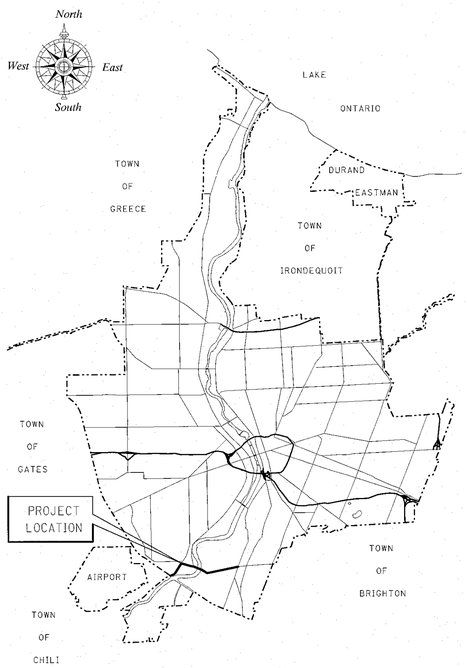


GENERAL LOCATION MAP

STREET LIMITS					
SCOTTSVILLE ROAD - ELMWOOD AVENUE					
IMPROVEMENT PROJECT					
#	STREET	START	END	DWG #	LENGTH MILES
1	SCOTTSVILLE RD	WEST CITY LINE	GENESEE STREET	54	0.36
2	ELMWOOD AVE	GENESEE STREET	MT. HOPE AVE	-	1.14
				TOTAL	1.50

#	STREET	P.I.N.

Lu Engineers

ENVIRONMENTAL • TRANSPORTATION • CIVIL

CITY OF ROCHESTER, NEW YORK

DEPARTMENT OF ENVIRONMENTAL SERVICES

SCOTTSVILLE ROAD – ELMWOOD AVENUE
STREET IMPROVEMENTS
PN #17102

STREET IMPROVEMENT PROGRAM

SCOTTSVILLE ROAD - ELMWOOD AVENUE

(WEST CITY LINE - MT. HOPE AVENUE)

IMPROVEMENT PROJECT

BID PLANS
APRIL 1, 2019

PROJECT LENGTH 1.50 MILES

COR 17102

STATE OF NEW YORK
JONATHAN W. BRAMAN
075865
LICENSED PROFESSIONAL ENGINEER

3/22/19
DATE

3/25/19
DATE
NORMAN JONES
COMMISSIONER
DEPARTMENT OF ENVIRONMENTAL SERVICES
CITY OF ROCHESTER

3.25.19
DATE
HOLLY E. BARRETT, P.E.
CITY ENGINEER
CITY OF ROCHESTER

3-22-19
DATE
TIMOTHY P. FRELIER, P.E.
DIRECTOR
MONROE COUNTY DEPARTMENT OF TRANSPORTATION

ALIGNMENT

ABBR.	DESCRIPTION
AH	AHEAD
AZ	AZIMUTH
BK	BACK
℄	BASELINE
BRG	BEARING
℄	CENTERLINE
CS	CURVE TO SPIRAL
e	SUPERELEVATION RATE (CROSS SLOPE)
EQ	EQUALITY
EXT	EXTERNAL
HCL	HORIZONTAL CONTROL LINE
HSD	HEADLIGHT SIGHT DISTANCE
L	LENGTH OF CIRCULAR CURVE
LS	LENGTH OF SPIRAL
LVC	LENGTH OF VERTICAL CURVE
E	CENTER CORRECTION OF VERTICAL CURVE
ML	MAIN LINE
PC	POINT OF CURVATURE
PI	POINT OF INTERSECTION
POL	POINT ON LINE
PSD	PASSING SIGHT DISTANCE
PT	POINT OF TANGENT
PVC	POINT OF VERTICAL CURVE
PVI	POINT OF VERTICAL INTERSECTION
PVT	POINT OF VERTICAL TANGENT
R	RADIUS
SC	SPIRAL TO CURVE
SSD	STOPPING SIGHT DISTANCE
ST	SPIRAL TO TANGENT
STA	STATION
T	TANGENT LENGTH
TGL	THEORETICAL GRADE LINE
TS	TANGENT TO SPIRAL
VC	VERTICAL CURVE

TOPOGRAPHY (DRAINAGE)

ABBR.	DESCRIPTION
BB	BOTTOM OF BANK (STREAM)
BC	BOTTOM OF CURB
BO	BOTTOM OF OPENING
CAP	CORRUGATED ALUMINUM PIPE
CB	CATCH BASIN
CIP	CAST IRON PIPE
℄ STRM	CENTERLINE OF STREAM
CMP	CORRUGATED METAL PIPE
CP	CONCRETE PIPE
CSP	CORRUGATED STEEL PIPE
CULV	CULVERT
DIA	DIAMETER
DMH	DRAINAGE MANHOLE
DS	DRAINAGE STRUCTURE PIPE
D'XING	DITCH CROSSING
EHW	EXTREME HIGH WATER
EL	ELEVATION
ELEV	ELEVATION
ELW	EXTREME LOW WATER
ES	END SECTION
HW	HEADWALL
INV	INVERT
MH	MANHOLE
MHW	MEAN HIGH WATER
OHW	ORDINARY HIGH WATER
OLW	ORDINARY LOW WATER
RCP	REINFORCED CONCRETE PIPE
SICPP	SMOOTH INTERIOR CORRUGATED POLYETHYLENE PIPE
TB	TOP OF BANK (STREAM)
TC	TOP OF CURB
TG	TOP OF GRATE
VCP	VITRIFIED CLAY PIPE

TOPOGRAPHY (MISCELLANEOUS)

ABBR.	DESCRIPTION
ABUT	ABUTMENT
AOBE	AS ORDERED BY ENGINEER
ASPH	ASPHALT
BDY	BOUNDARY
BLDG	BUILDING
BM	BENCH MARK
CC	CENTER TO CENTER
CONC	CONCRETE
CONST	CONSTRUCTION
CR	COUNTY ROAD
D	DEED DISTANCE
DM	DIRECT MEASUREMENT
DWY	DRIVEWAY
EP	EDGE OF PAVEMENT
ES	EDGE OF SHOULDER
FEE	FEE ACQUISITION
FEE WO/A	FEE ACQUISITION WITHOUT ACCESS
FP	FENCE POST
FD	FOUNDATION
FL	FENCE LINE
GAR	GARAGE
GR	GRAVEL
HO	HOUSE
HWY	HIGHWAY
IP	IRON PIN OR IRON PIPE
MB	MAILBOX
MON	MONUMENT
N&W	NAIL AND WASHER
OG	ORIGINAL GROUND
O/H	OVERHEAD
P	PARCEL
PAV'T	PAVEMENT
PE	PERMANENT EASEMENT
PED POLE	PEDESTRIAN POLE
⅊	PROPERTY LINE
POR	PORCH
RR	RAILROAD
RTE	ROUTE
ROW	RIGHT OF WAY
RW	RETAINING WALL
SH	STATE HIGHWAY
SHLDR	SHOULDER
SPK	SPIKE
ST	STREET
STK	STAKE
STY	STORY
SW	SIDEWALK
TE	TEMPORARY EASEMENT
TO	TEMPORARY OCCUPANCY
U/G	UNDERGROUND
WW	WING WALL

UTILITIES

ABBR.	DESCRIPTION
E	ELECTRIC
EMH	ELECTRIC MANHOLE
G	GAS
GP	GUY POLE
GSB	GAS SERVICE BOX (HOUSE LINE)
GV	GAS VALVE (MAIN LINE)
HYD	HYDRANT
LP	LIGHT POLE
LPG	LOW PRESSURE GAS
PP	POWER POLE
SA	SANITARY SEWER
SMH	SANITARY MANHOLE
ST	STORM SEWER
T	TELEPHONE
TCB	TRAFFIC CONTROL BOX
TELBOX	TELEPHONE BOX
TEL P	TELEPHONE POLE
TMH	TELEPHONE MANHOLE
CTV	CABLE TELEVISION
W	WATER
WSB	WATER SERVICE BOX (HOUSE LINE)
WV	WATER VALVE (MAIN LINE)

ABBREVIATION "C" IN CATEGORIES: DA, DM, DN, AND FH WITH:

B	BRIDGE
C	CUT
D	DAM
F	FILL
K	CULVERT
W	WALL
X	TO BE USED IF ONE OF THE ABOVE CANNOT BE DEFINED AT THE TIME THE EXPLORATION IS MADE

STANDARD SYMBOL (PLANS)	ITEM PAYMENT UNIT: ESTIMATE OF QUANTITIES SHEET	EQUIVALENT NOMENCLATURE: (SPECS/PROPOSAL)
"	-	INCHES
'	LF	LINEAR FEET
mi	MI	MILES
ft^2	SF	SQUARE FEET
YD^2	SY	SQUARE YARD
AC	AC	ACRES
YD^3	CY	CUBIC YARD
GAL	GAL	GALLON
lb	LB	POUND
TON	TON	TON

DRAWING INDEX

DWG NO.	TITLE OF DRAWING	SHEET NO.
-	COVER SHEET	1
A - 1	INDEX AND ABBREVIATIONS	2
LEG - 1	POINT LEGEND	3
LEG - 2	LINE LEGEND	4
EQ - 1	ESTIMATE OF QUANTITIES	5
GN - 1 TO GN - 3	GENERAL NOTES	6 - 8
SD - 1 TO SD - 2	SURVEY DATA	9 - 10
WZTC - 1 TO WZTC - 3	WORK ZONE TRAFFIC CONTROL NOTES AND DETAILS	11 - 13
TS - 1	TYPICAL SECTIONS	14
MD - 1 TO MD - 10	MISCELLANEOUS DETAILS	15 - 24
MT - 1 TO MT - 10	MISCELLANEOUS TABLES	25 - 34
PL - 1 TO PL - 10	ROADWAY PLANS	35 - 44
PMP - 1 TO PMP - 10	PAVEMENT MARKING PLANS	45 - 54

Lu Engineers
ENVIRONMENTAL • TRANSPORTATION • CIVIL
PROJECT MANAGER: J. OTTMAN
DRAWN BY: J. MESSENGER

Department of Environmental Services
Architecture and Engineering Services
City of Rochester, New York
ASSISTANT MANAGING ENGINEER LISA Y. REYES
CITY ENGINEER HOLLY E. BARRETT, P.E.

PROJECT TITLE
SCOTTSVILLE ROAD - ELMWOOD AVENUE
IMPROVEMENT PROJECT
(WEST CITY LINE - MT HOPE AVE)

NO.	REVISION	BY	DATE

ISSUED 03/06/2019
CHECKED JWO
DRAWN JRM
DESIGN JRM
SCALE NONE
PROJECT NUMBER 17102

DRAWING TITLE
INDEX AND ABBREVIATIONS
A-1

DRAWING NO. 2 OF 54

FILE NAME = 02a - Abbreviations.dgn
DATE/TIME = 06-MAR-2019 10:53
USER = JMessenger

ALIGNMENT

CELL	NAME	DESCRIPTION
	ACC	CENTER OF CURVATURE
	ACOGO	COGO
	ACS	CURVE TO SPIRAL
	ADPI_P	DETOUR, POINT OF INTERSECT.
	ADPL_P	DETOUR, POINT ON LINE
	AEQN	EQUATION
	AEQNAHD	EQUATION AHEAD
	AEQNBK	EQUATION BACK
	AEVT	EVENT STATION
	APC	POINT OF CURVATURE
	APCC	POINT OF COMPOUND CURVATURE
	API	POINT OF INTERSECTION
	APOB	POINT OF BEGINNING
	APOC	POINT OF CURVATURE
	APOE	POINT OF END
	APOL	POINT ON LINE
	APOS	POINT ON SPIRAL
	APOT	POINT ON TANGENT
	APOVC	POINT ON VERTICAL CURVE
	APOVT	POINT ON VERTICAL TANGENT
	APORC	POINT ON REVERSE CURVE
	APT	POINT OF TANGENCY
	APVC	POINT OF VERTICAL CURVATURE
	APVCC	POINT OF VERT. CMPND CURVE
	APVI	POINT OF VERT. INTERSECTION
	APVRC	POINT OF VERT. REVERSE CURVE
	APVT	POINT OF VERTICAL TANGENCY
	ASC	SPIRAL TO CURVE
	ASPI	SPIRAL POINT OF INTERSECTION
	ASTS	SPIRAL TO SPIRAL
	AST	SPIRAL TO TANGENT
	ATS	TANGENT TO SPIRAL
	AVEVT	VERTICAL EVENT POINT
	AVHIGH	VERTICAL HIGH POINT
	AVLOW	VERTICAL LOW POINT

CONTROL

	CBP	BASELINE, POINT
	CBPOL	BASELINE, POINT ON LINE
	CBSP	BASELINE, SPUR POINT
	CBTP	BASELINE, TIE POINT
	CPBM	BENCHMARK
	CPH	POINT, HORIZ. PHOTOGRAMMETRY
	CPSM	POINT, SURVEY MARKER, PERM.
	CPSV	POINT, VERT., PHOTOGRAMMETRY

DRAINAGE

CELL	NAME	DESCRIPTION
	DINV	INVERT
	DS	STRUCTURE, RECTANGULAR
	DSI	STRUCTURE, INVERT
	DSM	STRUCTURE, MANHOLE
	DSMTXX_P	STRUCTURE, MANHOLE, TYPE "XX" "XX" = 48, 60, 72, 96
	DSR	STRUCTURE, ROUND
	DST"X"CB P	STRUCTURE, RECT., WITH CURB TYPE "X" "X" = F, G, N, O, P, R
	DST"X" P	STRUCTURE, RECT., TYPE "X" "X" = I, K, L, M, O, P, U

ENVIRONMENTAL

CULV	EIOP_P	STR., INLET, OUTLET PROT.
GB	EIPGB_P	STR., INLET PROT., GRAVEL BAG
H/S	EIPHS_P	STR., INLET PROT., HAY/STRAW
PRFB	EIPP_P	STR., INLET PROT., PREFAB.
SF	EIPSF_P	STR., INLET PROT., SILT FENCE
	ERCB	RISER, CONCRETE BOX
	ETRS_P	TRAP, SEDIMENT
	EWFG	WETLAND FLAG

GEOTECHNICAL

	GDH	DRILL HOLE

LANDSCAPE

	LELS	ELEVATION, SPOT
	LFP	FLAG POLE
	LMB	MAILBOX
	LPB	PAPER BOX
	LPST	POST, SINGLE
	LRB	ROCK, BOULDER
	LSHC	SHRUB, CONIFEROUS
	LSHD	SHRUB, DECIDUOUS
	LTC	TREE, CONIFEROUS
	LTD	TREE, DECIDUOUS
	LTS	TREE, STUMP
	LTW P	TREE, WELL OR WALL
	LUKP	UNKNOWN POINT

ROW MAPPING

CELL	NAME	DESCRIPTION
	MDL1P	DEED LINE, TYPE 1
	MDL2P	DEED LINE, TYPE 2
	MDL3P	DEED LINE, TYPE 3
	MDL4P	DEED LINE, TYPE 4
	MDL5P	DEED LINE, TYPE 5
	MEEP	EASEMENT, EXISTING
	MEPAP_P	EASEMENT, PERM., APPROX.
	MEPP_P	EASEMENT, PERM., BACK LINE
	MEPSP_P	EASEMENT, PERM., SHAPE
	MFAP_P	FEE ACQUISITION, APPROX.
	MFP_P	FEE ACQUISITION, BACK LINE
	MFSP_P	FEE ACQUISITION, SHAPE
	MHBAP	HIGHWAY BNDRY., APPROX.
	MHBCP	HISTORICAL, BLDG. CORNERS
	MHBP	HIGHWAY BNDRY, PT.
	MJCP	PT., JURIS. CITY
	MPBC	PT., BUILDING CORNER
	MPCC	PT., CROSS CUT
	MPDH	PT., DRILL HOLE
	MPF	PT., FENCE LOCATION
	MPIP	PT., IRON PIPE
	MPIR	PT., IRON ROD
	MPM	PT., MONUMENT
	MPMM	PT., MONUMENT, MISC.
	MPN	PT., NAIL
	MPRS	PT., RAILROAD SPIKE
	MPSP	PT., SPIKE
	MPST	PT., STAKE
	MPTW	PT., TREE W/ WIRE
	MPWL	PT., WALL LOCATION

ROW ACQUISITION

M1 P1 FEE	MFS_P_T	FEE ACQUISITION
M1 P1 PE	MEPS_P_T	EASEMENT, PERMANENT
M1 P1 TE	METS_P_T	EASEMENT, TEMPORARY
M1 P1 TO	METS_P_T	OCCUPANCY, TEMPORARY
M1 P1 FEE WO/A	MFS_P_T	FEE ACQUISITION W/O ACCESS

ROADWAY

	RES P	ELEVATION, SPOT
	RGA	GUIDE RAIL, ANCHOR
	RGP	GUIDE POST, SINGLE

SIGNS

CELL	NAME	DESCRIPTION
	S	SINGLE POST
	S_P	SINGLE POST, PROPOSED
	SB_P	BACK TO BACK, PROPOSED
	SDEL	DELINEATORS
	SPM	PARKING METER
RFM	SRM	REFERENCE MARKERS
	SRSC3	SHLD, CTY, 123 DIG.
	SRSC4	SHLD, CTY, 4 DIG.
	SRSCT2	SHLD, CTY TOUR, 1-2 DIG.
	SRSCT4	SHLD, CTY TOUR, 3-4 DIG.
	SRSI	SHLD, INTERSTATE
	SRSN2	SHLD, NATIONAL, 2 DIG.
	SRSN3	SHLD, NATIONAL, 3 DIG.
	SRSS2	SHLD, STATE, 2 DIG.
	SRSS3	SHLD, STATE, 3 DIG.
	SRSS4	SHLD, STATE, 4 DIG.

TRAFFIC CONTROL

	TCBJ	BOX, JUNCTION
	TCBP	BOX, PULL BOX
	TCBS	BOX, SPLICE
	TCMC	MICROCOMPUTER CABINET
	TCPP	PED POLE
	TCSH	SIGNAL HEADS
	TCSP	SIGNAL POLE

TRAFFIC WORK ZONE

	TWZAP_P	ARROW PANEL
	TWZAPC_P	ARROW PANEL, CAUTION MODE
	TWZAPT_P	ARROW PANEL, TRAILER OR SUPPORT
	TWZBCD_P	BARRICADE (TYPE III)
	TWZCMS_P	CHANGEABLE MESSAGE SIGN (PVMS)
	TWZFLG_P	FLAGGER
	TWZFT_P	FLAG TREE
	TWZIA_P	IMPACT ATTENUATOR / CRASH CUSHION (TEMPORARY)
	TWZLUM_P	LUMINAIRE (TEMPORARY)
	TWZSDT_P	SYMBOL, DIRECTION OF TRAFFIC
	TWZSDTD_P	SYMBOL, DIRECTION OF TEMPORARY TRAFFIC DETOUR
	TWZSGN_P	SIGN (TEMPORARY)
	TWZSIG_P	SIGNAL, TRAFFIC OR PEDESTRIAN (TEMPORARY)
	TWZWL_P	WARNING LIGHT
	TWZWV_P	WORK VEHICLE
	TWZWVA_P	WORK VEHICLE WITH TRUCK MOUNTED ATTENUATOR

UTILITIES

CELL	NAME	DESCRIPTION
E	UEB	ELECTRIC, BOX
E	UEM	ELECTRIC, METER
E	UEMH	ELECTRIC, MANHOLE
	UEPT	ELECTRIC, POLE, TRANS.
G	UGM	GAS, METER
G	UGMH	GAS, MANHOLE
G	UGLM	GAS, LINE MARKER
FP	UGP	GAS/FUEL PUMP
	UGV	GAS, VALVE
	UGVT	GAS, VENT
	ULP	LIGHTING, POLE
	ULPM	LIGHTING, POLE, MEDIAN
	ULPP	LIGHTING, POLE, PED.
	UMFC	MISC. FILLER CAP
O	UOLM	OIL, LINE MARKER
	UP	POLE, WITH UTILITY
	UPD	POLE, DEAD (NO UTILITY)
	UPL	POLE, WITH LIGHT
S	USMH	SANITARY SEWER MANHOLE
P	UTB	TELEPHONE, BOOTH
T	UTLM	TELEPHONE, LINE MARKER
T	UTMH	TELEPHONE, MANHOLE
C	UTVLM	CABLE TV, LINE MARKER
C	UTVPB	CABLE TV, PULL BOX
	UUB	UNKNOWN, BOX
	UUJB	UNKNOWN, JUNCTION BOX
	UUMH	UNKNOWN, MANHOLE
	UUPB	UNKNOWN, PULL BOX
	UUVL	UNKNOWN, VALVE
	UUVT	UNKNOWN, VENT
U	UUW	UNKNOWN, WELL
	UWFH	WATER, FIRE HYDRANT
W	UWM	WATER, METER
W	UWMH	WATER, MANHOLE
	UWV	WATER, VALVE
W	UWW	WATER, WELL
CS	UWCS	WATER, CURB STOP

NOTES:

1. THE LEGEND ILLUSTRATES MAPPING FEATURES (EXISTING AND PROPOSED).
2. FEATURES ARE SHOWN AS EITHER LINEAR (ROADWAY GUIDE RAIL, ROADWAY SIDEWALK, UTILITY LINES, ETC.) OR POINT (SIGN, UTILITY POLE, ETC.).
3. FEATURES SHOWN ON THE LEGEND AS EXISTING FEATURES ALSO HAVE CORRESPONDING PROPOSED FEATURES.
4. PROPOSED FEATURE SYMBOLOGY IS IDENTICAL TO EXISTING FEATURE SYMBOLOGY EXCLUDING LINE WEIGHT. LINE WEIGHT FOR PROPOSED FEATURES IS THICKER (0.015 in ON B SIZE DRAWINGS).
5. MAPPING FEATURES NOT INCLUDED ON THE LEGEND SHEET DO NOT HAVE A UNIQUE SYMBOLOGY (SUCH AS THE PAVEMENT EDGE, PAVEMENT EDGE OF TRAVEL WAY) AND SHOULD BE LABELED ON THE PLANS.
6. FEATURES SHOWN AT THE HEAVIER WEIGHT ARE PROPOSED ONLY AND DO NOT HAVE CORRESPONDING EXISTING FEATURES.

FILE NAME = 02b - Legends.dgn
DATE/TIME = 06-MAR-2019 10:54
USER = JMessenger

Lu Engineers
ENVIRONMENTAL • TRANSPORTATION • CIVIL
PROJECT MANAGER: J. OTTMAN
DRAWN BY: J. MESSENGER

Department of Environmental Services
Architecture and Engineering Services
City of Rochester, New York
ASSISTANT MANAGING ENGINEER: LISA Y. REYES
CITY ENGINEER: HOLLY E. BARRETT, P.E.

