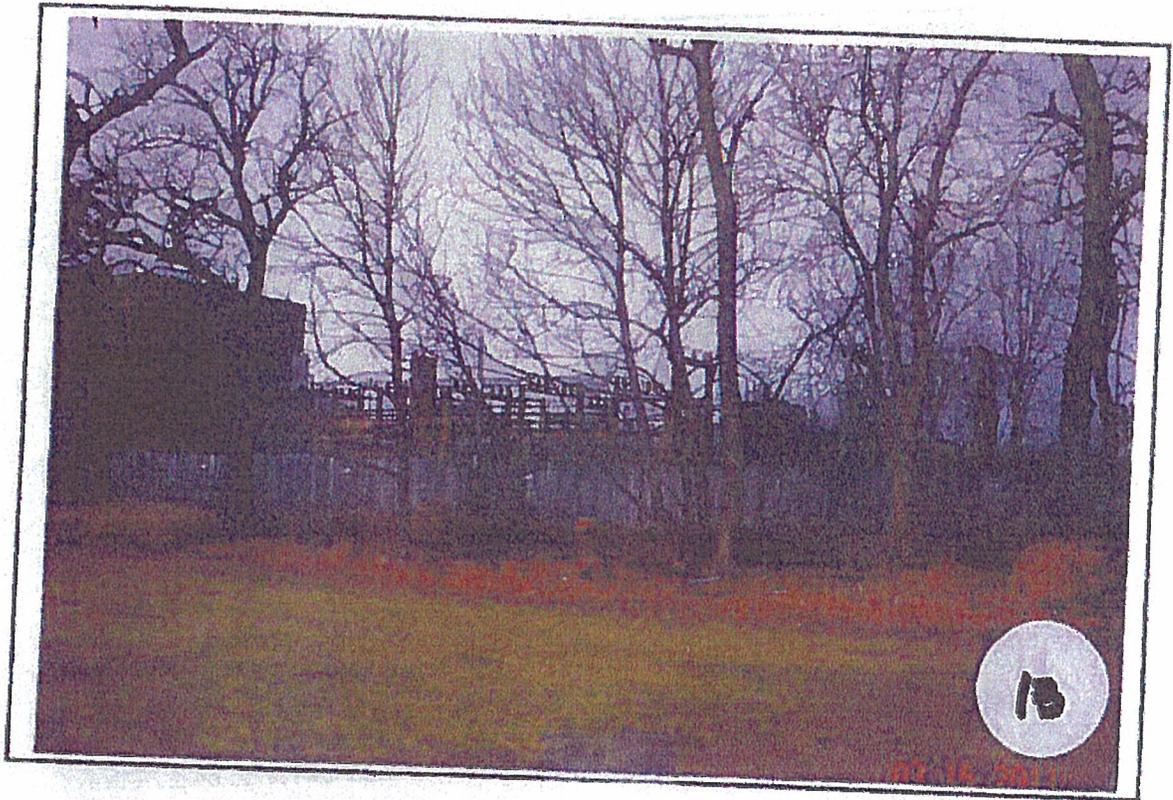
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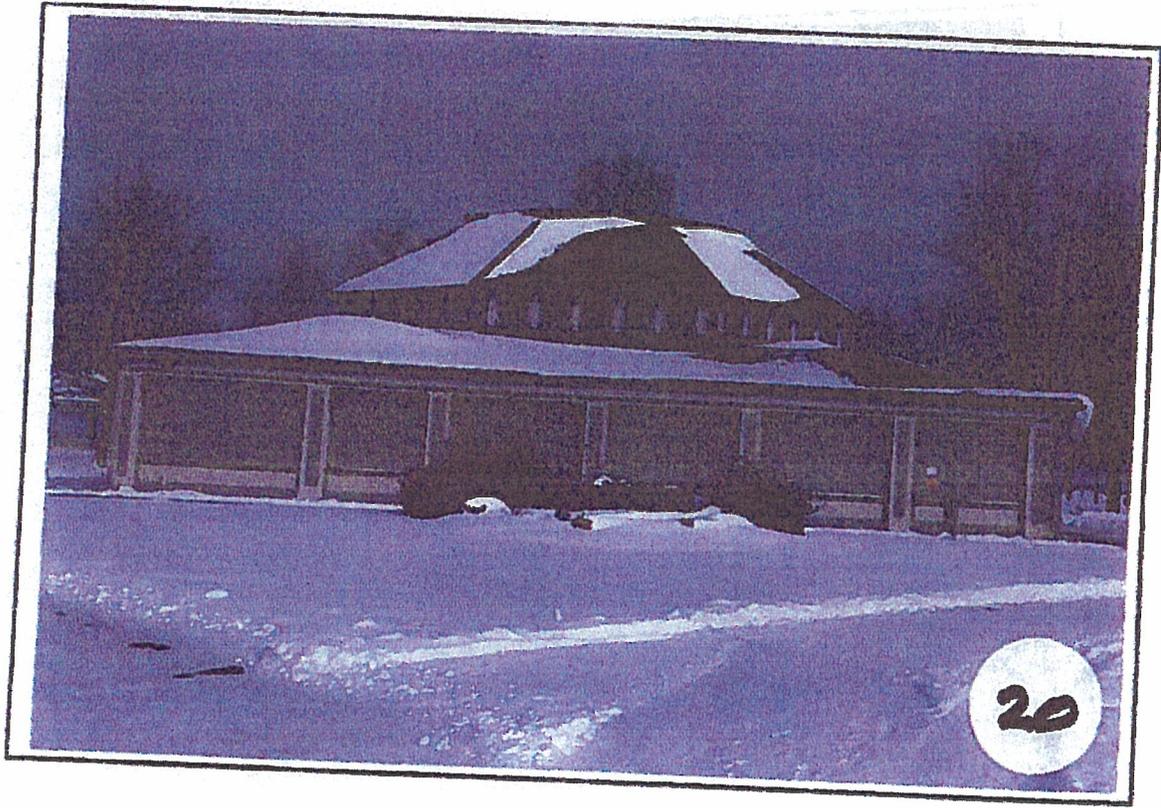
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March 28, 2011