PROJECT TITLE
SCOTTSVILLE ROAD - ELMWOOD AVENUE IMPROVEMENT PROJECT
(WEST CITY LINE - MT HOPE AVE)

NO.	REVISION	BY	DATE

ISSUED 03/06/2019
CHECKED JWO
DRAWN JRM
DESIGN JRM
SCALE
PROJECT NUMBER 17102

DRAWING TITLE
POINT LEGEND
LEG-1

DRAWING NO. 3
OF 54

ALIGNMENT

STYLE	NAME	DESCRIPTION
	AC	CONTROL (CENTERLINE)
	AD_P	DETOUR
	AT_P	TRANSITION CONTROL

BRIDGE

STYLE	NAME	DESCRIPTION
	BR	RAIL
	BSHT	SHEET PILING

CONTROL

STYLE	NAME	DESCRIPTION
	CB	BASELINE
	CBPR	BASELINE, PROJECTION

DRAINAGE

STYLE	NAME	DESCRIPTION
ST	DCP	CULVERT PIPE
ST	DCP_P	CULVERT PIPE (DIR)
	DDG_P	DITCH, GRASS LINED
	DDP_P	DITCH, PAVED INVERT
	DDS_P	DITCH, STONE LINED
	DFL_P	FLOW LINE
	DSSD	SLOTTED DRAIN
UD	DUD_P	UNDERDRAIN

ENVIRONMENTAL

STYLE	NAME	DESCRIPTION
S	EBLHS	BALE, STRAW
	ECT	CURTAIN, TURBIDITY
	EDMC	DAM, COFFER
	EDMEC_P	DAM, EARTHEN CHECK
	EDMGSC_P	DAM, GRAVEL BAG/SAND BAG CHECK
	EDMPC_P	DAM, PREFABRICATED CHECK
	EDMSC_P	DAM, STONE CHECK
	EFNS	FENCE, SILT
	EFNSV	FENCE, SILT & VEGETATION
	EFNV	FENCE, VEGETATION
AA	EWAA_P	WETLAND, ADJACENT AREA
FW	EWF	WETLAND, FEDERAL
FW SW	EWFS	WETLAND, FEDERAL AND STATE
SW	EWM	WETLAND, MITIGATION AREA
SW	EWS	WETLAND, STATE

LANDSCAPE

STYLE	NAME	DESCRIPTION
	LABL	AREA, BRUSH LINE
	LAHR	AREA, HEDGE ROW
	LAPB	AREA, PLANTING BED
	LAWA	AREA, WOODED AREA OUTLINE
	LAWE	AREA, WATERS EDGE
	LCUT_P	CUT LIMIT
	LFILL_P	FILL LIMIT
	LFNC	FENCE
	LTRC	TREE ROW, CONIFEROUS
	LTRD	TREE ROW, DECIDUOUS

LANDSCAPE

STYLE	NAME	DESCRIPTION
	LWH	WALL, H PILE
	LWR	WALL, RETAINING
	LWS	WALL, STONE

ROW MAPPING

STYLE	NAME	DESCRIPTION
	MDL	DEED LINE
PE	MEE	EASEMENT, EXISTING
PE	MEP_P	EASEMENT, PERMANENT
APE	MEPA_P	EASEMENT, PERMANENT, APPROX.
TE	MET_P	EASEMENT, TEMPORARY
ATE	META_P	EASEMENT. TEMPORARY, APPROX.
FEE	MF_P	FEE ACQUISITION, W/ ACCESS
AFEE	MFA_P	FEE ACQUISITION, APPROXIMATE
	MFS_P	FEE ACQUISITION, SHAPE
FEE W/OA	MFWOA_P	FEE ACQUISITION, W/O ACCESS
	MHA	HISTORICAL, ACQUISITION
HB	MHB	HIGHWAY BOUNDARY
AHB	MHBA	HIGHWAY BOUNDARY, APPROX.
	MHBW	HWY BOUNDARY, FACE OF WALL
HB W/OA	MHBWOA	HIGHWAY BOUNDARY, W/O ACCESS
	MJC	JURISDICTION, CITY
	MJCY	JURISDICTION, COUNTY
	MJHD	JURISDICTION, HISTORIC DISTRICT
	MJLL	JURIS., (GREAT, MILITARY) LOT LINE
	MJN	JURISDICTION, NATION
	MJPB	JURISDICTION, PUBLIC LANDS
	MJS	JURISDICTION, STATE
	MJT	JURISDICTION, TOWN
	MJV	JURISDICTION, VILLAGE
	MPL	PROPERTY LOT LINE
	MPLA	PROPERTY LOT LINE, APPROXIMATE
	MSL	SUB LOT LINE

ROADWAY

STYLE	NAME	DESCRIPTION
CZ	RCZ_P	CLEAR ZONE
	RG	GUIDE RAIL, MISCELLANEOUS
	RGB	GUIDE RAIL, BOX BEAM
	RGBM	GUIDE RAIL, BOX BEAM, MEDIAN
	RGC	GUIDE RAIL, CABLE
	RGCB	GUIDE RAIL, CONCRETE BARRIER
	RGP_P	GUIDE POST
	RGW	GUIDE RAIL, W BEAM
	RGWM	GUIDE RAIL, W BEAM, MEDIAN
	RPB	PARKING BUMPER
	RRC	RAIL ROAD, CATENARY
3R	RRER	RAIL ROAD, 3RD RAIL
	RRPLS_P	RAIL, PHOTO, LARGE SCALE
	RRPSS	RAIL, PHOTO, SMALL SCALE
	RRS	RUMBLE STRIP
	RRSLS_P	RAIL, SURVEY, LARGE SCALE

ROADWAY

STYLE	NAME	DESCRIPTION
	RRSSS	RAIL, SURVEY, SMALL SCALE

SIGNS

STYLE	NAME	DESCRIPTION
	SBLB	BILLBOARDS
	SM	MULTIPLE POST
	SSO	STRUCTURE, OVERHEAD
	SSOC	STRUCTURE, OVHD. CANTILEVER

STRIPING

STYLE	NAME	DESCRIPTION
	STB•	BROKEN LINE
	STDB•	DOUBLE BROKEN LINE
	STDL•	DOTTED LINE LONG
	STDS•	DOTTED LINE SHORT
	STFB•	FULL BARRIER LINE
	STH•	HATCH LINE
	STPB•	PARTIAL BARRIER LINE
	STRCT	ROUNDABOUT, CAT TRACKS
	STRYL	ROUNDABOUT, YIELD LINE
	STSB	STOP BAR
	STSE•	SOLID, EDGE
	STXL	X WALK, LADDER LINE
	STXLB	X WALK, LADDER BAR LINE

• = W (WHITE) OR Y (YELLOW)

TRAFFIC CONTROL

STYLE	NAME	DESCRIPTION
	TCSW	SIGNAL, SPAN WIRE

TRAFFIC WORK ZONE

STYLE	NAME	DESCRIPTION
	TWZBT_P	BARRIER, TEMPORARY
	TWZBTWL_P	BARRIER, TEMPORARY, W/ WARNING LIGHTS
	TWZCD_P	CHANNELIZING DEVICE
	TWZPMRC_P	PAVEMENT MARKING REMOVAL OR COVERING

UTILITIES

STYLE	NAME	
C	UC	CONDUIT, UNDERGROUND
]C[	UCH	CONDUIT, HANGING
OC	UCO	CONDUIT, OVERHEAD
E	UE	ELECTRIC LINE, UNDERGROUND
]E[	UEH	ELECTRIC LINE, HANGING
OE	UEO	ELECTRIC LINE, OVERHEAD
OET	UETO	ELECTRIC TRANSMISSION, OVERHEAD
	UESS	ELECTRIC, SUBSTATIONS
FO	UFO	FIBER OPTIC, UNDERGROUND
]FO[	UFOH	FIBER OPTIC, HANGING
OFO	UFOO	FIBER OPTIC, OVERHEAD
G	UG	GAS, UNDERGROUND
]G[	UGH	GAS, HANGING
OG	UGO	GAS, OVERHEAD
IC	UIC	INFORM CABLE, UNDERGROUND
]IC[	UICH	INFORM CABLE, HANGING
O	UO	OIL LINE, UNDERGROUND
]O[	UOH	OIL LINE, HANGING
	UPBP	POLE, BRACE, PUSH BRACE

UTILITIES

STYLE	NAME	DESCRIPTION
	UPGW	POLE, GUY WIRE
SA	USA	SANITARY SEWER, UNDERGROUND
]SA[	USAH	SANITARY SEWER, HANGING
SAF	USAF	SANITARY SEWER, FORCE MAIN, UGND
]SAF[	USAFH	SANITARY SEWER, FORCE MAIN, HANG
T	UT	TELEPHONE, UNDERGROUND
]T[	UTH	TELEPHONE, HANGING
OT	UTO	TELEPHONE, OVERHEAD
CTV	UTV	CABLE TV, UNDERGROUND
]CTV[	UTVH	CABLE TV, HANGING
OCTV	UTVO	CABLE TV, OVERHEAD
UU	UUU	UNKNOWN, UNDERGROUND
]UU[	UUH	UNKNOWN, HANGING
OUU	UUO	UNKNOWN, OVERHEAD
W	UW	WATER LINE, UNDERGROUND
]W[	UWH	WATER LINE, HANGING
OW	UWO	WATER LINE, OVERHEAD

NOTES:

1. THE LEGEND ILLUSTRATES MAPPING FEATURES (EXISTING AND PROPOSED).
2. FEATURES ARE SHOWN AS EITHER LINEAR (ROADWAY GUIDE RAIL, ROADWAY SIDEWALK, UTILITY LINES, ETC.) OR POINT (SIGN, UTILITY POLE, ETC.).
3. FEATURES SHOWN ON THE LEGEND AS EXISTING FEATURES ALSO HAVE CORRESPONDING PROPOSED FEATURES.
4. PROPOSED FEATURE SYMBOLOGY IS IDENTICAL TO EXISTING FEATURE SYMBOLOGY EXCLUDING LINE WEIGHT. LINE WEIGHT FOR PROPOSED FEATURES IS THICKER (0.015 in ON B SIZE DRAWINGS).
5. MAPPING FEATURES NOT INCLUDED ON THE LEGEND SHEET DO NOT HAVE A UNIQUE SYMBOLOGY (SUCH AS THE PAVEMENT EDGE, PAVEMENT EDGE OF TRAVEL WAY) AND SHOULD BE LABELED ON THE PLANS.
6. FEATURES SHOWN AT THE HEAVIER WEIGHT ARE PROPOSED ONLY AND DO NOT HAVE CORRESPONDING EXISTING FEATURES.

Lu Engineers
ENVIRONMENTAL • TRANSPORTATION • CIVIL
PROJECT MANAGER: J. OTTMAN
DRAWN BY: J. MESSENGER

Department of Environmental Services
Architecture and Engineering Services
City of Rochester, New York
ASSISTANT MANAGING ENGINEER: LISA Y. REYES
CITY ENGINEER: HOLLY E. BARRETT, P.E.

PROJECT TITLE
SCOTTSVILLE ROAD - ELMWOOD AVENUE IMPROVEMENT PROJECT
(WEST CITY LINE - MT HOPE AVE)

NO.	REVISION	BY	DATE

ISSUED 03/06/2019
CHECKED JWO
DRAWN JRM
DESIGN JRM
SCALE
PROJECT NUMBER 17102

DRAWING TITLE
LINE LEGEND
LEG-2

DRAWING NO. 4 OF 54

FILE NAME = 02b-Legends.dgn
DATE/TIME = 06-MAR-2019 10:54
USER = JMessenger

ESTIMATE OF QUANTITIES

ITEM	DESCRIPTION	UNIT	ESTIMATE	FINAL
201.06	CLEARING AND GRUBBING	LS	1.0	
203.02	UNCLASSIFIED EXCAVATION AND DISPOSAL	CY	970.2	
203.03	EMBANKMENT IN PLACE	CY	5.0	
R204.02	MILLING - 2" THICKNESS	SY	35,175.0	
R204.04	MILLING - 4" THICKNESS	SY	6,569.5	
304.15	SUBBASE COURSE, OPTIONAL TYPE	CY	123.0	
402.097203	9.5 F2 TOP COURSE HMA, 70 SERIES COMPACTION	TON	4,683.5	
402.197903	19 F9 BINDER COURSE HMA, 70 SERIES COMPACTION	TON	813.0	
402.198903	19 F9 BINDER COURSE HMA, 80 SERIES COMPACTION	TON	327.9	
402.378903	37.5 F9 BASE COURSE HMA, 80 SERIES COMPACTION	TON	1,184.0	
S406.01	PAVEMENT KEY	LF	893.0	
S407.0201	DILUTED TACK COAT	SY	50,838.9	
S412.07	TEMPORARY PAVEMENT	SF	1,000.0	
418.7603	ASPHALT PAVEMENT JOINT ADHESIVE	LF	29,688.7	
S604.5004	ADJUST EXISTING CATCH BASIN FRAME AND GRATE (INCLUDING EXCAVATION, BACKFILL AND PAVEMENT BASE RESTORATION)	EACH	66.0	
S604.53	CLEAN EXISTING CATCH BASIN AND LATERAL PIPE	EACH	15.0	
S604.60	ADJUST EXISTING SEWER MANHOLE FRAME AND COVER (INCLUDING EXCAVATION, BACKFILL)	EACH	11.0	
S604.65	CLEAN EXISTING MANHOLE	EACH	11.0	
S604.70	SEWER MANHOLE PRECAST CONCRETE FLAT TOP SLAB, 48" DIAMETER	EACH	1.0	
S608.12	CONCRETE SIDEWALK AND DRIVEWAY	CY	124.0	
S608.15	ASPHALT DRIVEWAY - LIGHT DUTY	SF	78.0	
S608.380602	EMBEDDED DETECTABLE WARNING SURFACE	SF	452.0	
S608.380702	EMBEDDED DETECTABLE WARNING SURFACE AT EXISTING LOCATION	SF	76.0	
S609.3005	5" STONE CURB (INCLUDING EXCAVATION, BACKFILL AND PAVEMENT BASE RESTORATION)	LF	969.2	
S609.340101	RESET EXISTING STONE CURB (INCLUDING EXCAVATION, BACKFILL AND PAVEMENT BASE RESTORATION)	LF	48.0	
S610.0602	SEEDING	SF	4,000.0	
S613.03	TOPSOIL	CY	50.0	
S617.01	FENCING FOR TREE PROTECTION	LF	300.0	
619.01	BASIC WORK ZONE TRAFFIC CONTROL	LS	1.0	
619.0901	TEMPORARY PAVEMENT MARKINGS STRIPES (TRAFFIC PAINT)	LF	16,306.0	
619.110511	PORTABLE, VARIABLE MESSAGE SIGN (PVMS) STANDARD SIZE - FULL MATRIX (LED) NO OPTIONAL EQUIPMENT SPEC, NO CELLULAR COM REQ	EACH	2.0	
625.01	SURVEY OPERATIONS	LS	1.0	
S626.07	RESET EXISTING SURVEY MONUMENT FRAME AND COVER	EACH	6.0	
S626.10	REPLACE EXISTING HORIZONTAL CONTROL MONUMENT WITH NEW HORIZONTAL CONTROL MONUMENT (RCS)	EACH	3.0	
627.50140008	CUTTING PAVEMENT	LF	4,671.0	
S637.10	FIELD OFFICE - TYPE A	MNTH	5.0	
646.40	FLEXIBLE DELINEATOR, SINGLE UNIT, ONE WAY ON FLEXIBLE POST	EACH	57.0	
646.42	FLEXIBLE DELINEATOR, DOUBLE UNIT ON FLEXIBLE POST	EACH	27.0	
R655.03	MANHOLE FRAME AND COVER - CASTING	EACH	11.0	
R655.06	TYPE A CATCH BASIN FRAME AND GRATE - FABRICATED	EACH	12.0	
R655.07	TYPE B CATCH BASIN FRAME AND GRATE - FABRICATED	EACH	4.0	
680.54	INDUCTANCE LOOP INSTALLATION	LF	4,344.0	
680.72	INDUCTANCE LOOP WIRE	LF	10,794.0	
685.11	WHITE EPOXY REFLECTORIZED PAVEMENT STRIPES - 20 MILS	LF	7,883.0	
685.12	YELLOW EPOXY REFLECTORIZED PAVEMENT STRIPES - 20 MILS	LF	8,423.0	
686.01000011	WHITE PREFORMED THERMOPLASTIC REFLECTORIZED PAVEMENT STRIPES	LF	15,073.5	
686.03000011	WHITE PREFORMED THERMOPLASTIC REFLECTORIZED PAVEMENT LETTERS	EACH	16.0	
686.04000011	WHITE PREFORMED THERMOPLASTIC REFLECTORIZED PAVEMENT SYMBOLS	EACH	48.0	
697.03	FIELD CHANGE PAYMENT	DC	100,000.0	
698.04	ASPHALT PRICE ADJUSTMENT	DC	100.0	
698.05	FUEL PRICE ADJUSTMENT	DC	100.0	
699.040001	MOBILIZATION (4% MAXIMUM)	LS	1.0	
S909.09	ADJUST EXISTING WATER VALVE BOX TO GRADE - EXTENSION ADJUSTMENT (INCLUDING EXCAVATION AND BACKFILL AND SURFACE RESTORATION)	EACH	39.0	
S912.030100	NEW 1" WATER SERVICE TAP AT WATER MAIN, CORPORATION STOP AND CONNECTION (INCLUDING EXCAVATION, BACKFILL AND SURFACE RESTORATION)	EACH	37.0	
S913.130100	NEW 1" POLYETHYLENE OR CROSS-LINKED POLYETHYLENE WATER SERVICE (INCLUDING EXCAVATION, BACKFILL AND SURFACE RESTORATION)	LF	923.5	
S914.030100	FURNISH AND INSTALL NEW 1" CURB STOP AND BOX AT NEW WATER SERVICE (INCLUDING EXCAVATION, BACKFILL AND SURFACE RESTORATION)	EACH	37.0	
S966.0332	32 POUND MAGNESIUM ANODE	EACH	37.0	
S999.0101	PROJECT SIGN - 6' X 4'	EACH	4.0	

FILE NAME = Estimate of Quantities.dgn
DATE/TIME = 18-MAR-2019 10:18
USER = JMessenger

Lu Engineers
ENVIRONMENTAL • TRANSPORTATION • CIVIL

PROJECT MANAGER: J. OTTMAN
DRAWN BY: J. MESSENGER

Department of Environmental Services
Architecture and Engineering Services
City of Rochester, New York

ASSISTANT MANAGING ENGINEER: LISA Y. REYES
CITY ENGINEER: HOLLY E. BARRETT, P.E.

PROJECT TITLE
SCOTTSVILLE ROAD - ELMWOOD AVENUE
IMPROVEMENT PROJECT
(WEST CITY LINE - MT HOPE AVE)

NO.	REVISION	BY	DATE

ISSUED 03/06/2019
CHECKED JWO
DRAWN JRM
DESIGN JRM
SCALE NONE
PROJECT NUMBER 17102

DRAWING TITLE
ESTIMATE OF QUANTITIES
EQ-1

DRAWING NO.
5
OF
54

DEFINITIONS:

THE WORDS "SHALL", "SHOULD", AND "MAY", AS USED IN THE CONTRACT DRAWINGS, HAVE THE FOLLOWING MEANINGS:

SHALL - A MANDATORY CONDITION. IN THE DESIGN, APPLICATION, OR LOCATION OF DEVICES. REQUIREMENTS HAVING "SHALL" STIPULATIONS ARE MANDATORY. NO DISCRETION IN FOLLOWING THEM IS ALLOWED.

SHOULD - AN ADVISORY CONDITION, WHERE "SHOULD" IS USED IN RELATION TO PROVISIONS, IT IS RECOMMENDED, AND NORMALLY IS TO BE FOLLOWED, BUT IS NOT MANDATORY. DEVIATION FROM SUCH PROVISIONS IS PERMISSIBLE IF, AND TO THE EXTENT, THERE IS JUSTIFIABLE CAUSE TO DO SO.

MAY - A PERMISSIVE CONDITION. NO REQUIREMENT FOR DESIGN OR APPLICATION IS INTENDED.

GENERAL NOTES:

1. THE CONTRACTOR SHALL FOLLOW THE PROVISIONS OF THE CITY OF ROCHESTER STANDARD SPECIFICATIONS, EXCEPT FOR THOSE AS REVISED IN SUPPLEMENTARY SECTION OF CONTRACT DOCUMENTS. REVISED SPECIFICATION PROVIDED IN SUPPLEMENTARY SECTION OF CONTRACT DOCUMENTS SUPERSEDES APPROPRIATE STANDARD SPECIFICATIONS.

2. THE CONTRACTOR IS TO USE CITY OF ROCHESTER STANDARD DETAILS, EXCEPT FOR THOSE PROVIDED IN THE CONTRACT DOCUMENTS. DETAILS PROVIDED IN CONTRACT DOCUMENTS SUPERSEDE APPROPRIATE STANDARD DETAIL.

3. THE CONTRACTOR'S ATTENTION IS DIRECTED TO THE FACT THAT, DUE TO THE NATURE OF THIS PROJECT, THE EXACT EXTENT OF PAVEMENT REPAIR WORK CANNOT ALWAYS BE ACCURATELY DETERMINED PRIOR TO THE COMMENCEMENT OF WORK. THESE CONTRACT DOCUMENTS HAVE BEEN PREPARED BASED ON FIELD INSPECTION, PAVEMENT CORES AND OTHER INFORMATION AVAILABLE AT THE TIME. ACTUAL FIELD CONDITIONS MAY REQUIRE MODIFICATIONS TO CONSTRUCTION DETAILS AND WORK QUANTITIES. THE CONTRACTOR SHALL PERFORM THE WORK IN ACCORDANCE WITH THE FIELD CONDITIONS.

4. THE CONTRACTOR SHALL PERFORM ALL WORK WITH CARE SO THAT ANY MATERIALS WHICH ARE TO REMAIN IN PLACE OR WHICH ARE TO REMAIN THE PROPERTY OF THE CITY, WILL NOT BE DAMAGED. IF THE CONTRACTOR DAMAGES ANY MATERIALS WHICH ARE TO REMAIN IN PLACE, OR WHICH ARE TO REMAIN THE PROPERTY OF THE CITY, THE DAMAGED MATERIALS SHALL BE REPAIRED OR REPLACED IN A MANNER SATISFACTORY TO THE RESIDENT PROJECT REPRESENTATIVE (R.P.R.) AT THE EXPENSE OF THE CONTRACTOR ANY SALVAGED MATERIAL, AS DETERMINED BY THE R.P.R., SHALL BE TRANSPORTED, AT THE CONTRACTOR'S EXPENSE, TO A LOCATION SPECIFIED BY THE CITY. THE MATERIALS SHALL BE UNLOADED UNDER THE DIRECTION OF A REPRESENTATIVE OF THE DEPARTMENT OF ENVIRONMENTAL SERVICES.

5. ALL AREAS DISTURBED BY THE CONTRACTOR'S OPERATION NOT DESIGNATED FOR OTHER TREATMENT SHALL BE FINE GRADED, TOP SOILED AND SEEDED. FINE GRADING SHALL BE SHAPED TO ALLOW FOR SURFACE DRAINAGE AND RETURNED TO ITS ORIGINAL CONDITION, AT THE CONTRACTOR'S EXPENSE.

6. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO CONFIRM DIMENSIONS IN THE FIELD PRIOR TO ANY NEW CONSTRUCTION OR REPLACEMENT.

7. WORK LIMITS ARE SHOWN ON THE PLAN DRAWINGS. THE ACTUAL LIMITS OF WORK SHALL BE CONFIRMED IN THE FIELD AT THE TIME OF CONSTRUCTION. THE R.P.R. SHALL DEFINE SPECIFIC LIMITS WHERE QUESTIONS ARISE.

8. IN ACCORDANCE WITH ITEM 619.01, BASIC WORK ZONE TRAFFIC CONTROL, THE CONTRACTOR SHALL MAINTAIN DRAINAGE FACILITIES AND OTHER HIGHWAY ELEMENTS, OLD OR NEW. THE CONTRACTOR SHALL DEVOTE PARTICULAR ATTENTION TO ALL DRAINAGE FACILITIES KEEPING THEM FULLY OPERATIVE AT ALL TIMES.

9. THE CONTRACTOR SHALL COORDINATE ALL GENERAL CONSTRUCTION ACTIVITIES AND ANY ROAD CLOSURES WITH THE CITY OF ROCHESTER FIRE DEPARTMENT, THE CITY OF ROCHESTER POLICE DEPARTMENT, ROCHESTER CITY SCHOOL DISTRICT AND THE MONROE COUNTY DEPARTMENT OF TRANSPORTATION.

10. THE CITY SHALL OBTAIN ALL APPLICABLE STATE AND FEDERAL PERMITS UNLESS OTHERWISE SPECIFIED IN THE CONTRACT DOCUMENTS. HOWEVER, THE CONTRACTOR SHALL AT ITS OWN EXPENSE, OBTAIN ALL NECESSARY CITY PERMITS. THESE MAY INCLUDE, BUT SHALL NOT BE LIMITED TO ZONING, FILL, BLASTING, HAULING, DISPOSAL, SIGN, UTILITY CONNECTION, AND BUILDING PERMITS. THE CONTRACTOR SHALL SUBMIT COPIES OF ALL LOCAL PERMITS AND LICENSES TO THE RESIDENT PROJECT REPRESENTATIVE.

11. MISCELLANEOUS MINOR CONSTRUCTION REMOVALS, SUCH AS PRIVATE SIGN POST, LIGHT POLE AND BASE REMOVAL AND DISPOSAL, SHALL BE INCLUDED IN THE COST OF ITEM 201.06, CLEARING AND GRUBBING. ITEMS SUCH AS SIGNS, SIGN POST, LIGHT POLES, FENCES, ETC., MAY AT THE PROPERTY/HOME OWNER'S OPTION BE PLACED ON THE OWNER'S PROPERTY ADJACENT TO THE WORK FOR THE OWNER'S DISPOSITION.

12. THE CONTRACTOR SHALL NOT KEEP AREA RESIDENTS FROM USING THEIR DRIVEWAYS FOR MORE THAN THREE (3) DAYS, EXCEPT FOR THE FOLLOWING: PLACEMENT OF CONCRETE DRIVEWAYS AND PLACEMENT OF CONCRETE SIDEWALKS ACROSS DRIVEWAYS FOR WHICH THE CONTRACTOR MAY KEEP AREA RESIDENTS FROM USING THEIR DRIVEWAYS FOR NO MORE THAN SEVEN (7) DAYS DUE TO REQUIRED CONCRETE CURING TIME. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO NOTIFY HOMEOWNERS IN WRITING WHEN ACCESS TO THEIR RESIDENCES WILL NOT BE POSSIBLE AT LEAST 24 HOURS IN ADVANCE.

GENERAL NOTES (CONTINUED):

13. THE CONTRACTOR SHALL BE REQUIRED TO PROTECT WORKERS AT ALL TIMES IN CONFORMANCE WITH APPLICABLE OSHA REGULATIONS. ANY PERMITS REQUIRED FOR WORKING IN CONFINED AREAS WILL BE THE RESPONSIBILITY OF THE CONTRACTOR TO OBTAIN.

14. CONTRACTOR SHALL CONTACT MCDOT DISPATCHER AT (585) 753-7750 A MINIMUM OF FIVE (5) WORKING DAYS PRIOR TO REMOVING OR RESETTING ANY TRAFFIC AND PARKING REGULATION SIGNS.

15. THE ROADWAY SHALL BE KEPT CLEAR OF MUD AND DEBRIS AT ALL TIMES.

16. WHENEVER ITEMS IN THE CONTRACT REQUIRE MATERIALS TO BE REMOVED AND DISPOSED OF, THE COST OF SUPPLYING A DISPOSAL AREA AND TRANSPORTATION TO THAT AREA SHALL BE INCLUDED IN THE UNIT PRICES BID FOR THOSE ITEMS.

17. ALL DISTURBED AREAS ADJACENT TO CURBING, DRIVEWAY AND SIDEWALK INSTALLATION SHALL BE RESTORED BY THE TREATMENT SHOWN ON THE PLANS BY TOPSOIL AND SEEDING OR AS ORDERED BY THE ENGINEER.

18. THE CONTRACTOR IS TO OBTAIN WORK RELEASES FROM THE RESPECTIVE PROPERTY OWNER WHERE WORK IS TO BE PERFORMED OUTSIDE THE PUBLIC RIGHT-OF-WAY, ON PRIVAELY OWNED PROPERTY. A COPY OF THE WORK RELEASE IS TO BE FILED WITH THE PROJECT MANAGER.

PAVEMENT NOTES:

1. PRIOR TO BEGINNING PAVING WORK, A PRE-PAVE MEETING WILL BE REQUIRED. THE CONTRACTOR SHALL NOTIFY THE CITY PROJECT REPRESENTATIVE AT LEAST THREE (3) WEEKS IN ADVANCE OF THE START OF PAVING OPERATIONS IN ORDER TO SCHEDULE A MEETING WITH THE CITY, THE CONTRACTOR, THE ASPHALT SUPPLIER AND THE DESIGN ENGINEER.

2. WHEN MATCHING INTO EXISTING PAVEMENT GRADES, THE CONTRACTOR SHALL SAW CUT THE EXISTING PAVEMENT WITH A NEAT, STRAIGHT VERTICAL EDGE. THE CONTRACTOR SHALL WARP NEW PAVEMENT GRADES TO MATCH EXISTING PAVEMENT GRADES, SAW CUT A MINIMUM OF 2" IN DEPTH.

3. ALL JOINTS BETWEEN EXISTING AND NEW ASPHALT SURFACES SHALL BE SEALED WITH ITEM 418.7603, ASPHALT PAVEMENT JOINT ADHESIVE.

4. SAW CUTS MADE IN THE ROADWAY ARE TO BE FULL DEPTH, WITH ALL OTHER SAW CUTS BEING A MINIMUM OF 2" IN DEPTH. SAW CUTS ARE TO BE MADE WHERE REQUIRED ON THE PLANS AND AS SHOWN ON THE DETAILS. THE COST OF ALL SAW CUTS ARE TO BE INCLUDED IN THE RESPECTIVE WORK RELATED ITEMS). ALL SAW CUTS WITHIN THE PAVEMENT ARE TO BE SEALED WITH ITEM 418.7603, ASPHALT PAVEMENT JOINT ADHESIVE.

5. WHERE SIDEWALKS, DRIVEWAYS, AND OTHER PAVED AREAS ARE NOTED TO BE REMOVED ON THE PLANS, THE PAYMENT SHALL BE MADE BY THE RESPECTIVE ITEMS OR UNCLASSIFIED EXCAVATION, TOPSOIL INSTALLATION, AND SEEDING OR OTHER DESIGNATED SURFACE TREATMENT.

6. ALL BACKFILL MATERIAL, AND PAVEMENT SUBBASE MATERIAL SHALL BE PLACED IN 6" UNCOMPACTED LIFTS AND THOROUGHLY COMPACTED TO 95% OF MAXIMUM DENSITY AT OPTIMUM MOISTURE CONTENT AS DETERMINED BY ASTM D-1557. THE PROCTOR VALUE AND OPTIMAL MOISTURE CONTENT OF THE APPROVED SUBBASE MATERIAL WILL BE OBTAINED BY THE CONTRACTOR AND PROVIDED TO THE R.P.R. IN A TIMELY MANNER AT NO ADDITIONAL COST.

7. CONTAMINATION OF THE SUBBASE PRIOR TO ASPHALT, CURB, AND SIDEWALK CONSTRUCTION WILL BE REPLACED AT THE CONTRACTOR'S EXPENSE.

8. ALL LOCATIONS REFERRED TO BY STATION UNDER THIS CONTRACT ARE APPROXIMATE. THE RESIDENT PROJECT REPRESENTATIVE SHALL DETERMINE THE EXACT LOCATION AND BEGIN AND END POINTS FOR ALL ITEMS OF WORK ASSOCIATED WITH THIS CONTRACT.

9. PAVEMENT CORES HAVE BEEN TAKEN AT VARIOUS LOCATIONS ALONG THE PROJECT STREETS AS IDENTIFIED ON THE ROADWAY PLANS. PAVEMENT CORE INFORMATION IS AVAILABLE AS SUPPLEMENTAL INFORMATION TO BIDDERS AND IS INCLUDED IN THE CONTRACT BOOK.

10. FULL DEPTH SAWCUTTING SHALL BE REQUIRED FOR ALL TRENCH AND EXCAVATION WORK IN PAVEMENT REHABILITATION AREAS, SIDE STREET WORK LIMITS AND PROJECT WORK LIMITS. THE COST FOR ALL SAWCUTTING SHALL BE INCLUDED IN THE RESPECTIVE ITEMS.

11. THE CONTRACTOR SHALL NOT DRIVE "NON-RUBBER TIRE" EQUIPMENT (MILLING MACHINES, STEEL DRUM ROLLERS, ETC) ACROSS BRIDGE DECKS, CULVERTS, AND THE FINISHED PAVEMENT SURFACE UNLESS PROPER PROTECTION IS PROVIDED TO THE SATISFACTION OF THE R.P.R.

12. THE COST TO FURNISH AND APPLY TACK COAT ON PAVEMENT EDGES, CURBING, MANHOLES AND STRUCTURES SHALL BE INCLUDED WITH RESPECTIVE ITEMS.

13. THE CONTRACTOR IS ADVISED THAT THE HOT MIX ASPHALT PAVEMENT COURSES CONSTRUCTED ON THIS PROJECT WILL BE DEVELOPED USING THE SUPERPAVE MIX DESIGN PROCEDURES AND SPECIFICATIONS. TO AVOID DAMAGE TO THE EXISTING UTILITIES, VIBRATORY COMPACTION WILL NOT BE PERMITTED HOWEVER, THE CONTRACTOR WILL BE RESPONSIBLE FOR COMPACTING THE PAVEMENT WITHIN SPECIFIED DENSITY RANGE PER THE SUPERPAVE SPECIFICATIONS INCLUDED IN THE CONTRACT DOCUMENTS.

SPECIAL NOTE- PG BINDER AND MIX DESIGN LEVEL

REQUIREMENTS OF THIS NOTE APPLY TO ALL SECTION 402 HOT MIX ASPHALT (HMA) ITEMS IN THIS CONTRACT.

1. PG BINDER- USE A PG 64S-22 (STANDARD) MEETING THE REQUIREMENTS OF AASHTO M332. STANDARD SPECIFICATION FOR PERFORMANCE GRADED ASPHALT BINDER USING MULTIPLE STRESS CREEP RECOVERY (MSCR) FOR THE PRODUCTION OF HMA MIXTURES FOR THIS PROJECT

2. PPA PROHIBITION- USE OF POLYPHOSPHORIC ACID (PPA) TO MODIFY THE PG BINDER PROPERTIES IS PROHIBITED. THIS PROHIBITION ALSO APPLIES TO THE USE OF PPA AS A CROSS- LINKING AGENT FOR POLYMER MODIFICATION.

3. MIX DESIGN- THE MIXTURE DESIGNS MUST BE DEVELOPED IN ACCORDANCE WITH THE CRITERIA SPECIFIED IN THE HMA ITEMS THAT ARE APPROPRIATE FOR AN "ESTIMATED TRAFFIC" LEVEL OF LESS THAN 30 MILLION ESALS.

THE PG BINDER FOR THIS PROJECT MAY BE MODIFIED WITH CRM ADDITIVES TO MEET THE REQUIREMENTS STATED ABOVE. HANDLING OF THE HMA SHALL BE DISCUSSED AT PRE-CONSTRUCTION AND PRE-PAVE MEETINGS.

GRANITE CURBING, CONCRETE SIDEWALK AND DRIVEWAY NOTES:

1. NEW GRANITE CURB SHALL TRANSITION WITHIN THE LAST 3 FEET TO MEET EXISTING TOP OF CURB.

2. ITEM S608.12 CONCRETE SIDEWALKS AND DRIVEWAYS ARE TO BE GENERALLY PLACED IN LENGTHS THAT HAVE CONTROL JOINTS THAT CREATE 5 FOOT BY 5 FOOT BLOCKS. AT STREET INTERSECTIONS THE SIDEWALK IS IRREGULARLY SHAPED TO CONFORM TO THE CORNER RADII, AT THESE AND ANY OTHER SIMILAR LOCATIONS AND WHERE UTILITY COVERS PENETRATE THE SIDEWALK. ADDITIONAL CONTROL JOINTS SHALL BE PLACE TO CONTROL CRACKING. ADDITIONAL CONTROL JOINT LOCATIONS ARE TO BE DETERMINED BY THE CONTRACTOR. ANY NEW SIDEWALK THAT CRACKS SHALL BE REPLACED AT THE CONTRACTORS EXPENSE. ANY SAWCUTTING NECESSARY TO INSTALL SIDEWALK SHALL BE INCLUDE IN THE PRICE BID FOR ITEM S608.12.

TRAFFIC SIGNAL NOTES:

1. THE CONTRACTOR SHALL CONTACT THE MONROE COUNTY DEPARTMENT OF TRANSPORTATION TRAFFIC SIGNAL GROUP (753-7772) A MINIMUM OF TWO WEEKS PRIOR TO THE REMOVAL OF TRAFFIC SIGNAL LOOPS.

2. ALL WORK PERFORMED AND MATERIALS SUPPLIED SHALL BE IN ACCORDANCE WITH THE LATEST CITY OF ROCHESTER AND NYSDOT STANDARD SPECIFICATIONS AND DETAILS INCLUDING ALL REVISIONS, AND THE MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES.

3. THE CONTRACTOR SHALL IDENTIFY EACH AND EVERY CABLE ENTERING THE CONTROLLER CABINETS AND PULL BOXES BY FUNCTION AND DESTINATION ON MCDOT APPROVED TAGS. THE TAGS SHALL BE ATTACHED TO THE CABLES WITH STAINLESS STEEL WIRE, PLASTIC OR NYLON LINE.

4. ALL NEW LOOPS SHALL BE CENTERED IN THE TRAFFIC LANE AND THE PROPOSED LOCATIONS SHALL BE APPROVED BY MCDOT PRIOR TO INSTALLATION.

5. AT INTERSECTIONS WHERE TRAFFIC SIGNALS ARE BEING MODIFIED (I.E. DETECTOR LOOP REPLACEMENT) SIGNAL CONTROLLER SHALL BE PLACED IN RECALL MODE WHILE DETECTION IS DISABLED.

6. AT INTERSECTIONS WHERE WORK REQUIRES FLAGGERS TO CONTROL TRAFFIC, SIGNALS SHALL BE PLACED IN FLASHING RED MODE IN ALL DIRECTIONS.

7. THE CONTRACTOR SHALL CONTACT MONROE COUNTY TRAFFIC SIGNAL MAINTENANCE (ERNIE FATTORE (585) 753-7772) 48 HOURS IN ADVANCE OF THE NEED TO MODIFY ANY TRAFFIC SIGNAL OPERATION.

TRAFFIC NOTES:

1. THE CONTRACTOR SHALL BE RESPONSIBLE TO MAINTAIN THE CONSTRUCTION SITE IN A SAFE AND ORDERLY FASHION THROUGHOUT THE PERIOD OF CONSTRUCTION WITH MINIMAL DISRUPTION OF THROUGH TRAFFIC AND INGRESS AND EGRESS FOR ALL PRIVATE DRIVEWAYS, PUBLIC STREETS, AND ENTRANCES TO BUSINESSES ALONG THE ROUTE OF CONSTRUCTION.

2. ALL SIGNS AND POSTS REMOVED SHALL BECOME THE PROPERTY OF THE CITY AT NO COST TO THE OWNER. THE CONTRACTOR SHALL STOCKPILE THIS MATERIAL ON-SITE IN A SECURE LOCATION AND CONTACT THE R.P.R. TO ARRANGE FOR A PICK-UP BY THE CITY.

ROADWAY PROFILE NOTES:

1. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ALL FIELD SURVEY REQUIRED TO MAINTAIN LINE AND GRADE. PAYMENT SHALL BE MADE UNDER ITEM 625.01, SURVEY OPERATIONS.

2. NO DESIGN PROFILE IS PROVIDED. THE DESIGN INTENT IS TO MATCH EXISTING GRADE AFTER MILL AND OVERLAY OPERATIONS ARE COMPLETE.

PROJECT STATIONING NOTES:

1. THE PROJECT PLANS HAVE BEEN DEVELOPED USING DIGITAL ORTHO IMAGERY. HORIZONTAL AND VERTICAL SURVEY CONTROL HAS NOT BEEN DEVELOPED FOR THIS PROJECT AS WORK IS GENERALLY TO BE PERFORMED IN THE STREET FROM CURB TO CURB.

2. STATION LINES FOR LOCATING PROJECT WORK HAVE BEEN ESTABLISHED ALONG THE CENTERLINE OF EACH STREET AND ARE SHOWN OF THE PLAN DRAWINGS. ALL STATIONING IS APPROXIMATE. THESE STATION LINES ARE NOT REFERENCED TO ANY COORDINATE SYSTEM. THE CONTRACTOR SHALL BE RESPONSIBLE FOR RE-ESTABLISHING THESE STATION LINES IN THE FIELD AND SHALL BE PAID FOR THIS WORK UNDER ITEM 625.01, SURVEY OPERATIONS.

SURVEY NOTES:

1. THE LOCATIONS OF PROPERTY LINES SHOWN ON THIS SET OF PLANS ARE APPROXIMATE. THE PROPERTY LINE LOCATIONS WERE DETERMINED FROM PLAT AND TAX MAP RECORDS.

2. ANY SURVEY MARKERS DEFINING THE CONSTRUCTION BASELINES OR BENCHMARKS WHICH ARE DISTURBED BY CONSTRUCTION OPERATIONS OR NEED TO BE ADJUSTED TO THE PROPOSED GRADE SHALL BE PROPERLY TIED AND ACCURATELY RESET UPON COMPLETION OF WORK. PAYMENT SHALL BE INCLUDED UNDER ITEM 625.01, SURVEY OPERATIONS.

MONUMENT NOTES:

1. ALL CONSTRUCTION ACTIVITIES SHALL BE PERFORMED SO AS NOT TO DISTURB RCS MONUMENTS.

2. DETECTABLE WARNING UNITS SHALL NOT BE INSTALLED IN CONFLICT WITH ANY RCS MONUMENTS

3. THE CITY OF ROCHESTER MAPS AND SURVEYS DIVISON SHALL BE NOTIFIED ABOUT GRADE ISSUES WHEN IN CONFLICT WITH THE PLANS AT (585) 428-6667.

4. EXPECTED MONUMENT WORK ON THIS PROJECT INCLUDES RESETTING EXISTING MONUMENT FRAMES AND COVERS TO FINISHED GRADE AT SPECIFIED LOCATIONS. ALL OTHER CITY OF ROCHESTER MONUMENTATION SHALL BE PROTECTED DURING CONSTRUCTION.

5. ALL RIGHT-OF-WAY MONUMENTS AND PROPERTY CORNERS ARE TO BE SAFEGUARDED AND PRESERVED. PRIOR TO START OF CONSTRUCTION, THE CONTRACTOR SHALL LOCATE AND SUBMIT A CONDITION INVENTORY OF ALL MONUMENTS AND SURVEY CONTROL WITHIN THE PROJECT LIMITS. ALL RIGHT-OF-WAY MONUMENTS OR PROPERTY CORNERS THAT ARE DISTURBED BY THE CONTRACTOR DURING CONSTRCTION ARE TO BE REPLACED BY A N.Y.S. LICENSED SURVEYOR AT THE CONTRACTORS EXPENSE.

RECORD DRAWING NOTES:

RECORD DRAWINGS ARE AVAILABLE THIS PROJECT. THEY ARE INCLUDED IN THE SUPPLEMENTAL INFORMATION WITHIN THE BID DOCUMENTS.

SPECIFICATIONS:

THE CONTRACTOR WILL BE REQUIRED TO OBTAIN A COPY OF THE CITY OF ROCHESTER DEPARTMENT OF ENVIRONMENTAL SERVICES STANDARD CONSTRUCTION CONTRACT DOCUMENTS.

NYSDOT STANDARD SHEETS:

203-01, 203-05, 209-01, 209-03, 209-05, 604-02, 608-01, 608-03, 609-02, 609-03, 625-01, 645-01, 645-02, 645-03, 655-03, 655-04, 685-01

Lu Engineers
ENVIRONMENTAL • TRANSPORTATION • CIVIL
PROJECT MANAGER: J. OTTMAN
DRAWN BY: J. MESSENGER

Department of Environmental Services
Architecture and Engineering Services
City of Rochester, New York
ASSISTANT MANAGING ENGINEER: LISA Y. REYES
CITY ENGINEER: HOLLY E. BARRETT, P.E.

PROJECT TITLE
SCOTTSVILLE ROAD - ELMWOOD AVENUE
IMPROVEMENT PROJECT
(WEST CITY LINE - MT HOPE AVE)

NO.	REVISION	BY	DATE

ISSUED	03/06/2019
CHECKED	JWO
DRAWN	JRM
DESIGN	JRM
SCALE	NONE
PROJECT NUMBER	17102

DRAWING TITLE
GENERAL NOTES
GN-1

DRAWING NO.
6
OF
54

FILE NAME = General Notes.dgn
DATE/TIME = 18-MAR-2019 10:21
USER = JMessenger

UTILITY NOTES:

1. THE CONTRACTOR SHALL BE REQUIRED TO COORDINATE ALL CONSTRUCTION ACTIVITIES WITH ANY AFFECTED UTILITY COMPANIES.

2. EXISTING UTILITIES (LOCATIONS, SIZES AND INVERTS) SHOWN ON THE PLANS HAVE BEEN PLOTTED FROM THE FIELD SURVEYS AND RECORD MAPS AND ARE NOT CERTIFIED AS TO THE ACCURACY OF THEIR LOCATION OR COMPLETENESS. THE CONTRACTOR SHALL BE RESPONSIBLE FOR DETERMINING THE EXACT LOCATIONS AND DEPTHS OF ALL UTILITIES AND STRUCTURES IN THE PATH OF OR CLOSELY PARALLEL TO OR UNDER THE PROPOSED CONSTRUCTION. IT IS THE CONTRACTOR'S RESPONSIBILITY TO NOTIFY THE VARIOUS UTILITY OWNERS IN AMPLE TIME FOR THEM TO LOCATE AND MARK THEIR FACILITIES. THE CONTRACTOR SHALL ALSO NOTIFY CONTROL STAKEOUT AT LEAST 48 HOURS IN ADVANCE OF COMMENCING ANY WORK (DIG SAFELY NEW YORK TELEPHONE NO. 1-800-962-7962 OR 811 OR WEB SITE WWW.DIGSAFELYNEWYORK.COM) AND ANY UTILITIES NOT SERVED BY DIG SAFELY/NEW YORK.

3. THE LOCATION, DEPTHS AND DATA AS TO UNDERGROUND CONDITIONS HAVE BEEN OBTAINED FROM RECORDS, SURFACE INDICATIONS AND DATA FURNISHED BY OTHERS.

4. THE APPROXIMATE LOCATION OF THE UNDERGROUND UTILITIES ARE SHOWN ON THE PLANS. THE CONTRACTOR SHALL VERIFY THE TRUE LOCATION BEFORE COMMENCING WORK. BEFORE ANY PIPE IS PLACED, THE CONTRACTOR SHALL UNCOVER ALL UTILITIES AT PIPE CROSSINGS TO ENABLE THE RESIDENT PROJECT REPRESENTATIVE TO VERIFY THE PROPOSED PIPE WITH GRADES SHOWN ON THE PLANS IS NOT OBSTRUCTED BY EXISTING UTILITIES. THE COST OF THIS WORK SHALL BE INCLUDED IN THE PRICE BID FOR VARIOUS ITEMS IN THE CONTRACT.

5. THE CONTRACTOR SHALL TAKE ALL PRECAUTIONS NECESSARY TO PRESERVE THE INTEGRITY OF EXISTING UTILITIES TO REMAIN AND SHALL PROVIDE UNINTERRUPTED SERVICE TO ALL USERS OF THE EXISTING UTILITIES. EXISTING UTILITIES TO REMAIN, ENCOUNTERED IN TRENCH OR PAVEMENT RECONSTRUCTION EXCAVATIONS, SHALL BE SUPPORTED AS NECESSARY OR AS DIRECTED BY THE RESIDENT PROJECT REPRESENTATIVE. THE COST FOR SUPPORTING UTILITIES SHALL BE INCLUDED IN THE PRICE BID FOR EXCAVATION ITEMS.

6. DURING UTILITY RELOCATION WORK REQUIRED FOR THIS CONTRACT, THE CONTRACTOR SHALL COOPERATE IN EVERY WAY WITH THE UTILITY OWNER, AND SHALL SCHEDULE WORK IN SUCH A WAY AS TO COMPLY WITH SHUT DOWN TIME AND ANY OTHER REQUIREMENTS OF THE UTILITY OWNER. NO ADDITIONAL PAYMENT SHALL BE MADE FOR ANY COST INCURRED DUE TO COMPLIANCE WITH UTILITY OWNER'S REQUIREMENTS. SUCH COSTS SHALL BE INCLUDED IN THE PRICE BID FOR VARIOUS ITEMS IN THE CONTRACT.

7. ADJUSTMENT, RELOCATION OR REPLACEMENT OF EXISTING TELEPHONE, ELECTRIC, GAS, CABLE TELEVISION, AND OTHER PRIVATE UTILITIES SHALL BE DONE BY OTHERS.

8. THE CONTRACTOR SHALL VERIFY THAT THE EXISTING CATCH BASIN LATERALS, WHICH ARE TO REMAIN IN USE, ARE OPERABLE BY CLEANING AND FLUSHING THE LATERALS. THE COST SHALL BE INCLUDED IN THE VARIOUS DRAINAGE ITEMS.

9. UNLESS OTHERWISE NOTED, ALL EXISTING CATCH BASINS, MANHOLES, AND UTILITY VALVES SHALL REMAIN UNDISTURBED AT EXISTING GRADE. THE CONTRACTOR SHALL EXERCISE CARE WHEN MILLING AROUND FRAMES AND COVERS. THE CONTRACTOR SHALL REPAIR OR REPLACE VALVES AND COVERS DAMAGE BY THE CONTRACTORS ACTIONS AT NO COST TO THE OWNER.

10. THESE PLANS HAVE A QUALITY LEVEL C (QLC).

    QUALITY LEVEL C (QLC):
    QUALITY LEVEL C IS THE THIRD HIGHEST DEGREE OF ACCURACY. THE INFORMATION SHOWN ON THE PLANS HAS BEEN OBTAINED BY SURVEYING AND PLOTTING VISIBLE ABOVE-GROUND UTILITY FEATURES AND BY USING PROFESSIONAL JUDGMENT IN CORRELATING THIS INFORMATION TO QUALITY LEVEL D INFORMATION. (SHOWN AS QLC)

    QUALITY LEVEL D (QLD):
    QUALITY LEVEL D IS THE LOWEST DEGREE OF ACCURACY. THE INFORMATION SHOWN ON THE PLANS WAS DERIVED SOLELY FROM EXISTING NYSDOT AND/OR UTILITY COMPANY RECORDS OR RECOLLECTIONS. (SHOWN AS QLD)

11. THE CONTRACTOR SHALL ESTABLISH AND MAINTAIN COMMUNICATION WITH AND COORDINATE WORK WITH THE FOLLOWING UTILITY COMPANIES AND BUSINESSES FOR THE DURATION OF THE PROJECT:

UTILITY COMPANY	CONTACT PERSON	PHONE #
RG&E ELECTRIC	GEORGE SAMPSON	724-8475
RG&E GAS	STEVE LAWOR	724-8204
TWC CABLE	PATRICK BROOKS	756-1308
FRONTIER TELEPHONE	MARK TODD	777-7320
CITY STREET LIGHTING	NED KELLY	428-7010
CITY WATER BUREAU	MIKE BUSHART	428-7567
MONROE COUNTY TRAFFIC SIGNAL	ERNIE FATTORE	753-7772

ROCHESTER PURE WATER DISTRICT (RPWD) NOTES:

1. CONTRACTOR IS TO CONSTRUCT ALL SEWER APPURTENANCES (MAIN SEWER, LATERALS, CATCH BASINS AND MANHOLES) IN CONFORMANCE WITH THE SPECIFICATIONS OF THE ROCHESTER PURE WATERS DISTRICT (WHEN APPLICABLE).

2. ANY NEW PORTION OF CATCH BASIN LATERAL INSTALLED WITHIN THE PUBLIC RIGHT-OF-WAY MUST BE SDR-21 PVC CONFORMING TO ASTM D-2241. JOINTS ON HORIZONTAL PORTIONS AND BENDS SHALL BE PUSH-ON NEOPRENE GASKETS. GLUED OR CHEMICALLY WELDED JOINTS WILL NOT BE ACCEPTABLE.

3. ANY DEDICATED MAINLINE SEWER PIPE AND ASSOCIATED FITTINGS INSTALLED AS PART OF THE CITY STREET PROJECT SHALL BE PVC SDR-35 CONFORMING TO ASTM D-3034.

4. CONTRACTOR IS TO TAKE APPROPRIATE MEASURES TO PREVENT DIRT, DEBRIS AND CONSTRUCTION MATERIALS FROM ENTERING SEWER APPURTENANCES (SEWER MAIN, LATERAL, CATCH BASINS AND MANHOLES) DURING CONSTRUCTION OF PROJECT.

5. RPWD IS TO BE NOTIFIED IMMEDIATELY IN THE EVENT OF ANY DAMAGE TO SEWER APPURTENANCES BY CALLING 1-585-753-7600, OPTION #1. ALL REPAIRS ARE TO BE PERFORMED IN THE PRESENCE OF A REPRESENTATIVE OF RPWD, AND ARE TO BE MADE AS PER RPWD SPECIFICATIONS.

6. RPWD WILL NOT MARK PRIVATE SERVICE LATERALS. THE CONTRACTOR WILL BE RESPONSIBLE FOR THE REPAIR OF ANY LATERALS DAMAGED DURING EXCAVATION/CONSTRUCTION ACTIVITIES. IF AVAILABLE, RECORD MAPS INDICATING LATERAL WYE LOCATIONS OF RECORDED WYES/TAPS MAY BE OBTAINED BY CONTACTING THE PURE WATERS RECORDS ROOM AT 753-7651 WEEKDAYS BETWEEN THE HOURS OF 8 AM AND 4 PM.

7. IN THE EVENT THAT EXISTING SEWER MAINS OR LATERAL PIPES ARE BROKEN DURING EXCAVATION, ANY AND ALL REPAIRS MUST BE EXTENDED LINEARLY. INTRODUCING NEW BENDS IN PIPE WILL NOT BE PERMITTED. ALL 5 INCH INSIDE DIAMETER LATERAL PIPE MUST BE REPAIRED USING 5 INCH SERVICE WEIGHT CAST IRON PIPE AND FERNCO COUPLERS. ALL OTHER PIPES ARE TO BE REPAIRED WITH SDR-21 PIPE AND FERNCO COUPLERS. CONTRACTOR MAY BE PERMITTED TO USE OAKUM AND EPOXY MORTAR TO CONNECT NEW PVC PIPE TO AN EXISTING BELL THAT IS IN GOOD CONDITION.

8. RPWD REQUIRES ALL OTHER UTILITIES TO CROSS PERPENDICULAR OR CLOSE AS POSSIBLE TO PERPENDICULAR TO DISTRICT FACILITIES.

9. STONE BEDDING MATERIAL USED FOR SEWER MAIN AND LATERAL PIPE REPAIRS IS TO BE CLASS B CONSISTING OF #1 AND #2 WASHED STONE AT A MINIMUM OF 6 INCH DEPTH.