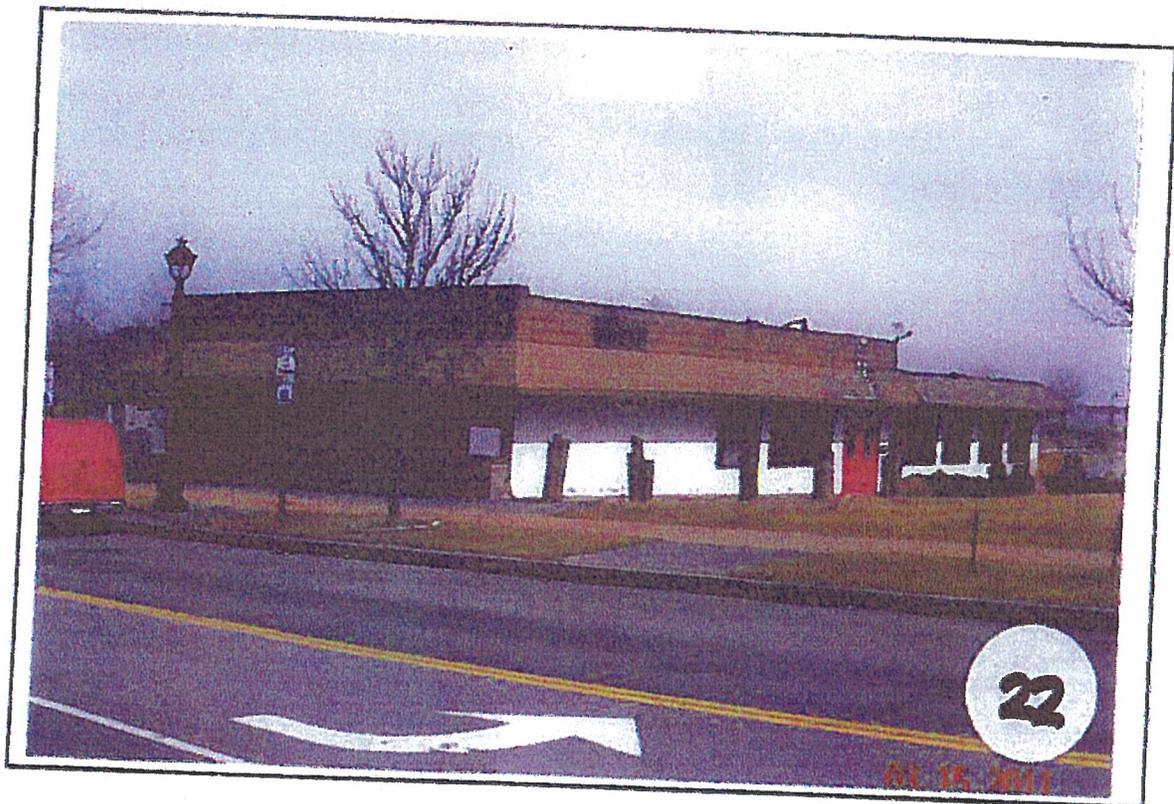
Port of Rochester, NY  
Application to the State Historic Preservation Office  
March 28, 2011







Port of Rochester, NY  
Application to the State Historic Preservation Office  
March 28, 2011



**STATE ENVIRONMENTAL QUALITY REVIEW  
(SEQR)**

**FINAL  
SITE SPECIFIC/GENERIC ENVIRONMENTAL IMPACT  
STATEMENT  
FEIS**

**Proposed Action:  
City of Rochester Port Public Marina and Mixed Use Development Project**

**SEQR Environmental Impact Statement  
NEPA Environmental Assessment**

**LEAD AGENCY:  
Thomas S. Richards  
Mayor, City of Rochester  
City Hall, Room 307A, 30 Church Street  
Rochester, NY 14614-1290**

**Appendix F**

**1829 Overlay Map**