10. WHEN NEW MANHOLE FRAMES AND COVERS ARE REQUIRED BY CONTRACT, THE CONTRACTOR WILL REPLACE EXISTING SEWER MANHOLE FRAMES AND COVERS (AS PER CONTRACT) WITH NEW $9^{9}/_{32}$ FRAMES AND COVERS. CONTRACTOR WILL BE RESPONSIBLE FOR THE PURCHASING AND DELIVERY OF THE FRAMES AND COVERS. FRAMES AND COVERS WILL MEET RPWD SPECIFICATIONS (A.S.T.M. A48 (LATEST REVISION) CLASS 30B). CONTRACTOR IS TO ADD/REMOVE ADDITIONAL COURSES OF BRICK/BLOCK AS/IF REQUIRED TO RETROFIT NEW FRAMES ONTO EXISTING MANHOLE STRUCTURES AT THE REQUIRED FINISHED GRADE.

11. WHERE AND EXISTING CATCH BASIN IS TO BE REMOVED, ALL PORTIONS OF THE CATCH BASIN WALLS AND FOUNDATION ARE TO BE REMOVED AND THE EXISTING LATERAL ABANDONED AS PER RPWD SPECIFICATIONS.

12. ALL REMOVED SEWER MANHOLE FRAMES AND COVERS AND CATCH BASIN GRATES, ACCESS COVERS, AND CAPSTONES ARE THE PROPERTY OF THE ROCHESTER PURE WATERS DISTRICT. RETURN ITEMS TO THE MCPW FLEET CENTER, 145 PAUL ROAD, ROCHESTER, NEW YORK, MONDAY THROUGH FRIDAY BETWEEN 8:00 A.M. AND 3:00 PM AT NO COST TO THE OWNER.

13. CONTRACTOR IS TO NOTIFY MCPW BY CALLING (585)753-7600, OPTION 5, MINIMUM OF 48 HOURS IN ADVANCE WHEN PLANNING ON WORKING ADJACENT TO EXISTING MC FIBER OPTIC UTILITIES / FACILITIES

14. THE DISTRICT MUST HAVE ACCESS TO ITS FACILITIES AT ALL TIMES. IF AT ANY TIME THE DISTRICT CANNOT ACCESS ITS FACILITIES (IE. CB OR MH), IT WILL BE THE RESPONSIBILITY OF CONTRACTOR TO PROVIDE ACCESS. THE MONROE COUNTY DISPATCH CENTER WILL BE SUPPLIED A COPY OF THE EMERGENCY PHONE LIST AND WILL BE INSTRUCTED TO CONTACT YOU IN THIS EVENT.

15. IN THE EVENT THAT EXISTING STREET DRAINAGE RECEIVING BASINS ARE DAMAGED OR BROKEN DURING EXCAVATION/CONSTRUCTION ACTIVITIES, ANY AND ALL REPAIRS MUST BE ACCOMPLISHED ACCORDING TO RPWD SPECIFICATIONS AS FOLLOWS:

    - BRICK BASINS ARE TO BE REPAIRED USING RED SEWER BRICK, ASTM C32, GRADE SS.
    - TYPE "S" MORTAR TO BE USED.
    - LATERALS TO BE REPAIRED WITH PVC, SDR-21 PIPE AND FERNCO PIPE COUPLERS.
    - IN THE EVENT OF DAMAGE TO A POURED CONCRETE RISER SECTION ON EXISTING BASINS, THE ENTIRE CONCRETE RISER SECTION SHALL BE REMOVED AND REPLACED WITH NEW POURED CONCRETE RISER AS PER RPWD SPECIFICATIONS.
    - IF A PRECAST CONCRETE BASIN IS DAMAGED OR BROKEN DURING COURSE OF WORK, IT MUST BE REPLACED WITH NEW IN KIND.

ROCHESTER PURE WATER DISTRICT (RPWD) NOTES (CONT.):

16. "AS-BUILT" DRAWINGS SHALL BE FURNISHED TO THE RPWD. IN ADDITION TO PAPER HARD COPIES, "AS-BUILT" DRAWINGS SHALL ALSO BE SUBMITTED ELECTRONICALLY AS GEOREFERENCED CAD DRAWINGS. THE "AS-BUILT" PLANS SHALL INDICATE THE EXACT LOCATION OF ALL SEWERS, MANHOLES, WYE BRANCHES, LATERALS, AND CLEANOUTS AS WELL AS LENGTHS AND SLOPES OF PIPES. ELEVATIONS OF ALL SEWER MANHOLE RIMS AND INVERTS AND LATERAL DEPTHS AT PROPERTY LINES OR RPWD EASEMENT LINES SHALL ALSO BE SHOWN.

WATER MAIN AND WATER SERVICES NOTES:

1. THE CONTRACTOR SHALL EXERCISE CAUTION WHEN WORKING NEAR EXISTING WATER MAINS WHICH ARE TO BE RETAINED IN SERVICE. NO VIBRATORY EQUIPMENT IS TO BE USED OVER OR ADJACENT (WITHIN A 5 FOOT DISTANCE) TO EXISTING WATER MAINS.

2. APPROPRIATE MEASURES SHOULD BE TAKEN TO PREVENT DIRT, DEBRIS AND GROUND WATER FROM CONTAMINATING THE WATER MAIN. THE GROUND WATER LEVEL SHALL NEVER BE LESS THAN ONE (1) FOOT BELOW THE PIPE INVERT. WHENEVER AN OPEN PIPE END IS UNATTENDED IT SHALL BE COVERED IN A WATER TIGHT MANNER.

3. ROCHESTER WATER BUREAU REQUIRES THAT A HYDRANT USE PERMIT TO OBTAINED BY THE CONTRACTOR PRIOR TO USING ANY HYDRANT AS A SOURCE OF WATER SUPPLY. PERMIT REQUIRES USE OF WATER METER AND BACKFLOW PREVENTER. WATER BUREAU WILL SUPPLY WATER METER AND BACKFLOW PREVENTER WITH PERMIT. PERMIT FEE INCLUDES REFUNDABLE DEPOSIT FOR USE OF WATER METER AND BACKFLOW PREVENTER.

4. THE CONTRACTOR SHALL VERIFY THE LOCATION OF THE WATER SERVICES AND THE DISPOSITION OF THE WATER SERVICES BEFORE BEGINNING ABANDONMENT OR RENEWAL. THE CONSEQUENCES OF THE CONTRACTOR'S FAILURE TO DO THIS SHALL BORNE SOLELY BY THE CONTRACTOR. ALL WATER SERVICE WORK SHALL BE COORDINATED WITH THE ROCHESTER WATER BUREAU.

5. INFORMATION GIVEN IN WATER SERVICE TABLES AND ON THE PLANS FOR TYPE OF MATERIAL OF THE EXISTING WATER SERVICE PERTAINS TO THE OUTSIDE SERVICE (WATER MAIN TO CURB STOP).

6. IF THE DRILLED-IN (MISSILING) METHOD IS TO BE USED FOR NEW WATER SERVICE INSTALLATIONS, THE CONTRACTOR SHALL VERIFY THE DEPTH AND LOCATION OF SEWER MAINS AND LATERALS PRIOR TO DRILLING OPERATIONS TO PREVENT DAMAGE TO SEWERS.

FORESTRY NOTES:

1. CONTRACTOR IS TO BE AWARE OF AND FAMILIAR WITH REQUIREMENTS OF SECTIONS 89-9 TREES AND 89-12 PENALTIES OF CITY CODE AS THEY WILL BE STRICTLY ADHERED TO. CITY CODE CAN BE FOUND ON CITY OF ROCHESTER'S WEB SITE AT HTTP://WWW.CITYOFROCHESTER.GOV/DERVICES, UNDER "RESEARCH/VIEW - CITY CHARTER AND CODE".

2. CONTRACTOR IS TO PROVIDE PROTECTION FOR EXISTING TREES WITHIN PROJECT LIMITS THROUGHOUT COURSE OF PROJECT TO MINIMIZE AS MUCH AS POSSIBLE ANY DAMAGE FROM OCCURRING TO EXISTING TREES AS RESULT OF CONTRACTOR'S OPERATIONS. TREE PROTECTION IS TO CONSIST OF ORANGE CONSTRUCTION SAFETY FENCING OR WOOD RAILS PLACED AROUND AND COMPLETELY ENCOMPASSING EACH EXISTING TREE, AND IS TO BE IN PLACE BEFORE ANY WORK IS STARTED, PER SECTION S617 TREE PROTECTION AND CONTRACT DOCUMENTS.

3. OVERALL EXISTING TREE ROOT SYSTEM MUST REMAIN STRUCTURALLY ADEQUATE FOR EXISTING TREE TO WITHSTAND HEAVY WINDSTORMS. IN AREAS OF CURB/CONCRETE GUTTER/PAVEMENT WORK, ANY EXISTING TREE ROOTS THAT ARE 2 INCHES OR LARGER IN DIAMETER AND ARE IN CONFLICT WITH SUCH WORK, MUST BE INSPECTED BY CITY FORESTRY BEFORE CUTTING OF TREE ROOTS CAN OCCUR. CONTRACTOR MUST CONTACT CITY FORESTRY AT (585) 428-7581 TO MAKE ARRANGEMENTS FOR SUCH INSPECTION. EXISTING TREE ROOTS THAT ARE SMALLER THAN 2 INCHES IN DIAMETER, ARE TO BE CUT CLEANLY WITH SHARP IMPLEMENTS.

4. IN AREAS OF SIDEWALK CONSTRUCTION, EXISTING TREE ROOTS THAT ARE LOCATED UNDER AREA OF SIDEWALK CONSTRUCTION ARE NOT TO BE CUT. IF ANY OF EXISTING TREE ROOTS ARE 2 INCHES OR LARGER IN DIAMETER, SUBBASE COURSE MATERIAL IS NOT TO BE INSTALLED WITHIN 1 FOOT OF EXISTING TREE ROOT(S), AND CONCRETE IS TO BE POURED OVER EXISTING TREE ROOT(S).

5. IN AREAS OF DRIVEWAY, DRIVEWAY APPROACHES AND SIDEWALK ACCESS RAMP CONSTRUCTION, EXISTING TREE ROOTS ARE NOT TO BE CUT BY MEANS OF MECHANICAL TREE ROOT CUTTING MACHINES. IF ANY OF EXISTING TREE ROOTS MUST BE CUT, THEY ARE TO BE CUT MANUALLY WITH SHARP IMPLEMENTS.

6. IN AREAS WHERE EXISTING TREE ROOTS BECOME EXPOSED BY CONSTRUCTION ACTIVITIES, THEY ARE TO BE COVERED WITH MULCH, TOPSOIL OR MOISTENED BURLAP, AND WATERED STARTING IMMEDIATELY AFTER EXPOSURE AND CONTINUING UNTIL EXCAVATED AREA IS RESTORED.

7. IF HEAVY EQUIPMENT MUST BE DRIVEN ON LAWN AREA AND OVER TREE ROOT ZONE, PLYWOOD IS TO BE PLACED ON LAWN AREA TO EXTENTS OF TREE ROOT ZONE TO DISPERSE AND MINIMIZE COMPACTION AND TO PREVENT SOIL RUTTING. NO EQUIPMENT OR MATERIALS ARE TO BE STORED OVER TREE ROOT ZONE.

8. CONTACT BRIAN LIBERTI, (585) 428-6971, FOR CITY FORESTRY FIELD REVIEWS.

9. ALL CUTTING FOR THE REMOVAL OF SOD AND SOIL IN ORDER TO ESTABLISH A FINISHED GRADE WITHIN 4 FEET OF EXISTING TREES MUST BE DONE MANUALLY.

FORESTRY NOTES (CONTINUED):

10. ROOT FOUNDATIONS MUST REMAIN ADEQUATE TO WITHSTAND HEAVY WINDSTORMS. TREE ROOTS 2" AND UNDER ARE TO BE CUT CLEANLY. CITY FORESTRY IS TO BE NOTIFIED (585-428-7581) TO SET UP INSPECTION IF ROOTS LARGER THAN 2" ARE IN CONFLICT WITH CONSTRUCTION.

11. ROOT SYSTEMS FOR STREET TREES SHALL NOT BE CUT FOR THE INSTALLATION OF ANY TYPE OF UTILITY BY THE CONTRACTOR OR CITY DEPARTMENT. CONTACT THE FORESTRY DIVISION 428-7281 FOR BRIDGING/TUNNELING SPECIFICATIONS.

12. TREE PROTECTION IS TO BE INSTALLED BEFORE ANY CONSTRUCTION ACTIVITIES BEGINS. THERE IS TO BE NO STOCK PILING OF MATERIALS OR EXCAVATED SPOILS UNDER THE CANOPY OF ANY TREES. VEHICLES AND CONSTRUCTION EQUIPMENT SHALL NOT BE PARKED ON ANY TREE LAWN.

SOIL EROSION AND WATER ABATEMENT NOTES:

1. CONTRACTOR IS TO BE IN FULL COMPLIANCE WITH REQUIREMENTS OF CITY OF ROCHESTER LAWS AND REGULATIONS, SUBSECTION 10 COMPLIANCE WITH AIR AND WATER LAWS AND REGULATIONS, WHICH STATE IN PART:

   A. SOIL EROSION AND WATER POLLUTION ABATEMENT. THE CONTRACTOR SHALL SCHEDULE AND CONDUCT ITS OPERATIONS TO MINIMIZE EROSION OF SOILS AND TO PREVENT SILTING AND MUDDYING OF STREAMS, RIVERS, IRRIGATION SYSTEMS, IMPOUNDMENTS (LAKES, RESERVOIRS, ETC.) AND LANDS ADJACENT TO OR AFFECTED BY THE WORK. CONSTRUCTION OF DRAINAGE FACILITIES AND PERFORMANCE, OR OTHER WORK, WHICH WILL CONTRIBUTE TO THE CONTROL OF EROSION AND SEDIMENTATION SHALL BE CARRIED OUT IN CONJUNCTION WITH EARTHWORK OPERATIONS, OR AS SOON THEREAFTER AS PRACTICABLE. THE AREA OF BARE SOIL EXPOSED AT ANY ONE TIME BY CONSTRUCTION OPERATIONS SHALL BE KEPT TO A MINIMUM.

   B. WHEN IT BECOMES NECESSARY, THE PROJECT MANAGER WILL INFORM THE CONTRACTOR OF UNSATISFACTORY CONSTRUCTION PROCEDURES AND PERATIONS INSOFAR AS EROSION CONTROL, WATER AND AIR POLLUTION ARE CONCERNED. IF THE UNSATISFACTORY CONSTRUCTION PROCEDURES AND OPERATIONS ARE NOT CORRECTED PROMPTLY, THE PROJECT MANAGER MAY SUSPEND THE PERFORMANCE OF ANY OR ALL OF OTHER CONSTRUCTION UNTIL THE UNSATISFACTORY CONDITION BEEN CORRECTED.

2. CONTRACTOR IS TO PROVIDE TEMPRARY CONCRETE WASHOUT FACILITY TO COLLECT, RETAIN AND RECYCLE 100% OF CONCRETE WASHOUT WASTE WATER AND SOLIDS IN ACCORDANCE WITH "EPA STORMWATER BEST MANAGEMENT PRACTICE - CONCRETE WASHOUT". ALL TOOLS AND EQUIPMENT USED FOR PLACEMENT OF CONCRETE MUST BE WASHED DOWN TO REMOVE RESIDUE CONCRETE BEFORE IT HARDENS. WASHOUT FACILITY IS TO BE WATERTIGHT/LEAK PROOF CONTAINER TO PREVENT LEACHING OF WASHWATER INTO SOIL. CONTAINER IS TO BE SIZED TO ALLOW FOR EVAPORATION OF WASHWATER AND RAINFALL, AND IS TO BE LOCATED TO BE CONVIENENT AND ACCESSIBLE FOR WORKERS AND CONCRETE TRUCKS. COST OF PROVIDING AND MAINTAINING CONCRETE WASHOUT FACILITY IS TO BE INCLUDED IN UNIT PRICE BID FOR RESPECTIVE WORK RELATED ITEMS.

3. THE CONTRACTOR IS REQUIRED TO TOPSOIL, SEED AND MULCH ALL DISTURBED AREAS AS SOON AS POSSIBLE AFTER GRADING TO THE SATISFACTION OF THE RESIDENT PROJECT REPRESENTATIVE (R.P.R.).

4. ALL EROSION AND SEDIMENT CONTROL MEASURES SHALL BE PLACED PRIOR TO STARTING EARTHWORK OPERATIONS AND SHALL REMAIN IN PLACE UNTIL THE SLOPES ARE STABILIZED WITH SEEDING AND/OR SLOPE PROTECTION, AS DETERMINED BY THE R.P.R.

5. THE COST OF INSTALLING, CLEANING, AND REMOVING TEMPORARY SOIL EROSION AND SEDIMENT CONTROL DEVICES SHALL BE PAID FOR UNDER THE RESPECTIVE ITEMS.

6. ANY DISTURBED AREAS WHICH ARE LEFT EXPOSED MORE THAN 14 DAYS, AND ARE NOT SUBJECT TO CONSTRUCTION TRAFFIC SHALL IMMEDIATELY RECEIVE TEMPORARY SEEDING MEETING THE REQUIREMENTS OF ITEM 209.1003, SEED AND MULCH, TEMPORARY. HOWEVER NO EXTRA PAYMENT WILL BE MADE FOR TEMPORARY SEEDING.

7. ANY GRADED AREAS NOT SUBJECT TO FURTHER DISTURBANCE OR CONSTRUCTION TRAFFIC SHALL BE ESTABLISHED WITH PERMANENT VEGETATIVE COVER, AS PER CONTRACT SPECIFICATIONS WITHIN 14 DAYS OF FINAL GRADING.

Lu Engineers
ENVIRONMENTAL • TRANSPORTATION • CIVIL
PROJECT MANAGER: J. OTTMAN
DRAWN BY: J. MESSENGER

Department of Environmental Services
Architecture and Engineering Services
City of Rochester, New York
ASSISTANT MANAGING ENGINEER: LISA Y. REYES
CITY ENGINEER: HOLLY E. BARRETT, P.E.

PROJECT TITLE
SCOTTSVILLE ROAD - ELMWOOD AVENUE
IMPROVEMENT PROJECT
(WEST CITY LINE - MT HOPE AVE)

NO.	REVISION	BY	DATE

ISSUED 03/06/2019
CHECKED JWO
DRAWN JRM
DESIGN JRM
SCALE NONE
PROJECT NUMBER 17102

DRAWING TITLE
GENERAL NOTES
GN-2

DRAWING NO. 7 OF 54

FILE NAME = General Notes.dgn
DATE/TIME = 06-MAR-2019 10:55
USER = JMessenger

GENERAL NOTES:

1. ACCESS ELEMENTS ARE TO BE CONSTRUCTED IN ACCORDANCE WITH "PROPOSED GUIDELINES FOR PEDESTRIAN FACILITIES IN THE PUBLIC RIGHT-OF-WAY (PROWAG)". FOLLOWING NOTES REITERATE OR AMEND REQUIREMENTS OUTLINED IN "PROPOSED GUIDELINES FOR PEDESTRIAN FACILITIES IN THE PUBLIC RIGHT-OF-WAY (PROWAG)" AND NYSDOT "HIGHWAY DESIGN MANUAL - CHAPTER 18 PEDESTRIAN FACILITY DESIGN". SEE BOTH DOCUMENTS FOR FURTHER INFORMATION, CLARIFICATION OR GUIDANCE AS NEEDED,

2. DESIGN TOLERANCES FOR DIMENSIONS, GRADES (RUNNING SLOPE) AND CROSS SLOPES SHOWN IN CONTRACT DOCUMENTS ARE MINIMUM AND MAXIMUM LIMITS FOR DESIGN AND FIELD LAYOUT OF ACCESSIBLE ELEMENTS. ACCESSIBLE ELEMENTS ARE NOT TO BE CONSTRUCTED WITH VALUES OUTSIDE LIMITS PER ACCESSIBILITY GUIDELINES AND FOR WORK ACCEPTANCE.

3. FOR PEDESTRIAN ACCESS, SIDEWALKS ARE TO HAVE MINIMUM CLEAR WIDTH OF 5 FEET, WHEREAS ACCESSIBILITY GUIDELINES REQUIRE MINIMUM CLEAR WIDTH OF 4 FEET. WIDTH OF SIDEWALK IS MEASURED FROM BACK EDGE OF CURB OR CONCRETE GUTTER. SIDEWALK SURFACE, GRADE AND CROSS SLOPE ARE TO BE MAINTAINED ACROSS DRIVEWAYS. WHERE SIDEWALK IS LESS THAN 5 FEET WIDE, PASSING SPACES ARE TO BE PROVIDED AT MAXIMUM INTERVALS OF 200 FEET. DRIVEWAYS THAT MEET CROSS SLOPE REQUIREMENTS ARE ACCEPTABLE FOR USE AS PASSING SPACE.

4. SIDEWALK CROSS SLOPE IS TO BE 1.50% MAXIMUM FOR DESIGN AND FIELD LAYOUT, 2% MAXIMUM PER ACCESSIBILITY GUIDELINES AND FOR WORK ACCEPTANCE.

5. SIDEWALK AND PEDESTRIAN STREET CROSSINGS ARE TO INCLUDE MINIMUM 4 FEET WIDE REDUCED VIBRATION ZONE, TO MINIMIZE PAINFUL VIBRATION THAT PERSONS USING WHEELED MOBILITY AIDS MAY EXPERIENCE WHEN TRAVERSING ROUGH AND UNEVEN SURFACES. REDUCED VIBRATION ZONE IS TO BE FREE OF ROUGH AND UNEVEN SURFACE FEATURES, i.e.: EXPOSED AGGREGATE, PAVERS WITH ROUNDED EDGES, PAVERS WITH CHAMFERED EDGES GREATER THAN 1/4 INCH, COBBLESTONES, DECORATIVE PATTERNING MATERIALS AND SURFACE IMPRINTS WIDER AND DEEPER THAN 1/4 INCH, AND ANY OTHER TYPE OF ROUGH, UNEVEN OR JOINTED SURFACES. ACCEPTABLE REDUCED VIBRATION SURFACES INCLUDE CONCRETE, ASPHALT, SQUARED EDGE PAVERS, PAVERS WITH CHAMFERED EDGES LESS THAN 1/4 INCH, DECORATIVE PATTERNING MATERIALS AND SURFACE IMPRINTS LESS THAN 1/4 INCH WIDE AND DEEP.

6. TO CHECK FIELD LAYOUT AND TO VERIFY WORK ACCEPTANCE, ALL GRADES (RUNNING SLOPES) AND CROSS SLOPES ARE TO BE MEASURED WITH 4 FOOT LONG DIGITAL LEVEL USING AT LEAST TWO READINGS. WHERE READINGS VARY MEASUREMENTS ARE TO BE AVERAGED. GRADES (RUNNING SLOPES) ARE TO BE MEASURED IN DIRECTION OF PEDESTRIAN TRAVEL ALONG CENTERLINE AND ON 12 INCH AND 18 INCH OFFSETS FROM CENTERLINE. CROSS SLOPES ARE TO BE MEASURED PERPENDICULAR TO DIRECTION OF PEDESTRIAN TRAVEL AT 5 FOOT INTERVALS.

7. JOINTS BETWEEN SIDEWALK FLAGS, ACCESS RAMPS, BLENDED TRANSITIONS, TURNING SPACES AND STREETS ARE TO BE FLUSH AND FREE FROM ABRUPT VERTICAL SEPARATIONS GREATER THAN 1/4 INCH. VERTICAL CHANGES BETWEEN 1/4 INCH AND 1/2 INCH ARE TO BE BEVELED ACROSS ENTIRE JOINT WITH STANDARD BEVEL SLOPE OF 1:3, BUT NO STEEPER THAN 1:2. VERTICAL SEPARATIONS GREATER THAN 1/2 INCH ARE UNACCEPTABLE, REQUIRING TOTAL REPLACEMENT.

8. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE LAYOUT, INSTALLATION AND THAT THE FINAL PRODUCT IS FULLY COMPLIANT WITH CURRENT ADA GUIDELINES AS PER PROPOSED RIGHT OF WAY ACCESSIBILITY GUIDELINES, 2011 (PROWAG). IF THE FINAL PRODUCT FAILS TO MEET ADA GUIDELINES, AS DETERMINED BY THE R.P.R. IN CONSULTATION WITH THE CITY, THE FINAL PRODUCT SHALL BE REPLACED AT NO ADDITIONAL COST TO THE CITY.

9. SIDEWALK CURB RAMPS SHALL CONFORM TO THE CITY OF ROCHESTER STANDARD DETAILS, THE DETAILS ON NYSDOT STANDARD SHEET 608-01 (SHEETS 1-9) AND THE DETAILS PROVIDED IN THE PLANS.

10. ALL CURB RAMPS ARE TO BE LAID OUT COMPLETELY BY THE CONTRACTOR AND APPROVED BY THE RESIDENT PROJECT REPRESENTATIVE (R.P.R.). PAYMENT FOR LAYOUT OF THE CURB RAMPS IS TO BE INCLUDED IN THE THE UNIT PRICE BID FOR ITEM S608.12, CONCRETE SIDEWALK AND DRIVEWAY.

11. VERTICAL FACE CURB SHOULD NOT BE USED IN THE PEDESTRIAN ACCESSIBLE ROUTE IMMEDIATELY ADJACENT TO THE SLOPED PORTION OF THE CURB RAMPS OR LANDINGS. SIDE FLARES SHALL BE PROVIDED WHEN FEASIBLE. SIDE FLARE SLOPES ACROSS A PEDESTRIAN CIRCULATION PATH CANNOT EXCEED A SLOPE OF 9.5%. SIDE FLARE SLOPES STEEPER THAN 9.5% MAY BE CONSTRUCTED WHERE PEDESTRIAN CIRCULATION PATHS DO NOT CROSS THE SIDE FLARES. FOR TYPICAL CURB RAMP AND SIDE FLARE CONFIGURATIONS SEE NYSDOT STANDARD SHEET SERIES 608-01.

12. THE CONTRACTOR SHALL HAVE ONLY ONE SIDE OF ANY INTERSECTION CLOSED AT ANY TIME. THE R.P.R. SHALL DETERMINE THE COMPLETENESS OF THE WORK AND APPROVE COMMENCING WORK ON THE OTHER SIDE OF THE INTERSECTION.

13. ALL WORK SHALL BE DONE SO AS NOT TO RESTRICT THE FREE MOVEMENT OF PEDESTRIANS ALONG ANY PEDESTRIAN FACILITY EXCEPT THAT MOVEMENT THROUGH AN AFFECTED CROSSWALK AND/OR CURB RAMP BEING RETROFITTED WITH THE DETECTABLE WARNING UNIT. PEDESTRIAN ACCOMMODATIONS MEETING ADA REGULATIONS AND NYSDOT STANDARDS MUST BE MAINTAINED FOR THE FREE FLOW OF PEDESTRIANS AROUND ALL CONSTRUCTION SITES.

GENERAL NOTES (CONTINUED):

14. IT IS THE INTENT OF THIS PROJECT TO PROVIDE AN END PRODUCT THAT MEETS ALL AMERICAN WITH DISABILITIES ACT (ADA) REQUIREMENTS WITHIN THE HIGHWAY BOUNDARY.

15. THE CONTRACTOR SHALL SURVEY AND DETERMINE THE ELEVATION OF THE EXISTING GROUND AROUND ALL PROPOSED CURB RAMPS. THE ELEVATION OF THE PROPOSED CURB RAMPS AND ANY GROUND RESTORATION SHALL BE CONSTRUCTED SO AS TO AVOID THE COLLECTION OF STANDING WATER AND TO PROVIDE FOR SMOOTH GRADES IN COMPLIANCE WITH THE AMERICANS WITH DISABILITIES ACT (ADA).

    A. WHERE THE EXISTING GROUND IS LOW, THE ELEVATION OF THE SIDEWALK SHALL BE RAISED, AS DETERMINED BY THE R.P.R..

    B. WHERE THE EXISTING GROUND IS HIGH, THE ELEVATION OF THE SIDEWALK SHALL BE LOWERED, AS DETERMINED BY THE R.P.R..

    C. ANY CUT OR FILL REQUIRED TO RESHAPE THE GROUND TO ACCOMMODATE ANY ADJUSTMENTS SHALL BE PAID FOR UNDER ITEM 203.02, UNCLASSIFIED EXCAVATION OR ITEM 203.03, EMBANKMENT IN PLACE, RESPECTIVELY.

    D. THE FINAL LOCATION, ELEVATION, GROUND RESTORATION AND DRAINAGE PROPERTIES MUST BE APPROVED BY THE R.P.R. PRIOR TO ACTUAL CONSTRUCTION.

16. IT IS STRONGLY RECOMMENDED THAT THE CONTRACTOR USE A DIGITAL LEVEL THAT IS CALIBRATED ON ALL FORMS PRIOR TO PLACING CONCRETE IN ORDER TO BUILD NEW CURB RAMPS WITH CORRECT RUNNING SLOPE AND CROSS SLOPE. REFER TO NYSDOT STANDARD SHEET SERIES 608-01 AND THE "CURB RAMP SLOPE AND DIMENSION REQUIREMENTS" TABLE FOR THE CORRECT SLOPES AND TOLERANCES.

17. IN AREAS WHERE EXISTING CURB RAMPS ARE BEING MODIFIED, REPLACED OR NEW CURB RAMPS ARE BEING INSTALLED; THE CURB RAMPS, TURN SPACE, CLEAR SPACE, SIDE FLARES, DETECTABLE WARNING UNITS AND CURB INSTALLATION SHALL BE PAID FOR UNDER THEIR RESPECTIVE ITEMS AS SHOWN IN THE "SIDEWALK" TABLE. ADDITIONAL SIDEWALK NECESSARY FOR TRANSITIONING TO EXISTING SIDEWALK, SEPARATE FROM THE CURB RAMPS, SIDE FLARES, TURNING SPACE OR CLEAR SPACE WILL REQUIRE PRIOR APPROVAL BY THE R.P.R. AND BE PAID FOR UNDER THE RESPECTIVE CONCRETE SIDEWALK ITEMS.

18. WHERE NEW CONSTRUCTION IS TO MEET EXISTING CONDITIONS, EXISTING MATERIAL SHALL BE FULL DEPTH CUT TO NEAT LINES AND THE OLD MATERIAL REMOVED SO AS TO NOT DAMAGE THE REMAINING MATERIAL. IF ANY ADJACENT MATERIAL IS DAMAGED OR MUST BE ALTERED THE CONTRACTOR SHALL REPAIR THESE AREAS IN ACCORDANCE WITH THE DETAILS OF THESE CONTRACT DOCUMENTS AND ANY APPLICABLE NYSDOT STANDARD SHEETS AS DETERMINED BY THE R.P.R.

19. RAMP TYPE NUMBERING CORRELATES TO THE NYSDOT STANDARD SHEET RAMP NUMBERS ON STANDARD SHEET SERIES 608-01. SOME CURB RAMPS IN THIS PROJECT ARE MODIFICATIONS OF THE CURB RAMPS ON THE STANDARD SHEETS. MODIFIED CURB RAMPS INCLUDE "MOD" AFTER THE CURB RAMP TYPE NUMBER. ALL SIDEWALK RAMPS ARE TO BE OF THE TYPE AS NOTED, AND LOCATED AS SHOWN IN THE PLANS. HEADER CURB FOR THE SIDEWALK RAMP IS TO BE 5 FEET LONG, WITH THE TRANSITION CURBS BEING FROM 3 TO 7 FEET IN LENGTH, PER DETAILS.

20. WHERE NEW SIDEWALK IS TO BE CONSTRUCTED ADJACENT TO OTHER PAVED AREAS, A MINIMUM OF 2 FEET OF THE ADJACENT PAVED AREA IS TO BE REPLACED IN KIND.

21. RECONSTRUCTION LIMITS BEYOND THE CURB RAMP SHALL BE KEPT TO A MINIMUM. PROVIDE TRANSITIONS BETWEEN PROPOSED CURB RAMP LANDING AND EXISTING SIDEWALK. THE CONTRACTOR SHALL NOT DAMAGE AREAS BEYOND THE SAW CUT LIMITS. DAMAGE BEYOND THE SAW CUT LIMITS SHALL BE REPLACED TO THE NEXT JOINT OR SCORE LINE. ANY SUCH DAMAGE SHALL BE REPAIRED BY THE CONTRACTOR AT NO COST TO THE CITY.

22. THE TRANSITION FROM 6 INCH CURB TO FLUSH CURB SHALL BE A SMOOTH AND CONSISTENT TRANSITION.

ACCESSIBLE CONNECTION NOTES:

1. ACCESS RAMP AND BLENDED TRANSITION ARE ACCESSIBLE CONNECTIONS BETWEEN SIDEWALK AND STREET. ACCESS RAMP IS DEFINED AS CONNECTION THAT HAS GRADE (RUNNING SLOPE) GREATER THAN 5%. BLENDED TRANSITION IS DEFINED AS CONNECTION THAT HAS MAXIMUM GRADE (RUNNING SLOPE) OF 5%.

2. ACCESS RAMPS AND BLENDED TRANSITIONS ARE TO BE MINIMUM 4 FEET WIDE PER ACCESSIBILITY GUIDELINES.

3. ACCESS RAMP AND BLENDED TRANSITION CROSS SLOPE IS TO BE AS FLAT AS PRACTICAL WHILE STILL PROVIDING POSITIVE DRAINAGE. CROSS SLOPE IS TO BE 1.50% MAXIMUM FOR DESIGN AND FIELD LAYOUT, 2% MAXIMUM PER ACCESSIBILITY GUIDELINES AND FOR WORK ACCEPTANCE.

4. WHERE ACCESS RAMP OR BLENDED TRANSITION ENCROACHES INTO SIDEWALK, CROSS SLOPE OF SIDE FLARES IS TO BE 9.50% MAXIMUM FOR DESIGN AND FIELD LAYOUT, 10% MAXIMUM PER ACCESSIBILITY GUIDELINES AND FOR WORK ACCEPTANCE, AS MEASURED PARALLEL TO STREET.

5. GRADE (RUNNING SLOPE) OF ACCESS RAMP IS TO BE 7.50% MAXIMUM FOR DESIGN AND FIELD LAYOUT, 8.30% MAXIMUM PER ACCESSIBILITY GUIDELINES AND FOR WORK ACCEPTANCE.

6. WHERE EXISTING CONDITIONS CANNOT BE OVERCOME TO ACCOMMODATE 8.30% MAXIMUM GRADE (RUNNING SLOPE) FOR ACCESS RAMP, LENGTH OF ACCESS RAMP CAN BE EXTENDED UP TO MAXIMUM LENGTH OF 15 FEET.

7. AT STREET CORNERS WITH DUAL PEDESTRIAN STREET CROSSINGS, SEPARATE ACCESS RAMPS OR BLENDED TRANSITIONS ARE TO BE PROVIDED FOR EACH PEDESTRIAN STREET CROSSING, ORIENTED IN DIRECTION OF PEDESTRIAN TRAVEL. WHERE EXISTING CONDITIONS CANNOT BE OVERCOME TO ACCOMMODATE SEPARATE ACCESS RAMPS OR BLENDED TRANSITIONS, SINGLE DIAGONAL ACCESS RAMP OR BLENDED TRANSITION MAY BE PERMITTED THAT IS ORIENTED TO SERVE BOTH PEDESTRIAN STREET CROSSINGS.

8. AT T-INTERSECTION, IT IS ONLY NECESSARY TO PROVIDE SINGLE PEDESTRIAN STREET CROSSING FOR THROUGH STREET FROM EITHER CORNER OF INTERSECTING STREET.

TURNING SPACE AND CLEAR SPACE NOTES:

1. TURNING SPACE IS DEFINED AS AN AREA AT TOP OF ACCESS RAMP OR BLENDED TRANSITION TO CHANGE DIRECTION FROM SIDEWALK INTO ACCESS RAMP OR BLENDED TRANSITION. CLEAR SPACE IS DEFINED AS AN AREA BEYOND BOTTOM GRADE BREAK THAT IS WITHIN WIDTH OF PEDESTRIAN STREET CROSSING, THAT IS TO BE KEPT CLEAR OF ANY IMPEDIMENT THAT MAY INTERFERE WITH ACCESS FOR PERSONS WITH DISABILITIES. TURNING SPACE MAY ALSO BE LOCATED AT BOTTOM OF ACCESS RAMP OR BLENDED TRANSITION, AND CAN OVERLAP CLEAR SPACE.

2. WHERE THERE ARE NO VERTICAL CONSTRAINTS AT BACK OF SIDEWALK, (E.G. STRUCTURES, FENCES, WALLS, CURB, ET CETERA), TURNING SPACE IS TO BE MINIMUM OF 4 FEET x 4 FEET. WHERE THERE ARE VERTICAL CONSTRAINTS, TURNING SPACE IS TO BE MINIMUM OF 4 FEET x 5 FEET, WITH 5 FEET DIMENSION BEING PERPENDICULAR TO CONSTRAINT.

3. CROSS SLOPE OF TURNING SPACE IS TO BE AS FLAT AS PRACTICAL WHILE STILL PROVIDING POSITIVE DRAINAGE. CROSS SLOPE IS TO BE 1.50% MAXIMUM FOR DESIGN AND FIELD LAYOUT, 2% MAXIMUM PER ACCESSIBILITY GUIDELINES AND FOR WORK ACCEPTANCE.

4. CLEAR SPACE IS TO BE MINIMUM OF 4 FEET x 4 FEET.

DETECTABLE WARNING SURFACE NOTES:

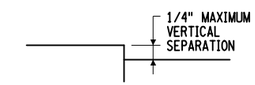
1. DETECTABLE WARNING SURFACE IS TO BE MINIMUM OF 24 INCHES DEEP SITUATED IN PREDOMINANT DIRECTION OF PEDESTRIAN TRAFFIC, AND EXTEND ACROSS FULL WIDTH OF ACCESS RAMP, BLENDED TRANSITION, OR FLUSH SURFACE, EXCLUDING SIDE FLARES.

2. DETECTABLE WARNING SURFACE IS TO BE PROVIDED AT FOLLOWING LOCATIONS:

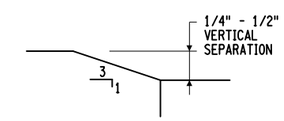
   - ACCESS RAMPS, BLENDED TRANSITIONS AND FLUSH SURFACES AT PEDESTRIAN STREET CROSSINGS
   - BOTH SIDES OF STREET MEDIANS AND PEDESTRIAN REFUGE ISLANDS, AND WHERE SIDEWALK IS MINIMUM 6 FEET LONG
   - BOTH SIDES OF STREET INTERSECTION STYLE DRIVEWAY OPENING
   - BOTH SIDES OF DRIVEWAY OPENING THAT IS CONTROLLED BY STOP SIGN OR TRAFFIC SIGNAL

3. DETECTABLE WARNING SURFACE IS TO PROVIDE LIGHT-ON-DARK OR DARK-ON-LIGHT CONTRAST WITH SURROUNDING SURFACE. DEFAULT COLOR FOR DETECTABLE WARNING SURFACE IS #33538 TRAFFIC YELLOW. OTHER ACCEPTABLE COLORS FOR USE IN CONCRETE AREAS #20109 RED BROWN AND #36081 DARK GUNSHIP GRAY; IN ASPHALT AND OTHER SIMILAR DARK SURFACE AREAS #36495 LIGHT GRAY AND #37925 INSIGNIA WHITE; OR APPROVED EQUIVALENTS. COLORS ARE TO BE IN ACCORDANCE WITH FEDERAL STANDARD 595C.

4. ROWS OF TRUNCATED DOMES ARE TO BE ALIGNED PERPENDICULAR OR RADIAL TO LOWER GRADE BREAK ON ACCESS RAMPS AND BLENDED TRANSITIONS WITH GRADE (RUNNING SLOPE) OF 5% OR GREATER. WHERE DETECTABLE WARNING DOMES ARE ARRAYED RADIALLY TRUNCATED DOMES MAY DIFFER IN DIAMETER AND CENTER-TO-CENTER SPACING. WHERE GRADE (RUNNING SLOPE) IS LESS THAN 5%, ORIENTATION IS LESS CRITICAL AND TRUNCATED DOMES MAY DIFFER FROM PERPENDICULAR OR RADIAL ALIGNMENT TO LOWER GRADE BREAK.



MAXIMUM ACCEPTABLE
VERTICAL SURFACE SEPARATION



BEVELED
VERTICAL SURFACE SEPARATION

ELEMENT TOLERANCES

ELEMENT	DESIGN AND FIELD LAYOUT	ACCESSIBILITY GUIDELINES AND WORK ACCEPTANCE
SIDEWALK CROSS SLOPE	1.50% MAXIMUM	2% MAXIMUM
TURNING SPACE CROSS SLOPE	1.50% MAXIMUM	2% MAXIMUM
ACCESS RAMP CROSS SLOPE	1.50% MAXIMUM	2% MAXIMUM
BLENDED TRANSITION CROSS SLOPE	1.50% MAXIMUM	2% MAXIMUM
SIDE FLARE CROSS SLOPE (WHERE SIDE FLARE ENCROACHES INTO PAVED AREA)	9.50% MAXIMUM	10% MAXIMUM
SIDE FLARE CROSS SLOPE (WHERE SIDE FLARE ABUTS LAWN AREA)	20% MAXIMUM	20% MAXIMUM
ACCESS RAMP GRADE (RUNNING SLOPE)	7.50% MAXIMUM	8.30% MAXIMUM
BLENDED TRANSITION GRADE (RUNNING SLOPE)	4.50% MAXIMUM	5% MAXIMUM

Lu Engineers
ENVIRONMENTAL • TRANSPORTATION • CIVIL
PROJECT MANAGER: J. OTTMAN
DRAWN BY: J. MESSENGER

Department of Environmental Services
Architecture and Engineering Services
City of Rochester, New York
ASSISTANT MANAGING ENGINEER: LISA Y. REYES
CITY ENGINEER: HOLLY E. BARRETT, P.E.

PROJECT TITLE
SCOTTSVILLE ROAD - ELMWOOD AVENUE IMPROVEMENT PROJECT
(WEST CITY LINE - MT HOPE AVE)

NO.	REVISION	BY	DATE

ISSUED 03/06/2019
CHECKED JWO
DRAWN JRM
DESIGN JRM
SCALE NONE
PROJECT NUMBER 17102

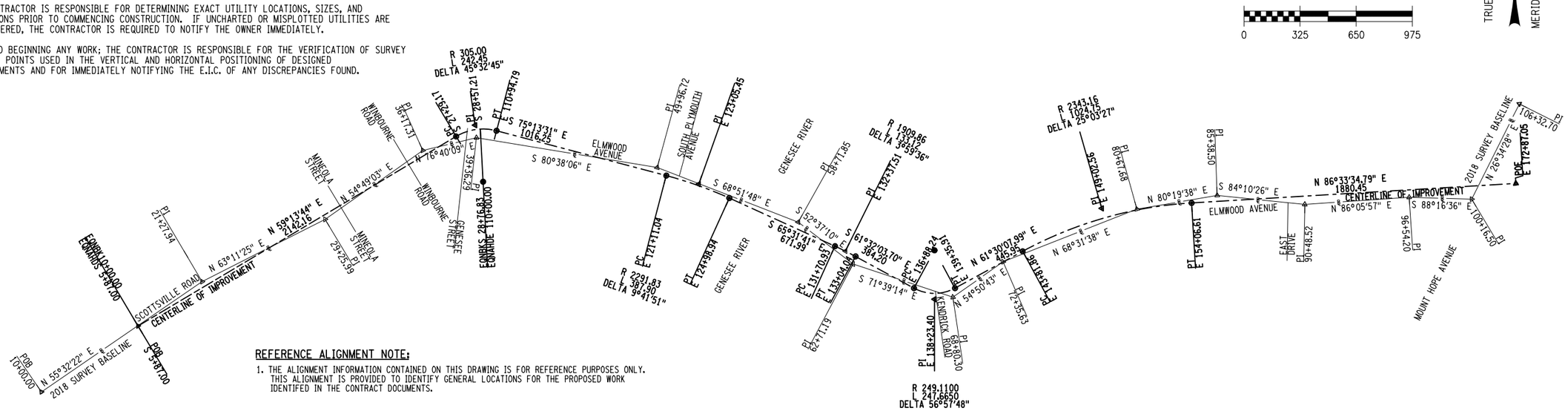
DRAWING TITLE
PEDESTRIAN ACCESSIBILITY GUIDELINES GN-3

DRAWING NO. 8 OF 54

FILE NAME = General Notes.dgn
DATE/TIME = 06-MAR-2019 10:56
USER = JMessenger

NOTES:

1. THE HORIZONTAL DATUM SHOWN HEREON IS REFERENCED TO THE NEW YORK STATE PLANE COORDINATE SYSTEM, NAD 83(2011) TRANSVERSE MERCATOR PROJECTION, WESTERN ZONE. BASELINE COORDINATES WERE ESTABLISHED BY A COMBINATION OF GPS SURVEY METHODS USING A TRIMBLE R8S AND CONVENTIONAL MEASUREMENTS USING A LEICA TS12.
2. THE ELEVATIONS SHOWN HEREON ARE REFERENCED TO NAVD 88 DATUM. ELEVATIONS WERE ESTABLISHED FOR BASELINE AND PROJECT BENCHMARKS BY A COMBINATION OF GPS SURVEY METHODS USING A TRIMBLE R8S AND CONVENTIONAL LEVELING MEASUREMENTS USING A TRIMBLE DINI.
3. ALL DISTANCES AND COORDINATES IN U.S. SURVEY FEET UNLESS OTHERWISE NOTED. COORDINATES REPORTED ARE GRID. COMBINED SCALE FACTOR OF 0.99999080.
4. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO NOTIFY DIG SAFELY - NEW YORK FOR ALL UNDERGROUND UTILITY LOCATIONS PRIOR TO ANY EXCAVATION.
5. UTILITY INFORMATION HAS BEEN PLOTTED FROM AVAILABLE SOURCES AND THEIR PURPOSE LOCATIONS AND SIZE SHOULD BE CONSIDERED APPROXIMATE ONLY.
6. THE CONTRACTOR IS RESPONSIBLE FOR DETERMINING EXACT UTILITY LOCATIONS, SIZES, AND ELEVATIONS PRIOR TO COMMENCING CONSTRUCTION. IF UNCHARTED OR MISPLOTTED UTILITIES ARE ENCOUNTERED, THE CONTRACTOR IS REQUIRED TO NOTIFY THE OWNER IMMEDIATELY.
7. PRIOR TO BEGINNING ANY WORK; THE CONTRACTOR IS RESPONSIBLE FOR THE VERIFICATION OF SURVEY CONTROL POINTS USED IN THE VERTICAL AND HORIZONTAL POSITIONING OF DESIGNED IMPROVEMENTS AND FOR IMMEDIATELY NOTIFYING THE E.I.C. OF ANY DISCREPANCIES FOUND.



REFERENCE ALIGNMENT NOTE:

1. THE ALIGNMENT INFORMATION CONTAINED ON THIS DRAWING IS FOR REFERENCE PURPOSES ONLY. THIS ALIGNMENT IS PROVIDED TO IDENTIFY GENERAL LOCATIONS FOR THE PROPOSED WORK IDENTIFED IN THE CONTRACT DOCUMENTS.

REFERENCE ALIGNMENT:

	STATION	NORTHING	EASTING
ELEMENT: LINEAR			
POB	S 5+87.00	1139184.63	1398340.40
PC	S 27+29.17	1140280.58	1400180.99
TANGENT DIRECTION:	59°13'44"		
TANGENT LENGTH:	2142.17		
ELEMENT: CIRCULAR			
PC	S 27+29.17	1140280.58	1400180.99
PI	S 28+57.21	1140346.09	1400291.00
CC		1140018.52	1400337.03
EQNBK	S 28+76.83	1140323.09	1400320.90
EQNAHD	E 110+00.00	1140323.09	1400320.90
PT	E 110+94.79	1140313.44	1400414.81
RADIUS:		305.00	
DELTA:		45°32'45" RIGHT	
DEGREE OF CURVATURE(ARC):		18°47'08"	
LENGTH:		242.45	
TANGENT:		128.04	
CHORD:		236.12	
MIDDLE ORDINATE:		23.78	
EXTERNAL:		25.79	
TANGENT DIRECTION:		59°13'44"	
RADIAL DIRECTION:		149°13'44"	
CHORD DIRECTION:		82°00'07"	
RADIAL DIRECTION:		194°46'29"	
TANGENT DIRECTION:		104°46'29"	
ELEMENT: LINEAR			
PT	E 110+94.79	1140313.44	1400414.81
PC	E 121+11.04	1140054.27	1401397.46
TANGENT DIRECTION:		104°46'29"	
TANGENT LENGTH:		1016.25	
ELEMENT: CIRCULAR			
PC	E 121+11.04	1140054.27	1401397.46
PI	E 123+05.45	1140004.69	1401585.45
CC		1137838.22	1400813.00
PT	E 124+98.94	1139924.16	1401762.39
RADIUS:		2291.83	
DELTA:		9°41'51" RIGHT	
DEGREE OF CURVATURE(ARC):		2°30'00"	
LENGTH:		387.90	
TANGENT:		194.41	
CHORD:		387.44	
MIDDLE ORDINATE:		8.20	
EXTERNAL:		8.23	
TANGENT DIRECTION:		104°46'29"	
RADIAL DIRECTION:		194°46'29"	
CHORD DIRECTION:		109°37'24"	
RADIAL DIRECTION:		204°28'20"	
TANGENT DIRECTION:		114°28'20"	
ELEMENT: LINEAR			
PT	E 124+98.94	1139924.16	1401762.39
PC	E 131+70.93	1139645.78	1402374.01
TANGENT DIRECTION:		114°28'20"	
TANGENT LENGTH:		671.99	
ELEMENT: CIRCULAR			
PC	E 131+70.93	1139645.78	1402374.01
PI	E 132+37.51	1139618.20	1402434.62
CC		1137907.50	1401582.85
PT	E 133+04.04	1139586.47	1402493.15
RADIUS:		1909.86	
DELTA:		3°59'36" RIGHT	
DEGREE OF CURVATURE(ARC):		3°00'00"	
LENGTH:		133.12	
TANGENT:		66.58	
CHORD:		133.09	
MIDDLE ORDINATE:		1.16	
EXTERNAL:		1.16	
TANGENT DIRECTION:		114°28'20"	
RADIAL DIRECTION:		204°28'20"	
CHORD DIRECTION:		116°28'08"	
RADIAL DIRECTION:		208°27'56"	
TANGENT DIRECTION:		118°27'56"	
ELEMENT: LINEAR			
PT	E 133+04.04	1139586.47	1402493.15
PC	E 136+88.24	1139403.34	1402830.90
TANGENT DIRECTION:		118°27'56"	
TANGENT LENGTH:		384.20	
ELEMENT: CIRCULAR			
PC	E 136+88.24	1139403.34	1402830.90
PI	E 138+23.40	1139338.93	1402949.72
CC		1139622.34	1402949.64
PT	E 139+35.91	1139403.41	1403068.49
RADIUS:		249.11	
DELTA:		56°57'48" LEFT	
DEGREE OF CURVATURE(ARC):		23°00'01"	
LENGTH:		247.66	
TANGENT:		135.15	
CHORD:		237.59	
MIDDLE ORDINATE:		30.15	
EXTERNAL:		34.30	
TANGENT DIRECTION:		118°27'56"	
RADIAL DIRECTION:		208°27'56"	
CHORD DIRECTION:		89°59'02"	
RADIAL DIRECTION:		151°30'08"	
TANGENT DIRECTION:		61°30'08"	
ELEMENT: LINEAR			
PT	E 139+35.91	1139403.41	1403068.49
PC	E 143+81.86	1139616.18	1403460.41
TANGENT DIRECTION:		61°30'08"	
TANGENT LENGTH:		445.95	
ELEMENT: CIRCULAR			
PC	E 143+81.86	1139616.18	1403460.41
PI	E 149+02.56	1139864.62	1403918.02
CC		1137556.93	1404578.39
PT	E 154+06.61	1139895.87	1404437.78
RADIUS:		2343.16	
DELTA:		25°03'27" RIGHT	
DEGREE OF CURVATURE(ARC):		2°26'43"	
LENGTH:		1024.75	
TANGENT:		520.70	
CHORD:		1016.60	
MIDDLE ORDINATE:		55.80	
EXTERNAL:		57.16	
TANGENT DIRECTION:		61°30'08"	
RADIAL DIRECTION:		151°30'08"	
CHORD DIRECTION:		74°01'51"	
RADIAL DIRECTION:		176°33'35"	
TANGENT DIRECTION:		86°33'35"	
ELEMENT: LINEAR			
PT	E 154+06.61	1139895.87	1404437.78
POE	E 172+87.05	1140008.71	1406314.84
TANGENT DIRECTION:		86°33'35"	
TANGENT LENGTH:		1880.45	

PRUDENT ENGINEERING

PROJECT MANAGER: M. VENTURO
DRAWN BY: C. CHAMBERS

Department of Environmental Services
Architecture and Engineering Services
City of Rochester, New York

ASSISTANT MANAGING ENGINEER: LISA Y. REYES
CITY ENGINEER: HOLLY E. BARRETT, P.E.

PROJECT TITLE
SCOTTSVILLE ROAD - ELMWOOD AVENUE IMPROVEMENT PROJECT (WEST CITY LINE - MT HOPE AVE)

NO.	REVISION	BY	DATE

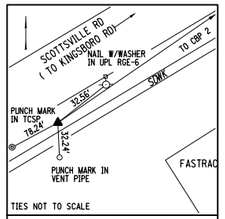
ISSUED 03/06/2019
CHECKED MV
DRAWN CC
DESIGN CC
SCALE 1" = 650'
PROJECT NUMBER 17102

DRAWING TITLE
SURVEY DATA SHEET
SD-1

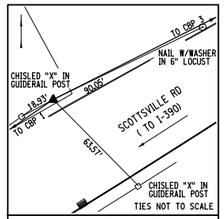
DRAWING NO. 9 OF 54

FILE NAME = 294/029/COR_BORDER_04-EDIT_03-12-19
DATE/TIME = 03/13/2019
USER = PRP

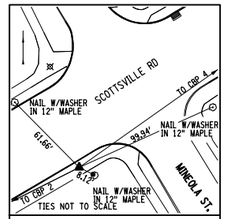
BASELINE TIES



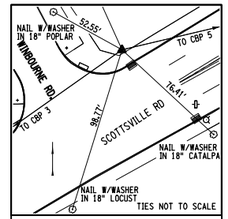
℄ STA. 10+00.00
CBP 1 IS A CAPPED IRON ROD IN GROUND SOUTH OF PAVEMENT EDGE
NYSPCS WEST ZONE NAD83 (2011)
N: 1138800.917
E: 1397778.071



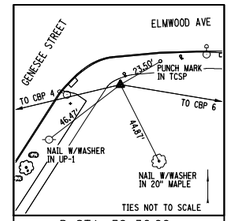
℄ STA. 21+27.94
CBP 2 IS A CAPPED IRON ROD IN GROUND NORTH OF SIDEWALK EDGE
NYSPCS WEST ZONE NAD83 (2011)
N: 1139439.148
E: 1398708.072



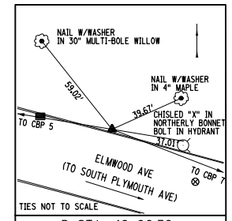
℄ STA. 29+25.99
CBP 3 IS A MAG NAIL IN SIDEWALK SOUTH OF ROADWAY
NYSPCS WEST ZONE NAD83 (2011)
N: 1139799.092
E: 1399420.340



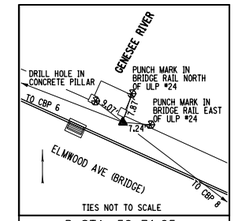
℄ STA. 36+17.31
CBP 4 IS A CAPPED IRON ROD IN GROUND NORTH OF ROADWAY
NYSPCS WEST ZONE NAD83 (2011)
N: 1140197.422
E: 1399985.376



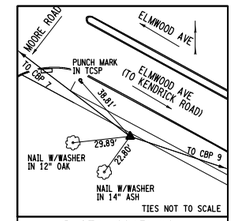
℄ STA. 39+36.29
CBP 5 IS A CAPPED IRON ROD IN GROUND SOUTH OF PAVEMENT EDGE
NYSPCS WEST ZONE NAD83 (2011)
N: 1140270.970
E: 1400295.759



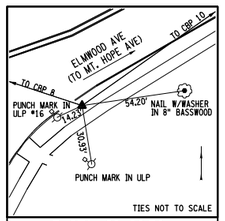
℄ STA. 49+96.72
CBP 6 IS A CAPPED IRON ROD IN GROUND NORTH OF PAVEMENT EDGE
NYSPCS WEST ZONE NAD83 (2011)
N: 1140098.415
E: 1401342.054



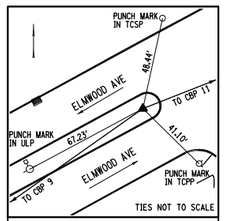
℄ STA. 58+71.85
CBP 7 IS A MAGNAIL SET IN SIDEWALK ON THE NE SIDE OF BRIDGE
NYSPCS WEST ZONE NAD83 (2011)
N: 1139782.849
E: 1402158.305



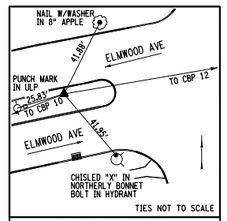
℄ STA. 62+71.19
CBP 8 IS A CAPPED IRON ROD IN GROUND SOUTH OF PAVEMENT
NYSPCS WEST ZONE NAD83 (2011)
N: 1139540.406
E: 1402475.629



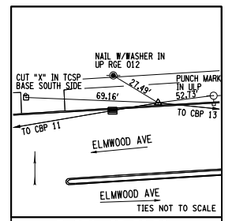
℄ STA. 68+80.30
CBP 9 IS A CAPPED IRON ROD IN GROUND SOUTH OF PAVEMENT EDGE
NYSPCS WEST ZONE NAD83 (2011)
N: 1139348.685
E: 1403053.784



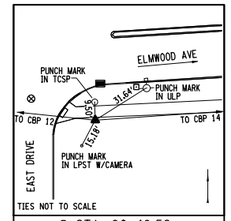
℄ STA. 72+35.63
CBP 10 IS A LARGE MAG HUB SET IN GROUND AT END OF MEDIAN
NYSPCS WEST ZONE NAD83 (2011)
N: 1139553.277
E: 1403344.300



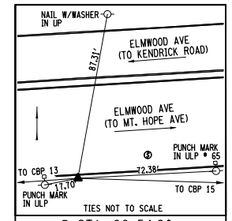
℄ STA. 80+67.68
CBP 11 IS A CAPPED IRON ROD IN GROUND AT END OF MEDIAN
NYSPCS WEST ZONE NAD83 (2011)
N: 1139857.857
E: 1404118.598



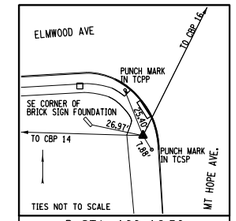
℄ STA. 85+38.50
CBP 12 IS A CAPPED IRON ROD IN GROUND NORTH OF PAVEMENT
NYSPCS WEST ZONE NAD83 (2011)
N: 1139936.964
E: 1404582.724



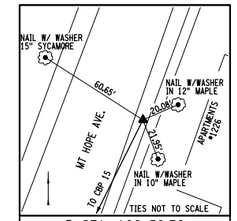
℄ STA. 90+48.52
CBP 13 IS A CAPPED IRON ROD IN GROUND SOUTH OF PAVEMENT EDGE
NYSPCS WEST ZONE NAD83 (2011)
N: 1139885.192
E: 1405090.114



℄ STA. 96+54.20
CBP 14 IS A CAPPED IRON ROD IN GROUND SOUTH OF PAVEMENT EDGE
NYSPCS WEST ZONE NAD83 (2011)
N: 1139926.396
E: 1405694.386



℄ STA. 100+16.50
CBP 15 IS A MAGNAIL SET IN A SIDEWALK JOINT
NYSPCS WEST ZONE NAD83 (2011)
N: 1139915.501
E: 1406056.528



℄ STA. 106+32.70
CBP 16 IS A CAPPED IRON ROD IN GROUND EAST OF PAVEMENT EDGE
NYSPCS WEST ZONE NAD83 (2011)
N: 1140466.602
E: 1406332.191

BENCHMARK TABLE

BENCHMARK NO.	ELEVATION	NORTHING	EASTING	DESCRIPTION	BENCHMARK NO.	ELEVATION	NORTHING	EASTING	DESCRIPTION
1	537.41	1139371.3	1398723.1	CUT SQUARE ON SOUTHEAST WINGWALL OF CANAL BRIDGE	8	545.86	1139600.4	1403355.0	CUT "X" IN TRAFFIC SIGNAL POLE BASE ON SOUTHERLY SIDE
2	538.46	1139839.9	1399485.3	CUT "X" ON NORTHERLY BONNET BOLT ON FIRE HYDRANT	9	551.03	1139825.9	1404145.8	CUT "X" ON NORTHERLY BONNET BOLT ON FIRE HYDRANT
3	541.44	1140141.2	1399991.5	CUT "X" IN LIGHT POLE BASE ON NORTHERLY SIDE	10	551.81	1139939.7	1404513.6	CUT "X" IN TRAFFIC SIGNAL POLE BASE ON SOUTHERLY SIDE
4	544.93	1140284.2	1400315.8	CUT "X" IN TRAFFIC SIGNAL POLE BASE ON NORTHERLY SIDE	11	561.71	1139902.6	1405111.6	CUT "X" ON NORTHERLY BONNET BOLT ON FIRE HYDRANT
5	529.94	1140089.7	1401378.0	CUT "X" ON NORTHERLY BONNET BOLT ON FIRE HYDRANT	12	577.55	1139925.2	1405676.8	CUT "X" IN LIGHT POLE BASE ON NORTHERLY SIDE
6	533.23	1139570.3	1402449.8	CUT "X" IN TRAFFIC SIGNAL POLE BASE ON NORTHERLY SIDE	13	573.23	1139909.3	1406061.6	CUT "X" IN TRAFFIC SIGNAL POLE BASE #85 ON NORTHERLY SIDE
7	531.19	1139332.1	1402920.1	CUT "X" IN TRAFFIC SIGNAL POLE BASE ON NORTHERLY SIDE					

NOTES:

1. THE HORIZONTAL DATUM SHOWN HEREON IS REFERENCED TO THE NEW YORK STATE PLANE COORDINATE SYSTEM, NAD 83(2011) TRANSVERSE MERCATOR PROJECTION, WESTERN ZONE. BASELINE COORDINATES WERE ESTABLISHED BY A COMBINATION OF GPS SURVEY METHODS USING A TRIMBLE R8S AND CONVENTIONAL MEASUREMENTS USING A LEICA TS12.
2. THE ELEVATIONS SHOWN HEREON ARE REFERENCED TO NAVD 88 DATUM. ELEVATIONS WERE ESTABLISHED FOR BASELINE AND PROJECT BENCHMARKS BY A COMBINATION OF GPS SURVEY METHODS USING A TRIMBLE R8S AND CONVENTIONAL LEVELING MEASUREMENTS USING A TRIMBLE DINI.
3. ALL DISTANCES AND COORDINATES IN U.S. SURVEY FEET UNLESS OTHERWISE NOTED. COORDINATES REPORTED ARE GRID. COMBINED SCALE FACTOR OF 0.99999080.
4. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO NOTIFY DIG SAFELY - NEW YORK FOR ALL UNDERGROUND UTILITY LOCATIONS PRIOR TO ANY EXCAVATION.
5. UTILITY INFORMATION HAS BEEN PLOTTED FROM AVAILABLE SOURCES AND THEIR PURPOSE LOCATIONS AND SIZE SHOULD BE CONSIDERED APPROXIMATE ONLY.
6. THE CONTRACTOR IS RESPONSIBLE FOR DETERMINING EXACT UTILITY LOCATIONS, SIZES, AND ELEVATIONS PRIOR TO COMMENCING CONSTRUCTION. IF UNCHARTED OR MISPLOTTED UTILITIES ARE ENCOUNTERED, THE CONTRACTOR IS REQUIRED TO NOTIFY THE OWNER IMMEDIATELY.
7. PRIOR TO BEGINNING ANY WORK; THE CONTRACTOR IS RESPONSIBLE FOR THE VERIFICATION OF SURVEY CONTROL POINTS USED IN THE VERTICAL AND HORIZONTAL POSITIONING OF DESIGNED IMPROVEMENTS AND FOR IMMEDIATELY NOTIFYING THE E.I.C. OF ANY DISCREPANCIES FOUND.

PRUDENT ENGINEERING
PROJECT MANAGER: M. VENTURO
DRAWN BY: C. CHAMBERS

Department of Environmental Services
Architecture and Engineering Services
City of Rochester, New York

ASSISTANT MANAGING ENGINEER: LISA Y. REYES
CITY ENGINEER: HOLLY E. BARRETT, P.E.

PROJECT TITLE
SCOTTSVILLE ROAD - ELMWOOD AVENUE IMPROVEMENT PROJECT
(WEST CITY LINE - MT HOPE AVE)

NO.	REVISION	BY	DATE

ISSUED 03/06/2019
CHECKED MV
DRAWN CC
DESIGN CC
SCALE 1" = 650'
PROJECT NUMBER 17102

DRAWING TITLE
SURVEY DATA SHEET
SD-2

DRAWING NO.
10
OF
54

FILE NAME = 294/029/COR_BORDER_04-EDIT_03-12-19
DATE/TIME = 03/13/2019
USER = PRP

GENERAL NOTES:

1. THE CONTRACTOR SHALL MAINTAIN TRAFFIC THROUGHOUT THE LENGTH OF THE CONTRACT IN ACCORDANCE WITH THE REQUIREMENTS OF SECTION 619 OF THE CURRENT STANDARD SPECIFICATIONS, THE NATIONAL MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES (N.M.U.T.C.D.) IN ADDITION TO THE NEW YORK STATE (N.Y.S.) SUPPLEMENT INCLUDING LATEST REVISIONS, THE MAINTENANCE AND PROTECTION OF TRAFFIC DETAILS IN THE PLANS AND PROPOSAL OF THIS CONTRACT, AND AS DIRECTED BY THE RESIDENT PROJECT REPRESENTATIVE (R.P.R).

2. FOR TYPICAL APPLICATIONS OF TRAFFIC CONTROL DEVICES IN CONSTRUCTION AREAS NOT SPECIFIED IN THE PLANS, THE PROVISIONS OF PART 6 OF THE N.M.U.T.C.D. WITH N.Y.S. SUPPLEMENT SHALL APPLY. THE STANDARDS OF APPLICATION NOTED THEREIN AND ON THE PLANS ARE TO BE CONSIDERED MINIMUM STANDARDS.

3. THE WORK ZONE TRAFFIC CONTROL (WZTC) DETAILS IN THE NYSDOT STANDARD SHEETS ARE STANDARD FOR MOST COMMON SITUATIONS. ADDITIONAL PROTECTION SHALL BE PROVIDED WHERE SPECIAL CIRCUMSTANCES OCCUR AS DETERMINED BY THE R.P.R. AND PAID FOR UNDER ITEM 619.01, BASIC WORK ZONE TRAFFIC CONTROL.

4. THE CONTRACTOR SHALL PROVIDE CONSTRUCTION APPROACH SIGNAGE IN ACCORDANCE WITH THE N.M.U.T.C.D. SECTION 6F.16 (WARNING SIGN FUNCTION, DESIGN AND APPLICATION) AND 6F.17 (POSITION OF ADVANCE WARNING SIGNS) TO THE R.P.R. FOR APPROVAL.

5. THE CONTRACTOR MUST SUBMIT TO THE R.P.R., IN WRITING, PROPOSED REVISIONS TO THE TRAFFIC CONTROL PLAN FOR REVIEW AND APPROVAL BY THE R.P.R. FIVE (5) WORKING DAYS PRIOR TO THE PLANNED IMPLEMENTATION OF SUCH PROPOSED REVISIONS, EXCEPT FOR CHANGES THAT ALTER THE BASIC CONCEPT OF THE TRAFFIC CONTROL PLAN. SUCH CONCEPTUAL CHANGES MUST BE SUBMITTED TO THE R.P.R. AND THE CITY FOR APPROVAL THIRTY (30) WORKING DAYS PRIOR TO IMPLEMENTATION OF SUCH REVISIONS.

6. PRIOR TO THE START OF ANY WORK OPERATIONS, ALL RELATED WORK FOR PROPOSED WORK ZONE TRAFFIC CONTROL, AS DIRECTED BY THE R.P.R., SHALL BE COMPLETE. THIS INCLUDES BUT IS NOT LIMITED TO, ALL SIGNS, SIGNALS, PAVEMENT MARKINGS, BARRIERS, DELINEATION (CONES, DRUMS, ETC.), FLAGGERS, PAVEMENT MODIFICATIONS, AND ANY OTHER RELATED WORK.

7. IF AT ANY TIME THE R.P.R. DETERMINES THAT TRAFFIC IS NOT BEING PROPERLY MAINTAINED WITHIN A WORK ZONE, THE CONTRACTOR SHALL IMMEDIATELY CORRECT THE INDICATED DEFICIENCY, AS DIRECTED BY THE R.P.R..

8. TRAFFIC CONTROL DEVICES SHALL BE KEPT IN PROPER POSITION, CLEAN, AND LEGIBLE, REGARDLESS OF VARYING CONDITIONS.

9. TRAFFIC SHALL BE MAINTAINED IN ACCORDANCE WITH ALL THE PROVISIONS OF ITEM 619.01, BASIC WORK ZONE TRAFFIC CONTROL.

10. WEEDS, SHRUBBERY, CONSTRUCTION MATERIALS, EQUIPMENT, VEHICLES OR WORKERS SHALL NOT OBSCURE TRAFFIC CONTROL DEVICES OR OBSTRUCT TRAFFIC. THE CONTRACTOR SHALL BE REQUIRED TO TRIM ANY FOLIAGE OBSTRUCTING THE VISIBILITY OF ANY TRAFFIC CONTROL DEVICES, WHETHER PERMANENT, TEMPORARY, OR CONSTRUCTION, AS DETERMINED BY THE R.P.R. THE COST SHALL BE INCLUDED IN THE PRICE BID FOR BASIC WORK ZONE TRAFFIC CONTROL, ITEM 619.01.

11. THE CONTRACTOR SHALL TAKE EXTRA CARE TO MAINTAIN A DUST FREE ENVIRONMENT. CALCIUM CHLORIDE AND/OR WATER SHALL BE APPLIED FOR DUST CONTROL AS DETERMINED BY THE R.P.R.. THE WORK AREA IS TO BE CLEANED DAILY AND LEFT IN A MANNER ACCEPTABLE TO THE R.P.R.. THE COST SHALL BE INCLUDED IN THE PRICE BID FOR BASIC WORK ZONE TRAFFIC CONTROL, ITEM 619.01.

12. THE LUMP SUM PRICE BID FOR ITEM 619.01, BASIC WORK ZONE TRAFFIC CONTROL, SHALL INCLUDE ALL EQUIPMENT, MATERIALS, AND LABOR NECESSARY TO ADEQUATELY AND SAFELY MAINTAIN AND PROTECT TRAFFIC, EXCEPT AS PROVIDED FOR IN SEPARATE PAYMENT ITEMS IN THE PROPOSAL.

13. WARNING LIGHTS MOUNTED ON BARRICADES OR OTHER CHANNELIZING DEVICES SHALL BE A STEADY BURN, LOW INTENSITY LIGHTS, EXCEPT FOR THE FIRST ONE (1) IN THE SERIES WHICH SHALL BE HIGH INTENSITY FLASHING LIGHTS. PLASTIC DRUMS (C5-1) OR TYPE III BARRICADES MAY BE PLACED ALONG PAVEMENT TAPERS. REFER TO N.Y.S.D.O.T. STANDARD SHEET 619-02 FOR TYPE III BARRICADES WITH WARNING LIGHTS.

14. THE CONTRACTOR SHALL SCHEDULE THEIR CONSTRUCTION OPERATIONS IN A MANNER SO AS TO MINIMIZE THE LENGTH OF TIME THAT NORMAL TRAFFIC FLOWS ARE DISTURBED.

15. THE CONTRACTOR SHALL USE STEEL PLATES, TEMPORARY PAVEMENT, CONES, BARRELS, BARRICADES AND PROTECTIVE FENCING AS NECESSARY AND AS DIRECTED BY THE R.P.R. TO PROTECT AND MAINTAIN BOTH PEDESTRIAN AND TRAFFIC FLOW WITHIN THE PROJECT AREA. COST TO BE INCLUDED IN THE PRICE BID FOR BASIC WORK ZONE TRAFFIC CONTROL, ITEM 619.01.

16. ALL LANE TAPERS AND TRANSITIONS SHALL BE IN ACCORDANCE WITH TABLE 6H-4. OF THE N.M.U.T.C.D. OR AS DETERMINED BY THE R.P.R..

17. SPECIAL ATTENTION SHALL BE GIVEN BY THE CONTRACTOR TO THE MAINTENANCE OF A SATISFACTORY TRAVELED WAY DURING THE SHUTDOWN OF CONSTRUCTION OPERATIONS IN COMPLIANCE WITH THE REQUIREMENTS OF SECTION 619.01, BASIC WORK ZONE TRAFFIC CONTROL.

GENERAL NOTES (CONTINUED):

18. WHEN THE VISIBILITY OF THE TRAVELING PUBLIC IS RESTRICTED DUE TO WEATHER CONDITIONS AND/OR THE WORK OPERATIONS COMMENCE PRIOR TO DAWN OR CONTINUE BEYOND DUSK, FLASHING BEACONS AND STEADY BURNING LIGHTS SHALL BE PROVIDED BY THE CONTRACTOR AND PLACED AS DIRECTED BY THE RESIDENT PROJECT REPRESENTATIVE. IF A FLAGGING OPERATION IS NECESSARY BEYOND THE DAYLIGHT HOURS, FLAGGERS ARE TO USE FLASHLIGHTS WITH ORANGE CONES ATTACHED PER N.M.U.T.C.D..

WORK AREA NOTES:

1. THE CONTRACTOR SHALL COORDINATE ANY WORK WITH OTHER CONTRACTORS, UTILITY COMPANIES, OR MUNICIPALITIES IN ORDER TO ENSURE ALL TRAFFIC IS PROPERLY AND SAFELY MAINTAINED AND PROTECTED AT ALL TIMES.

2. WHEN TWO OR MORE WORK AREAS ARE ADJACENT, OVERLAP, OR ARE IN CLOSE PROXIMITY AS DETERMINED BY THE R.P.R., THE CONTRACTOR SHALL ENSURE THERE ARE NO CONFLICTING SIGNS AND THAT LANE CONTINUITY IS MAINTAINED THROUGHOUT ALL WORK AREAS. WORK AREAS ARE DEFINED AS THAT AREA IN WHICH TRAFFIC IS RESTRICTED BECAUSE OF CONSTRUCTION ACTIVITIES OR THAT AREA WHICH INVOLVES A DROP-OFF GREATER THAN 2 INCHES ADJACENT TO THE EDGE OF PAVEMENT. THE CONTRACTOR SHALL MAINTAIN A MINIMUM OF 500 FEET DISTANCE BETWEEN CONSTRUCTION OPERATIONS ON ALTERNATE SIDES OF THE ROADWAY.

BARRIER/SHADOW VEHICLE NOTES:

1. BARRIER AND SHADOW VEHICLES SHALL BE REQUIRED AS PER STANDARD SHEET TITLED "WORK ZONE TRAFFIC CONTROL LEGENDS AND NOTES."

2. A SHADOW VEHICLE IS REQUIRED FOR SETTING UP AND REMOVAL OF TEMPORARY TRAFFIC CONTROL SIGNS AND DEVICES ON CONTROLLED ACCESS HIGHWAYS WHERE WORKERS ARE IN A LANE OR SHOULDER EXPOSED TO TRAFFIC.

3. WHEN MULTIPLE WORK OPERATIONS WITHIN THE SAME WORK ZONE ARE SEPARATED BY A LONGITUDINAL DISTANCE GREATER THAN 360 FEET, EACH OPERATION SHALL BE CONSIDERED AN INDEPENDENT WORK AREA AND SHALL BE SUBJECT TO ALL BARRIER VEHICLE REQUIREMENTS REGARDLESS OF THE PRESENCE OF BARRIER VEHICLES AT UPSTREAM WORK OPERATIONS.

4. NO WORK ACTIVITY, EQUIPMENT, VEHICLES AND/OR MATERIALS SHALL BE LOCATED BETWEEN THE BARRIER OR SHADOW VEHICLE AND THE ACTIVE WORK ARE (ROLL AHEAD DISTANCE).

5. THE CONTRACTOR MAY BE REQUIRED TO PROVIDE A BARRIER VEHICLE IN CONJUNCTION WITH POLICE PRESENCE IN THE WORK ZONE, TO BE INCLUDED IN THE UNIT BID PRICE FOR BASIC WORK ZONE TRAFFIC CONTROL.

TYPE III BARRICADES NOTE:

1. TYPE III BARRICADES SHALL BE USED FOR SIDEWALK CLOSURES AND FLAGGING OPERATIONS ON 2-LANE ROADS AT INTERSECTIONS AS PER NYSDOT STANDARD SHEETS 619-50, 619-51 AND 619-61.

CONTRACTORS VEHICLES AND EQUIPMENT NOTES:

1. VEHICLES OR EQUIPMENT BELONGING TO THE CONTRACTOR OR THE CONTRACTOR'S EMPLOYEES SHALL NOT BE PARKED WITHIN THE CLEAR ZONE, ON THE PAVEMENT SHOULDERS ALONG THE ROADWAY WHERE PARKING IS NOT NORMALLY PERMITTED, OR ANY OTHER AREAS DEEMED HAZARDOUS BY THE R.P.R..

2. VEHICLES OR EQUIPMENT BELONGING TO THE CONTRACTOR OR THE CONTRACTOR'S EMPLOYEES SHALL NOT BE PARKED IN A MANNER WHICH OBSTRUCTS SIGNS, BARRIERS, BARRICADES, OR OTHER TRAFFIC CONTROL DEVICES.

3. VEHICLES BELONGING TO THE CONTRACTOR OR THE CONTRACTOR'S EMPLOYEES ALSO SHALL NOT BE PARKED IN A MANNER WHICH INTERFERES WITH ACCESS TO ABUTTING PROPERTIES.

4. VEHICLES PARKED BEHIND GUIDE RAIL/ BARRIER SHALL BE PARKED BEYOND THE GUIDE RAIL/ BARRIER DEFLECTION DISTANCE.

5. THE CONTRACTOR SHALL PLAN AND INCORPORATE ACCESS POINTS INTO THE WORK ZONE SUCH THAT, TO THE EXTENT PRACTICAL, THE CONTRACTOR'S VEHICLES ENTERING AND LEAVING THE WORK ZONE SHALL NOT IMPEDE THE MOVEMENT OF THROUGH TRAFFIC IN THE ADJACENT OPEN LANES.

6. WHERE CONTRACTORS' VEHICLES ENTER OR EXIT A WORK ZONE A SPOTTER/FLAGGER MAY BE REQUIRED AS DETERMINED BY THE R.P.R. FLAGGER/SPOTTER SHALL MEET ALL REQUIREMENTS AS SPECIFIED IN FLAGGER NOTES. COST FOR THIS WORK SHALL BE INCLUDED IN THE PRICE BID FOR BASIC WORK ZONE TRAFFIC CONTROL, ITEM 619.01.

7. CONTRACTOR'S VEHICLES MUST AT ALL TIMES COMPLY WITH THE DESIGNATED TRAFFIC FLOW.

8. AT NO TIME SHALL THE CONTRACTOR OPERATE EQUIPMENT IN THE ACTIVE/OPEN TRAVEL LANE, IN THE OPPOSITE DIRECTION OF TRAFFIC BEING MAINTAINED, WITHOUT FIRST STOPPING TRAFFIC USING A FLAGGER.

9. IN THE EVENT THAT THE CONTRACTOR MUST LOAD A TRUCK IN THE ACTIVE/OPEN TRAVEL LANE, A FLAGGER SHALL BE USED TO STOP TRAFFIC. IN ACCODANCE WITH SECTION 619 OF THE NYSDOT STANDARD SPECIFICATIONS, TRAFFIC SHALL NOT BE STOPPED FOR MORE THAN 5 MINUTES WHILE WAITING FOR A TRUCK TO LOAD.

CONSTRUCTION AND TRAFFIC SIGNAGE NOTES:

1. ALL SIGNS AND TRAFFIC CONTROL DEVICES SHALL BE IN ACCORDANCE WITH THE N.M.U.T.C.D. WITH N.Y.S. SUPPLEMENT. THE LOCATIONS OF THE SIGNS SHOWN ON THE TRAFFIC CONTROL PLANS AND DETAILS, FOUND IN THE CONTRACT DOCUMENTS, ARE APPROXIMATE. THE EXACT LOCATIONS SHALL BE DETERMINED BY THE R.P.R.

2. ANY EXISTING SIGNS WHICH CONFLICT WITH THE WORK ZONE TRAFFIC CONTROL SIGN LAYOUT SHALL BE COVERED, REMOVED, STORED OR RESET, AS DETERMINED BY THE R.P.R.. BLANK COVERS USED TO COVER PORTIONS OF EXISTING SIGNS SHALL BE OF A COLOR AND REFLECTORIZED MATERIAL MATCHING THAT OF THE SIGN BEING PARTIALLY COVERED. THE CONTRACTOR SHALL BE LIABLE FOR ANY DAMAGE TO SIGNS CAUSED BY THE METHODS USED TO TEMPORARILY REMOVE, RELOCATE OR COVER SIGN PANELS OR SIGN TEXT, AT NO ADDITIONAL COST TO THE CITY. ALL APPROPRIATE EXISTING SIGNS SHALL BE RESTORED TO THEIR ORIGINAL CONDITION AND/OR LOCATION UNLESS OTHERWISE REPLACED IN THIS CONTRACT. THIS WORK SHALL BE INCLUDED IN THE PRICE BID FOR BASIC WORK ZONE TRAFFIC CONTROL, ITEM 619.01.

3. THE MODIFICATION, RELOCATION OR ADJUSTMENT OF EXISTING SIGNS FOR WORK ZONE TRAFFIC CONTROL PURPOSES, NOT ALREADY SHOWN ON THE PLANS, SHALL BE INCLUDED IN THE PRICE BID FOR BASIC WORK ZONE TRAFFIC CONTROL, ITEM 619.01.

4. SIGNS AT OR NEAR INTERSECTIONS SHALL BE PLACED SO THAT THEY DO NOT OBSTRUCT A MOTORIST'S LINE OF SIGHT.

5. THE COST FOR PROVIDING, INSTALLING, MAINTAINING, AND REMOVING CONSTRUCTION SIGNS SHALL BE INCLUDED IN THE AMOUNT BID FOR ITEM 619.01, BASIC WORK ZONE TRAFFIC CONTROL.

6. ALL EXISTING TRAFFIC AND PARKING SIGNS WITHIN THE CONTRACT LIMITS SHALL BE MAINTAINED BY THE CONTRACTOR THROUGHOUT THE ENTIRE PERIOD OF CONSTRUCTION UNTIL THESE SIGNS ARE REPLACED WITH PERMANENT SIGNS OR COVERED WHEN THEY DIRECTLY CONFLICT WITH THE MAINTENANCE AND PROTECTION OF TRAFFIC. THIS INCLUDES SIGNAGE AT INTERSECTING STREETS.

7. TRAFFIC MAINTENANCE SIGNS NOT CONSISTENT BETWEEN WORKING AND/OR NON-WORKING HOURS SHALL BE COVERED AND/OR REMOVED FROM THE DRIVER'S VISION WHEN NOT IN USE.

8. PORTABLE SIGN SUPPORTS WILL BE ALLOWED ONLY AT LOCATIONS DEPICTED ON THE DETAILS OR AT LOCATIONS APPROVED BY THE RESIDENT PROJECT REPRESENTATIVE. PORTABLE SIGN SUPPORTS SHALL ADHERE TO THE PROVISIONS OF THE N.M.U.T.C.D.. LATERAL PLACEMENT OF PORTABLE SIGNS SHALL BE SUCH TO ACHIEVE MAXIMUM VISIBILITY.

9. SIGN SUPPORTS SHALL BE SUCH AS TO RESIST OVERTURNING IN STRONG WINDS AND PROVIDE A MINIMUM MOUNTING HEIGHT OF 7'-0" MEASURED TO BOTTOM OF SIGN.

10. THE COVERING AND/OR REMOVING, RELOCATING, AND REPLACING OF EXISTING SIGN PANELS AND ASSEMBLIES SHALL BE AS DIRECTED BY THE RESIDENT PROJECT REPRESENTATIVE. PAYMENT SHALL BE INCLUDED IN THE PRICE BID FOR BASIC WORK ZONE TRAFFIC CONTROL, ITEM 619.01.

11. CARE SHOULD BE TAKEN NOT TO DAMAGE ANY EXISTING PERMANENT SIGNS TO REMAIN. ANY SIGN DAMAGED SHALL BE REPLACED BY THE CONTRACTOR, AT NO ADDITIONAL COST TO THE OWNER.

12. ALL CONSTRUCTION SIGNS SHALL BE NEW OR IN LIKE NEW CONDITION.

13. IF CONSTRUCTION RELATED TRAFFIC CONGESTION BACKS UP BEYOND THE INSTALLED ADVANCE SIGN SEQUENCE, ADDITIONAL ADVANCE SIGNING SHALL BE PLACED IN ADVANCE OF THE CONGESTION.

14. UNDER NO CIRCUMSTANCES SHALL A SIGN PANEL BE TRUNCATED OR TRIMMED SO THAT IT WILL NOT INTERFERE WITH THE TRAVEL WAY. IF THIS CONDITION EXISTS, THE R.P.R. WILL DIRECT THE CONTRACTOR TO RELOCATE THAT SIGN AS NOT TO CAUSE ANY OBSTRUCTION WITH THE TRAVEL WAY.

15. WARNING FLAGS ON SIGNS MAY BE USED TO INCREASE TARGET VALUE AND VISIBLY OF WORK ZONE SIGNS DURING DAYLIGHT HOURS.

16. CONTRACTOR MUST CONTACT MCDOT DISPATCHER AT (585) 753-7750 A MINIMUM OF FIVE (5) WORKING DAYS PRIOR TO REMOVING OR RESETTING ANY TRAFFIC OR PARKING REGULATION SIGNS.

PAVEMENT MARKINGS NOTES:

1. EXISTING PAVEMENT MARKINGS OUTSIDE OF PREVIOUSLY DISTURBED CONSTRUCTION ZONES SHALL BE MAINTAINED AND RE-STRIPED IF REQUIRED BY THE R.P.R.. THIS MAY INCLUDE, BUT IS NOT LIMITED TO RE-STRIPING THE EXISTING ROADWAY PRIOR TO OR DIRECTLY AFTER WINTER SHUTDOWN AND/OR RE-STRIPING AREAS THAT ARE NOT SCHEDULED FOR WORK UNTIL LATER PHASES OF CONSTRUCTION. EXISTING MARKINGS, LETTERS AND SYMBOLS WILL BE RE-APPLIED AS NECESSARY AS DETERMINED BY THE R.P.R.. PAVEMENT MARKINGS THAT HAVE BEEN OBLITERATED BY THE OPERATION OF UTILITY COMPANIES SHALL BE MAINTAINED AS ORDER BY THE R.P.R..

2. TEMPORARY LONG LINE MARKINGS (ITEM 619.0901) SHALL BE APPLIED WITHIN 24 HOURS OF COMPLETING MILLING OPERATIONS ON EACH STREET. MARKINGS SHALL INCLUDE 4" YELLOW FULL BARRIER CENTERLINE ON ALL STREETS.

3. PERMANENT LONG LINE MARKINGS SHALL BE APPLIED WITHIN 24 HOURS OF PLACING TOP COURSE HMA.

4. THE CONTRACTOR SHALL MAINTAIN EXISTING PAVEMENT MARKINGS WITHIN THE CONSTRUCTION LIMITS AS DETERMINED BY THE R.P.R. PAYMENT SHALL BE INCLUDED UNDER THE APPROPRIATE PAVEMENT MARKING ITEMS.

DELINEATION AND GUIDING DEVICES NOTES:

1. PAYMENT FOR CHANNELIZATION DEVICES IS INCLUDED UNDER ITEM 619.01, BASIC WORK ZONE TRAFFIC CONTROL, UNLESS PROVIDED FOR WITH SPECIFIC ITEMS IN THE CONTRACT DRAWINGS AND SPECIFICATIONS.

2. DELINEATION AND GUIDING DEVICES AT OR NEAR INTERSECTIONS SHALL BE PLACED SO THAT THEY DO NOT OBSTRUCT A MOTORIST'S LINE OF SIGHT.

3. WHERE POSSIBLE ALL DELINEATION AND GUIDING DEVICES ARE TO BE PLACED SO AS TO PROVIDE A MINIMUM 2'-0" CLEARANCE TO THE TRAVELED WAY UNLESS OTHERWISE NOTED.

4. ALL TRAFFIC CONTROL DEVICES INITIALLY FURNISHED FOR THIS PROJECT SHALL BE IN ACCEPTABLE CONDITION AS DEFINED BY THE GUIDELINES PAMPHLET DEVELOPED BY THE AMERICAN TRAFFIC SAFETY SERVICE ASSOCIATION (ATSSA). AS THE CONDITION OF DEVICES DETERIORATE OVER TIME, THOSE THAT FALL BELOW THE MARGINAL CATEGORY SHALL BE REPLACED.

OPEN CUTS IN PAVEMENT NOTE:

1. ALL ROADWAY AREAS TEMPORARILY CLOSED FOR FULL DEPTH REPAIRS, TRENCHING FOR CURB WORK OR EXCAVATION REQUIRED FOR DRAINAGE STRUCTURE ADJUSTMENT, SHALL BE RE-OPENED AT THE END OF THE WORK DAY. AT ALL TIMES WHEN WORK IS NOT IN PROGRESS, ALL OPENINGS SHALL BE COVERED WITH ANCHORED STEEL PLATES OR SHALL BE BACKFILLED IN ACCORDANCE WITH SECTION 203-3.15 OR TO A DEPTH AS ORDER BY THE R.P.R., IF STEEL PLATES ARE THE CHOSEN OPTION. THE CONTRACTOR SHALL SUBMIT THE PROPOSED METHOD OF ANCHORING THE STEEL PLATES TO THE R.P.R FOR APPROVAL PRIOR TO STARTING ANY ROADWAY EXCAVATION. THE STEEL PLATES SHALL BE RAMPED WITH A BITUMINOUS MATERIAL PRIOR TO OPENING THE LANES. THE COST OF THIS WORK INCLUDING ALL LABOR, MATERIALS EQUIPMENT AND ANY RE-EXCAVATION OR TEMPORARY RESTORED AREAS SHALL BE INCLUDED IN THE UNIT PRICE BID FOR ITEM 619.01, BASIC WORK ZONE TRAFFIC CONTROL.

NYSDOT STANDARD SHEETS FOR WORK ZONE TRAFFIC CONTROL:

1. THE CONSTRUCTOR SHALL REFERENCE THE FOLLOWING NYSDOT STANDARD SHEETS FOR WORK ZONE TRAFFIC CONTROL FOR THE PROJECT:
   - 619-02 TYPE III CONSTRUCTION BARRICADES
   - 619-04 PORTABLE TEMPORARY WOODEN SIGN SUPPORT
   - 619-10 WORK ZONE TRAFFIC CONTROL GENERAL NOTES
   - 619-11 WORK ZONE TRAFFIC CONTROL LEGEND AND NOTES
   - 619-12 SIGN TABLE
   - 619-20 SHOULDER CLOSURE 2-LANE 2-WAY ROADWAY
   - 619-21 SHOULDER CLOSURE 2-LANE 2-WAY ROADWAY MULTIPLE LOCATIONS
   - 619-50 SIDEWALK DETOUR OR DIVERSION
   - 619-51 SIDEWALK CLOSURE AND PEDESTRIAN DETOUR
   - 619-60 FLAGGING OPERATION - 2-LANE 2-WAY ROADWAY
   - 619-61 FLAGGING OPERATION - 2-LANE 2-WAY ROADWAY INTERSECTION

TIME RESTRICTION AND DATE RESTRICTION NOTES:

1. THE FOLLOWING DATE RESTRICTIONS APPLY TO THIS PROJECT:

   NO WORK (EXCEPT EMERGENCY REPAIRS) SHALL BE PERMITTED ON THE FOLLOWING DAYS:

   MAY 27, 2019 - MEMORIAL DAY
   JULY 4, 2019 - INDEPENDENCE DAY
   SEPTEMBER 2, 2019 - LABOR DAY
   NOVEMBER 28, 2019 - THANKSGIVING DAY

2. NO NIGHTTIME WORK WILL BE ALLOWED UNLESS PRIOR APPROVAL IS GIVEN BY THE CITY. ADDITIONAL WORK ZONE TRAFFIC CONTROL WILL BE REQUIRED INCLUDING THE ADDITION OF REFLECTIVE MATERIALS AND LIGHTING.

3. NORMAL WORKING HOURS ARE FROM 7:00AM TO 5:00PM WEEKDAYS. THE CONTRACTOR SHALL NOT CLOSE ANY TRAVEL LANES WITHIN PROJECT LIMITS DURING THE FOLLOWING PEAK TRAFFIC HOURS:

   WEST OF GENESEE RIVER: EASTBOUND TRAFFIC (7AM TO 9AM),
   WESTBOUND TRAFFIC (4PM TO 6PM)
   EAST OF GENESEE RIVER: EB & WB TRAFFIC (7AM TO 9AM & 4PM TO 6PM)

4. THE CONTRACTOR SHALL PROVIDE SAFE AND CONTINUOUS ACCESS FOR VEHICULAR, BICYCLE, AND PEDESTRIAN TRAFFIC THROUGHOUT THE PROJECT LIMITS DURING THESE HOLIDAYS. A MINIMUM OF ONE (1) THROUGH TRAVEL LANES SHALL BE MAINTAINED IN EACH DIRECTION AND IN ALL AREAS. NO WORK THAT INTERRUPTS TRAFFIC FLOW THROUGH THE PROJECT WILL BE ACCEPTABLE DURING THESE PERIODS UNLESS OTHERWISE INDICATED ON THE PLANS.

FILE NAME = Work Zone Traffic Control Notes.dgn
DATE/TIME = 12-MAR-2019 11:18
USER = JMessenger

Lu Engineers
ENVIRONMENTAL • TRANSPORTATION • CIVIL
PROJECT MANAGER: J. OTTMAN
DRAWN BY: J. MESSENGER

Department of Environmental Services
Architecture and Engineering Services
City of Rochester, New York
ASSISTANT MANAGING ENGINEER: LISA Y. REYES
CITY ENGINEER: HOLLY E. BARRETT, P.E.

PROJECT TITLE
SCOTTSVILLE ROAD - ELMWOOD AVENUE
IMPROVEMENT PROJECT
(WEST CITY LINE - MT HOPE AVE)

NO.	REVISION	BY	DATE

ISSUED	03/06/2019
CHECKED	JWO
DRAWN	JRM
DESIGN	JRM
SCALE	NONE
PROJECT NUMBER	17102

DRAWING TITLE
WORK ZONE TRAFFIC
CONTROL NOTES
WZTC-1

DRAWING NO.
11
OF
54

PUBLIC ACCESS NOTES:

1. PEDESTRIAN TRAFFIC SHALL BE MAINTAINED AT ALL TIMES DURING CONSTRUCTION. THE CONTRACTOR SHALL FOLLOW THE STANDARD SHEETS TO CROSS PEDESTRIANS AT THE CLOSEST CROSSWALK POSSIBLE.

2. ACCESS TO RESIDENTIAL AND COMMERCIAL DRIVEWAYS SHALL BE MAINTAINED AT ALL TIMES, UNLESS OTHER AGREEMENTS, SUITABLE TO THE OWNERS, CAN BE MADE. PROPERTY OWNERS WHOSE DRIVEWAYS WILL BE MADE INACCESSIBLE SHALL BE NOTIFIED BY THE CONTRACTOR AT LEAST 24 HOURS PRIOR TO RESTRICTING USE OF THE DRIVE. THE CONTRACTOR SHALL MAINTAIN ACCESS TO COMMERCIAL DRIVEWAYS AT ALL TIMES WHEN A FACILITY IS IN USE. FOR MULTIPLE ACCESS PROPERTIES, ONLY ONE DRIVEWAY MAY BE CLOSED AT ONE TIME. ACCESS SHALL BE RESTORED TO ALL DRIVEWAYS AS SOON AS POSSIBLE.

3. WHEN DIRECT ACCESS TO DRIVEWAYS IS NOT POSSIBLE DUE TO NECESSARY CONSTRUCTION OPERATIONS, THE CONTRACTOR SHALL PLAN ALTERNATIVE MEANS OF ACCESS AND SUBMIT SUCH PLANS TO THE R.P.R. FOR APPROVAL. COST FOR THIS WORK SHALL BE INCLUDED IN THE PRICE BID FOR BASIC WORK ZONE TRAFFIC CONTROL, ITEM 619.01.

4. SUITABLE RAMPS SHALL BE INSTALLED TO MAINTAIN SMOOTH TRANSITIONS FROM RESIDENTIAL AND COMMERCIAL DRIVEWAYS TO AND FROM THE WORK AREA. THE MATERIAL SHALL CONSIST OF CRUSHED STONE OR ASPHALT CONCRETE, WHICHEVER IS DEEMED MORE APPROPRIATE FOR THE CIRCUMSTANCE, AS DETERMINED BY THE R.P.R. PLASTIC DRUMS OR OTHER APPROVED TRAFFIC CONTROL DEVICES SHALL BE USED TO DELINEATE THE DRIVEWAY AND TO CONTROL ACCESS DURING NON-WORKING HOURS. THE COST OF THIS WORK SHALL BE INCLUDED IN THE PRICE BID FOR BASIC WORK ZONE TRAFFIC CONTROL, ITEM 619.01.

NOTIFICATIONS AND EMERGENCY ACCESS NOTES:

1. RESPONSIBILITY FOR EMERGENCY REPAIRS - THE CONTRACTOR SHALL SUBMIT TO THE APPROPRIATE LAW ENFORCEMENT AND EMERGENCY AGENCIES IN WRITING THE NAME, ADDRESS, AND TELEPHONE NUMBER OF THE PERSON OR PERSONS AUTHORIZED TO SECURE LABOR, MATERIALS, AND EQUIPMENT FOR EMERGENCY REPAIRS OUTSIDE OF NORMAL WORKING HOURS. DUPLICATE COPIES OF THE ABOVE SHALL BE FILED WITH THE RESIDENT PROJECT REPRESENTATIVE.

2. THE CONTRACTOR IS REQUIRED TO CONTACT THE APPROPRIATE SCHOOL AND EMERGENCY SERVICES ORGANIZATIONS WITH RESPECT TO THE EFFECT OF ROAD WORK, TRAVEL LANE REDUCTIONS, AND DETOURS. THIS CONTACT SHALL BE MADE AS CONDITIONS CHANGE AND AT LEAST 21 DAYS PRIOR TO IMPLEMENTING EACH TRAFFIC PHASE AND/OR DETOUR TO ALLOW ADEQUATE TIME FOR THE ORGANIZATIONS TO COORDINATE AND MAKE NECESSARY ADJUSTMENTS TO RESPONSE SCHEDULES AND ROUTES.

3. THE CONTRACTOR SHALL NOTIFY LOCAL AUTHORITIES RESPONSIBLE FOR EMERGENCY SERVICES IN ADVANCE OF ANY SUBSTANTIAL CHANGE IN TRAFFIC PATTERNS.

4. AT LEAST ONE 10 FEET WIDE TRAVEL LANE OF SAFELY TRAVERSABLE ROAD IN EACH DIRECTION OR ONE 10 FEET WIDE TRAVEL LANE OF SAFELY TRAVERSABLE ROAD WITH THE USE OF FLAGGERS MUST BE MAINTAINED THROUGHOUT THE PROJECT AREA AT ALL TIMES FOR EMERGENCY VEHICLES. WORK ON ONE SIDE OF THE HIGHWAY SHALL BE COMPLETED PRIOR TO ESTABLISHING A WORK AREA ON THE OPPOSITE SIDE.

5. ACCESS TO THE STRONG MEMORIAL HOSPITAL EMERGENCY DEPARTMENT DRIVEWAYS MUST REMAIN UNIMPEDED AT ALL TIMES.

FLAGGERS NOTES:

1. THE USE OF TEMPORARY SIGNALS AS A SUBSTITUTE FOR FLAGGERS WILL NOT BE ALLOWED ON THIS PROJECT.

2. FLAGGING OPERATIONS FOR THIS PROJECT SHALL BE IN ACCORDANCE WITH THE N.M.U.T.C.D. WITH N.Y.S. SUPPLEMENT, AND SECTION 619 OF THE STANDARD SPECIFICATIONS.

3. WHEN FLAGGERS ARE BEING USED FOR TRAFFIC CONTROL PURPOSES, FLAGGER SIGNS (W20-7) SHALL BE PLACED ON ALL APPROACHES TO THE AREAS BEING FLAGGED. THE COST OF FLAGGER SIGNS SHALL BE INCLUDED IN THE UNIT PRICE BID FOR ITEM 619.01, BASIC WORK ZONE TRAFFIC CONTROL. FLAGGER SIGNS SHALL NOT BE USED FOR BRIEF PERIODS OF INCIDENTAL FLAGGING, AS DETERMINED BY THE R.P.R. FLAGGER SIGNS SHALL NOT BE VISIBLE WHEN FLAGGERS ARE NOT BEING USED.

4. STOP / SLOW PADDLES SHALL BE USED FOR FLAGGING AND SHALL CONFORM TO THE REQUIREMENTS IN THE N.M.U.T.C.D. BUT SHALL BE A MINIMUM OF 24 INCHES. THE COST OF THESE PADDLES AND ALL FLAGGERS SHALL BE INCLUDED IN THE UNIT PRICE BID FOR ITEM 619.01, BASIC WORK ZONE TRAFFIC CONTROL.

5. FLAGGERS SHALL NOT POSITION THEMSELVES DIRECTLY IN THE PATH OF ONCOMING TRAFFIC. THE FLAGGER SHOULD STAND EITHER ON THE SHOULDER ADJACENT TO THE TRAFFIC BEING CONTROLLED OR IN THE CLOSED / CHANNELIZED LANE. AT ALL TIMES THE CONTRACTOR SHALL PROVIDE FLAGGERS WITH A CLEAR ESCAPE PATH, FREE OF PARKED VEHICLES, EQUIPMENT OR OTHER OBSTACLES.

6. THE CONTRACTOR SHALL PROVIDE FLAGGERS WHERE SIGHT DISTANCES ARE IMPAIRED BY THE OPERATION OR IN ANY OTHER SITUATION AS DETERMINED BY THE R.P.R..

7. FLAGGERS SHALL BE REQUIRED TO USE TWO-WAY RADIOS, WALKIE-TALKIES OR OTHER FORMS OF ENHANCED COMMUNICATION WHEN ONE FLAGGER IS NOT VISABLE TO THE OTHER OR IF THE R.P.R. DEEMS IT NECESSARY. ALL COSTS SHALL BE INCLUDED IN THE UNIT PRICE BID FOR ITEM 619.01, BASIC WORK ZONE TRAFFIC CONTROL.

FLAGGERS NOTES:

8. FLAGGERS SHALL USE THE FREE-HAND FOR EMPHASIS AS SHOWN IN PART 6 OF THE N.M.U.T.C.D.. EACH FLAGGER SHALL CONTROL NO MORE THAN ONE APPROACH OF TRAFFIC UNLESS THE APPROACHES PRESENT UNUSUALLY LOW SPEEDS AND/OR UNUSUALLY LOW VOLUMES, WITH ADEQUATE SIGHT DISTANCE FOR SAFE HANDLING OF TRAFFIC, AS DETERMINED BY THE R.P.R..

9. THE R.P.R. SHALL DETERMINE THE NUMBER OF FLAGGERS FOR EACH WORK ZONE. UNDER CERTIAN GROMETRIC AND TRAFFIC SITUATIONS, MORE THAN ONE FLAGGER STATION MAY BE REQUIRED FOR EACH DIRECTION OF TRAFFIC. AT TWO-WAY UNUSUALLY LOW VOLUME AND/OR UNUSUALLY LOW SPEED SHORT LANE CLOSURES, WHERE ADEQUATE SIGHT DISTANCES AVAILABLE FOR SAFE HANDLING OF TRAFFIC, THE USE OF ONE FLAGGER MAY BE SUFFICIENT. IN THIS SITUATION, A POSTITION MAY HAVE TO BE TAKEN OPPOSITE THE WORK TO OPERATE EFFECTIVELY.

10. FLAGGER STATIONS SHOULD BE VISABLE FAR ENOUGH AHEAD TO PERMIT ALL VEHICLES TO STOP. THE FLAGGER SHOULD BE STATIONED FAR ENOUGH AHEAD OF THE WORK TO WARN THEM (FOR EXAMPLE WITH HORNS, WHISTLES, ETC.) OF APPROACHING DANGER, SUCH AS VEHICLES OUT OF CONTROL.

11. FLAGGERS SHALL BE PROVIDED WITH HAND HELD AUDIBLE ALARMS TO WARN OF VEHICLE INTRUSIONS.

12. WHEN A SIDE ROAD OR DRIVEWAY INTERSECTS THE HIGHWAY WITHIN THE TEMPORARY TRAFFIC CONTROL ZONE ADDITIONAL TRAFFIC CONTROL DEVICES AND FLAGGERS SHALL BE PROVIDED.

PEDESTRIAN NOTES:

1. THE CONTRACTOR SHALL PROVIDE, DELINEATE AND MAINTAIN A USABLE PATH OF TRAVEL FOR PEDESTRIANS EITHER THROUGH OR AROUND WORK ZONES FOR THE DURATION OF THIS PROJECT. THE PATH OF TRAVEL SHALL COMPLY WITH THE "CONTINUOUS PASSAGE" PROVISIONS OF THE CURRENT AMERICANS WITH DISABILITIES ACT ACCESSIBILITY GUIDELINES (ADAAG) AND THE 2011 PROPOSED ACCESSIBILITY GUIDELINES FOR PEDESTRIAN FACILITIES IN THE PUBLIC RIGHT OF WAY (PROWAG). THE CONTRACTOR MAY UTILIZE ANY COMBINATION OF THE FOLLOWING TO PROVIDE THE USABLE PATH OF TRAVEL: EXISTING SIDEWALKS/SHOULDERS; TEMPORARY SIDEWALKS/SHOULDERS; FINAL SIDEWALKS/SHOULDERS; AND EXISTING OR TEMPORARY SIDEWALK CURB RAMPS. THE USABLE TRAVEL PATH SHALL BE CLEARLY MARKED AND STAGED BETWEEN INTERSECTING ROADS TO ENSURE SAFE PEDESTRIAN CROSSING AND ACCESSIBILITY. PEDESTRIAN ACCESS TO RESIDENTIAL AND COMMERCIAL PROPERTIES SHALL BE MAINTAINED FOR THE DURATION OF CONSTRUCTION.

2. THE CONTRACTOR SHALL CONDUCT WORK OPERATIONS TO MINIMIZE THE INTERRUPTION OF PEDESTRIAN TRAFFIC. THE TEMPORARY STORAGE OF CONSTRUCTION EQUIPMENT OR MATERIALS SHALL NOT BE PERMITTED ON ANY USABLE TRAVEL PATH THAT IS OPEN TO PEDESTRIAN TRAFFIC.

3. IF THE USABLE TRAVEL PATH IS LESS THAN 5.0 FT WIDE AND OVER 200 FT IN LENGTH, THE CONTRACTOR SHALL FURNISH A 5.0 FT BY 5.0 FT AREA WHERE WHEELCHAIRS CAN PASS EVERY 200 FT.

4. IF TEMPORARY SIDEWALK IS CONSTRUCTED AS PART OF THE USABLE TRAVEL PATH, IT SHALL BE ASPHALT CONCRETE MEETING THE REQUIREMENTS OF SECTION 608-1 OF THE STANDARD SPECIFICATIONS. ALL TEMPORARY SIDEWALK SHALL MEET THE FOLLOWING CRITERIA, IN ADDITION TO THOSE CONTAINED IN THE ADAAG AND PROWAG:

   A. MINIMUM SIDEWALK WIDTH = 5.0 FT
   B. MAXIMUM SIDEWALK CROSS SLOPE = 1.5% (1:66.6)
   C. THICKNESS OF ASPHALT CONCRETE SIDEWALK = 2.0 in
   D. IF TEMPORARY SIDEWALK IS TO BE INSTALLED IN AREAS OF EXISTING GRASS, STRIP TURF TO A DEPTH OF 2.0 in BEFORE INSTALLATION.

5. IF TEMPORARY SIDEWALK IS CONSTRUCTED AS PART OF THE USABLE TRAVEL PATH, TEMPORARY SIDEWALK CURB RAMPS SHALL BE PROVIDED AT CURBED CROSSINGS. TEMPORARY CURB RAMPS SHALL BE CONSTRUCTED USING ASPHALT CONCRETE MEETING THE REQUIREMENTS OF SECTION 608-1 OF THE STANDARD SPECIFICATIONS. ALL TEMPORARY CURB RAMPS SHALL MEET THE FOLLOWING CRITERIA, IN ADDITION TO THOSE CONTAINED IN THE ADAAG PROWAG :

   A. MINIMUM CURB RAMP WIDTH = 5.0 FT
   B. MAXIMUM CURB RAMP RUNNING SLOPE = 8.33% (1:12)
   C. MAXIMUM CURB RAMP CROSS SLOPE = 1.5% (1:66.6)
   D. THICKNESS OF ASPHALT CONCRETE CURB RAMP = 2.0 in
   E. IF TEMPORARY CURB RAMPS ARE TO BE INSTALLED IN AREAS OF EXISTING GRASS, STRIP TURF TO A DEPTH OF 2.0 in BEFORE INSTALLATION.

6. ALL LABOR, MATERIALS AND EQUIPMENT NECESSARY TO INSTALL AND REMOVE TEMPORARY SIDEWALK AND CURB RAMPS, INCLUDING THE RESTORATION OF DISTURBED AREAS, SHALL BE INCLUDED IN THE PRICE BID FOR BASIC WORK ZONE TRAFFIC CONTROL, ITEM 619.01.

7. "SIDEWALK CLOSED" SIGNS IF REQUIRED SHALL BE PLACED WHEN A USABLE TRAVEL PATH IS CLOSED TO PEDESTRIAN TRAFFIC. ADDITIONAL SIGNS SHALL BE PLACED TO DIRECT PEDESTRIANS TO AN ALTERNATE ACCESSIBLE ROUTE. ALL SIGNS SHALL BE INCLUDED IN THE PRICE BID FOR BASIC WORK ZONE TRAFFIC CONTROL, ITEM 619.01.

PEDESTRIAN NOTES (CONTINUED):

8. FOR LOCATIONS WHERE EXISTING SIDEWALK WAS REMOVED OR NEW SIDEWALKS ARE PROPOSED, THE CONTRACTOR SHALL PROVIDE A COMPACTED SURFACE MEETING ADAAG REQUIREMENTS WITHIN TWO WEEKS (14 CALENDAR DAYS) OF CURB INSTALLATION. THE CONTRACTOR MAY ALLOW PEDESTRIAN TRAFFIC TO USE THE COMPACTED SURFACE FOR A MAXIMUM OF TWO WEEKS (14 CALENDAR DAYS). AFTER THE TWO WEEK PERIOD THE CONTRACTOR MUST PROVIDE A PAVED SURFACE (FINAL OR TEMPORARY) MEETING THE ADAAG REQUIREMENTS. IF THE CONTRACTOR CHOOSES NOT TO PLACE THE FINAL SIDEWALK SURFACE, NO SEPARATE PAYMENT WILL BE MADE FOR THE TEMPORARY SURFACE. THE CONTRACTOR MUST ALSO PROVIDE CURB RAMPS IN ACCORDANCE WITH ADAAG AND PROWAG REQUIREMENTS. IF THE CONTRACTOR CHOOSES TO PROVIDE THE FINAL SIDEWALK SURFACE AFTER THE TWO WEEK PERIOD, A TEMPORARY CURB RAMP MAY BE INSTALLED. THE COST OF THIS TEMPORARY RAMP IS TO BE INCLUDED IN THE PRICE BID FOR BASIC WORK ZONE TRAFFIC CONTROL, ITEM 619.01.

9. ANY CONSTRUCTION ACTIVITY THAT RESULTS IN THE BREAKAGE OF SLABS OR THE REMOVAL OF SEGMENTS OF EXISTING SIDEWALK SERVING AS THE USABLE TRAVEL PATH SHALL BE REPAIRED IMMEDIATELY USING ASPHALT CONCRETE MEETING THE REQUIREMENTS OF SECTION 608 OF THE STANDARD SPECIFICATIONS. THE REPAIRED SURFACE SHALL BE IN CONFORMANCE WITH THE CRITERIA OUTLINED ABOVE, AS WELL AS THOSE CONTAINED IN THE ADAAG. SIDEWALK REPAIRS DUE TO REQUIRED CONSTRUCTION ACTIVITIES SHALL BE PAID FOR UNDER ASPHALT CONCRETE SIDEWALKS, DRIVEWAYS AND BICYCLE PATHS. SIDEWALK THAT IS DAMAGED BY CARELESS CONSTRUCTION ACTIVITIES OR PRACTICES SHALL BE IMMEDIATELY REPAIRED, AS DETERMINED BY THE R.P.R., AT NO COST TO THE CITY.

10. TEMPORARY SAFETY FENCING SHALL BE PLACED ALONG ANY SIDEWALK ADJACENT TO ANY EXCAVATION OR VERTICAL DROP-OFF OVER 6 in. SAFETY FENCING SHALL BE CONTINUOUS PLASTIC FLUORESCENT ORANGE SUPPORTED BY METAL OR WOOD POSTS. THE FENCING SHOULD BE A MINIMUM OF 4.0 FT IN HEIGHT AND MAY INCLUDE A TOP AND BOTTOM RAIL TO PROVIDE ADDITIONAL SUPPORT, AS DETERMINED BY THE R.P.R. THE SAFETY FENCE SHALL CONFORM TO SECTION 107-05 OF THE STANDARD SPECIFICATIONS. ALL LABOR, MATERIALS AND EQUIPMENT NECESSARY TO INSTALL AND REMOVE TEMPORARY SAFETY FENCING SHALL BE INCLUDED IN THE PRICE BID FOR BASIC WORK ZONE TRAFFIC CONTROL, ITEM 619.01.

11. THE INSTALLATION OF TEMPORARY/FINAL SIDEWALK SHALL BE STAGED SO THAT WORK IS STARTED AND COMPLETED ON ONE SIDE OF THE ROAD PRIOR TO THE START OF WORK ON THE OPPOSITE SIDE OF THE ROAD. WHEN A SECTION OF THE SIDEWALK/USABLE TRAVEL PATH MUST BE CLOSED AND PEDESTRIAN TRAFFIC IS SHIFTED TO THE SIDEWALK/USABLE TRAVEL PATH ON THE OPPOSITE SIDE OF THE ROAD, PEDESTRIANS SHALL BE DIRECTED TO CROSS AT THE NEXT AVAILABLE INTERSECTION. AT NO TIME SHALL TEMPORARY MID-BLOCK PEDESTRIAN CROSSINGS BE PERMITTED.

LANE CLOSURE NOTES:

1. A SINGLE, LONG LANE CLOSURE IS PREFERRED OVER A SERIES OF SUCCESSIVE SHORTER LANE CLOSURES. THE LENGTH OF THE LANE CLOSURE SHALL BE THE MINIMUM NECESSARY TO ACCOMPLISH THE PLANNED WORK SAFELY AND EFFICIENTLY. THE APPROPRIATE LENGTH OF LANE CLOSURE SHALL BE DETERMINED BY THE CONTRACTOR AND APPROVED BY THE R.P.R..

2. THE CONTRACTOR SHALL START SHORT TERM LANE CLOSURES TO PROVIDE OPTIMUM VISIBILITY, (I.E. BEFORE CURVES AND CRESTS), TO THE EXTENT PROJECT AND TRAFFIC CONDITIONS PERMIT.

3. THE CONTRACTOR SHALL NOT MIX CHANNELIZATION DEVICES IN A LINEAR CLOSURE OR TAPER (I.E. CONES, VERTICAL PANELS AND DRUMS SHALL NOT BE USED IN THE SAME TAPER OR CLOSURE). HOWEVER, DIFFERENT CHANNELIZATION DEVICES MAY BE USED IN DIFFERENT AREAS OF A PROJECT.

4. IF, IN THE OPINION OF THE R.P.R., SHORT TERM LANE CLOSURES ARE CREATING EXCESSIVE DELAYS TO TRAFFIC OR ARE CREATING A SAFETY CONCERN DUE TO SLOW OR STOPPED TRAFFIC, AFFECTED LANE CLOSURES SHALL BE REMOVED AND THE ROADWAY OPENED TO TRAFFIC.

5. THE R.P.R. MAY REQUIRE THAT ALL LANES BE RE-OPENED AT ANY TIME IF THE ROUTE IS NEEDED FOR EMERGENCY PURPOSES. THIS COULD INCLUDE INCIDENTS AT LOCATIONS NOT WITHIN THE PROJECT LIMITS.

6. ANY ROAD CLOSINGS FOR ANY REASON SHALL BE APPROVED BY THE CITY.

TRAVEL LANE WIDTH NOTES:

1. A MINIMUM TRAVEL LANE WIDTH OF 10' FEET SHALL BE MAINTAINED ON ALL ROADWAYS WITHIN THE PROJECT LIMITS AT ALL TIMES, UNLESS OTHERWISE NOTED.

2. THE CONTRACTOR SHALL PROVIDE A WRITTEN NOTICE TO THE ENGINEER, A MINIMUM OF 21 CALENDAR DAYS IN ADVANCE OF PERFORMING ANY WORK THAT RESULTS IN THE REDUCED WIDTH OF AN EXISTING ROADWAY, SO THAT THE ENGINEER MAY NOTIFY THE REGIONAL PERMIT ENGINEER IN A TIMELY MANNER.

SURFACE CONDITIONS RESULTING IN BUMPS, DIPS AND/OR DROP-OFFS:

1. DURING THE COURSE OF THIS PROJECT, THE CONTRACTOR SHALL KEEP THE TRAVEL WAY FREE OF BUMPS THAT MAY BE CREATED BY THEIR WORK OPERATIONS (MILLING, TERMINATION AREAS & BITUMINOUS CONCRETE PAVING). TRANSVERSE VERTICAL FACES BETWEEN PAVEMENT SURFACES THAT EXCEED 1 INCH IN HEIGHT SHALL BE SLOPED OR TAPERED BEFORE THE WORK AREA IS OPEN TO TRAFFIC.

2. TEMPORARY SLOPED TAPERS REQUIRED PER STANDARD SPECIFICATION 619-3.02 SHALL BE CONSTRUCTED FROM BITUMINOUS PATCHING MATERIAL (PLANT MIX OR SUITABLE WINTER MIX). THESE TAPERS SHALL BE MAINTAINED AND REMAIN IN PLACE UNTIL THE NEED FOR TAPERING THE ELIMINATED BY SUBSEQUENT WORK. THE COST FOR THIS WORK SHALL BE INCLUDED IN THE PRICE BID FOR BASIC WORK ZONE TRAFFIC CONTROL.

3. "BUMP" WARNING SIGNS (W8-1) WITH A SUPPLEMENTAL SIGN (W13-1) SHALL BE POSTED UPSTREAM AND A DRUM WITH TYPE A WARNING LIGHT SHALL BE PLACED ADJACENT TO THE CONDITION. THE POSTED SPEED FOR THE W13-1 SIGN SHALL BE 10 MPH LESS THAN THE POSTED SPEED FOR THE AREA. REFER TO THE NMUTCD TABLE 2C-4 FOR ADVANCE POSTING DISTANCE. THE COST FOR THIS WORK IS TO BE INCLUDED IN THE LUMP SUM PRICE BID FOR ITEM 619.01. OTHER WORK ACTIVITIES AT WHICH THE TRAVELING PUBLIC MAY ENCOUNTER A BUMP WHICH IS SUFFICIENTLY ABRUPT TO CAUSE THE DEFLECTION OF THEIR VEHICLE AT PREVAILING DRIVING SPEEDS (EX. TERMINAL ENDS OF PAVEMENT RECONSTRUCTION WORK) WILL ALSO REQUIRE A "BUMP" SIGN AND DRUM WITH TYPE A WARNING LIGHT (AS ORDER BY THE R.P.R.) AREAS SUCH AS THESE SHOULD CONTAIN A RAMP OF SUFFICIENT LENGTH SUCH THAT THERE IS NO CONSIDERABLE DISCOMFORT FELT BY THE MOTORIST.

4. THE CONTRACTOR SHALL NOT RUN TRAFFIC ON AN UNPAVED SURFACE FOR MORE THAN 14 DAYS.

5. WHEN MAINTAINING TRAFFIC ON PAVED OR MILLED SURFACES, THE CONTRACTOR SHALL MARK ALL HAZARDS PROJECTING UP FROM THE DRIVING SURFACE (I.E. VALVES, MANHOLES, ETC.) THE CONTRACTOR SHALL PROVIDE TEMPORARY SLOPED TAPERS FOR ALL VALVES, MANHOLES, ETC. LOCATED WITHIN THE TRAVEL LANES. THE CONTRACTOR IS RESPONSIBLE FOR PLACEMENT AND REMOVAL OF THESE TAPERS. TEMPORARY TAPERS SHALL BE CONSTRUCTED FROM BITUMINOUS PATCHING MATERIAL (PLANT MIX OR SUITABLE WINTER MIX). FOR VALVES, MANHOLES, ETC. LOCATED OUTSIDE THE ACTUAL TRAVEL LANES (I.E. SHOULDERS, CENTERLINE) THE CONTRACTOR MAY MARK THE FEATURE AS A HAZARD WITH A DRUM, IF TAPERING IS NOT POSSIBLE. ALL VALVES, MANHOLES, ETC. LOCATED WITHIN AN INTERSECTION SHALL BE TAPERED THESE TAPERS SHALL BE MAINTAINED AND REMAIN IN PLACE UNTIL THE NEED FOR TAPERING IS ELIMINATED BY SUBSEQUENT WORK. THE COST OF THIS WORK INCLUDING REMOVAL OF TAPERS, SHALL BE INCLUDED IN THE PRICE BID FOR BASIC WORK ZONE TRAFFIC CONTROL.

6. WHEN TRAFFIC IS MAINTAINED ON A ROADWAY DURING MILLING OPERATIONS, THE CONTRACTOR SHALL NOT LEAVE LONGITUDINAL DROP-OFFS EXPOSED TO TRAFFIC AT THE END OF THE DAY.

7. SUITABLE RAMPS (AS DIRECTED BY THE R.P.R.) OR SUBBASE MATERIAL SHALL BE USED BY THE CONTRACTOR TO PROVIDE ACCESS TO DRIVEWAYS AND PEDESTRIAN RAMPS WITHIN BOXED OUT AREAS. PLASTIC DRUMS OR OTHER APPROVED DELINEATION DEVICES SHALL FLANK OUT THESE DRIVEWAY CROSSINGS TO CONTROL ACCESS DURING NON-WORKING HOURS. PAYMENT FOR THESE RAMPS SHALL BE INCLUDED IN THE UNIT PRICE BID FOR ITEM 619.01, BASIC WORK ZONE TRAFFIC CONTROL

8. RAMPS PLACED AT PEDESTRIAN CROSSINGS SHALL BE FLUSH (HAVE NO REVEAL).

9. WHEN TRAFFIC IS TO BE MAINTAINED NEXT TO A DROP-OFF THAT IS 0-24 INCHES, OR WHERE SOFT OR UNSAFE AREAS OCCUR, AN APPROVED DELINEATOR SHALL BE PLACED ALONG THE EDGE OF THE TRAVELED WAY AT AN INTERVAL OF NOT MORE THAN 20 FEET. WHERE THE DROP-OFF IS GREATER THAN 24 INCHES, A TEMPORARY CONCRETE BARRIER SHALL BE PLACED. A DROP-OFF IS ELIMINATED IF TAPERED AWAY BY A 1 (VERTICAL) ON 4 (HORIZONTAL) OR FLATTER SLOPE FROM THE SURFACE OF THE TRAVELED WAY.

10. ALL DROP-OFFS FROM EDGES OF TRAVELED WAY SHALL BE ADDRESSED PER THE CONDITIONS AND DIRECTIONS SET FORTH IN SECTION 619-3.02 PARAGRAPH K AND TABLE 619-4 OF THE N.Y.S.D.O.T. STANDARD SPECIFICATIONS. NO DROP-OFF GREATER THAN 6 IN. SHALL REMAIN OVER A WEEKEND OR HOLIDAY. A DROP-OFF IS CONSIDERED ELIMINATED IF TAPERED AWAY BY A 1-ON-4 OR FLATTER SLOPE.

UTILITY NOTES:

1. THE CONTRACTOR SHALL COORDINATE ANY NECESSARY UTILITY RELOCATIONS AS REQUIRED FOR IMPLEMENTATION OF THE WORK ZONE TRAFFIC CONTROL PLAN WITH THE APPROPRIATE UTILITY COMPANIES OR AGENCIES. THE COST OF ANY NECESSARY RELOCATIONS AND COORDINATION WITH AGENCIES SHALL BE INCLUDED IN THE PRICE BID FOR 619.01.

2. THE CONTRACTOR SHALL COORDINATE THEIR WORK WITH THE UTILITY CONTRACTORS SO THAT TRAFFIC CONTROL OPERATIONS OCCURING WITHIN OR ABUTTING THE CONTRACT LIMITS DO NOT CONFLICT. IT IS ANTICIPATED THAT UTILITY WORK WILL BE ON-GOING THROUGHOUT THE DURATION OF THE CONTRACT.

Lu Engineers
ENVIRONMENTAL • TRANSPORTATION • CIVIL
PROJECT MANAGER: J. OTTMAN
DRAWN BY: J. MESSENGER

Department of Environmental Services
Architecture and Engineering Services
City of Rochester, New York
ASSISTANT MANAGING ENGINEER: LISA Y. REYES
CITY ENGINEER: HOLLY E. BARRETT, P.E.

PROJECT TITLE
SCOTTSVILLE ROAD - ELMWOOD AVENUE
IMPROVEMENT PROJECT
(WEST CITY LINE - MT HOPE AVE)

NO.	REVISION	BY	DATE

ISSUED	03/06/2019
CHECKED	JWO
DRAWN	JRM
DESIGN	JRM
SCALE	NONE
PROJECT NUMBER	17102

DRAWING TITLE
WORK ZONE TRAFFIC CONTROL NOTES
WZTC-2

DRAWING NO.
12
OF
54

FILE NAME = Work Zone Traffic Control Notes.dgn
DATE/TIME = 06-MAR-2019 10:57
USER = JMessenger

PROJECT SPECIFIC NOTES:

1. OPEN TRENCHES WILL BE ALLOWED OVERNIGHT AS FOLLOWS AT THE END OF THE WORK DAY; OPEN TRENCHES WITHIN THE ROADWAY PAVEMENT AREAS SHALL BE BROUGHT TO GRADE PER PAVEMENT RESTORATION DETAILS IF UTILITY WORK HAS BEEN COMPLETED IN THAT TRENCH, IF NOT, THE CONTRACTOR MAY USE STEEL ROAD PLATES AS APPROVED BY THE R.P.R. TO COVER OPEN TRENCHES OR BACKFILL OPEN TRENCHES WITH SUITABLE MATERIAL AS APPROVED BY THE R.P.R. WITHIN THE ROADWAY. OPEN TRENCHES IN LAWN OR SIDEWALK AREAS SHALL BE FULLY BACKFILLED OR COVERED WITH STEEL PLATES AND FENCED. ALL TEMPORARY METHODS OF BACKFILLING AND THE USE OF STEEL ROAD PLATES SHALL BE INCLUDED IN THE UNIT PRICE BID FOR ITEM 619.01, BASIC WORK ZONE TRAFFIC CONTROL.

2. WHEN CONSTRUCTION ACTIVITIES PROHIBIT DIRECT ACCESS TO A SCHOOL BUS STOP OR RTS BUS STOP LOCATION, THE CONTRACTOR SHALL IMMEDIATELY COORDINATE WITH THE ROCHESTER TRANSIT AUTHORITY OR RESPECTIVE SCHOOL DISTRICT TO EITHER:

   (A) TEMPORARILY RELOCATE THE BUS STOP TO THE NEAREST LOCATION WHERE SAFE, ADEQUATE PEDESTRIAN SPACE AND ACCESS CAN BE PROVIDED; OR

   (B) COORDINATE TO PROVIDE AND MAINTAIN A SAFE, TEMPORARY DROP-OFF LOCATION FOR BUS PATRONS.

   IN EITHER CASE, THE ALTERNATE FACILITY SHALL BE WELL-MARKED AND HAVE A STABLE, TRAVERSABLE SURFACE AT ALL TIMES AND MEET ALL REQUIREMENTS FOR ADA COMPLIANCE. THE COST FOR SUCH PROVISIONS SHALL BE INCLUDED IN THE PRICE BID FOR ITEM 619.01, BASIC WORK ZONE TRAFFIC CONTROL.

SEQUENCE OF OPERATIONS / STANDARD SHEETS NOTES:

1. ADJUST OR REPLACE CATCH BASINS, MANHOLES AND SURVEY MONUMENT COVERS UTILIZING DAILY SHOULDER CLOSURE, SINGLE LANE CLOSURE AND/OR FLAGGING OPERATIONS PER NYSDOT STANDARD SHEETS 619-20, 619-30, 619-60 AND 619-61.

2. REPLACE SIDEWALKS AND CURB RAMPS UTILIZING SIDEWALK DETAILS PER NYSDOT STANDARD SHEETS 619-50 AND 619-51.

3. PERFORM SINGLE COURSE MILLING AND OVERLAY, TWO COURSE MILLING AND OVERLAY, DEEP REPAIR AND FULL DEPTH REPAIR UTILIZING DAILY LANE CLOSURE AND FLAGGING OPERATION DETAILS PER NYSDOT STANDARD SHEETS 619-30, 619-60 AND 619-61.

4. INSTALL TEMPORARY AND PERMANENT PAVEMENT MARKINGS UTILIZING MOBILE OPERATION DETAILS.

5. INSTALL TRAFFIC SIGNAL LOOPS UTILIZING DAILY LANE CLOSURE AND FLAGGING OPERATION DETAILS PER NYSDOT STANDARD SHEETS 619-61.

6. INSTALL NEW HIGHWAY SIGNS UTILIZING A SHORT DURATION SHOULDER AND/OR LANE CLOSURE OPERATION DETAILS PER NYSDOT STANDARD SHEETS 619-20, 619-30 AND 619-60.

Lu Engineers
ENVIRONMENTAL • TRANSPORTATION • CIVIL

PROJECT MANAGER: J. OTTMAN
DRAWN BY: J. MESSENGER

Department of Environmental Services
Architecture and Engineering Services
City of Rochester, New York

ASSISTANT MANAGING ENGINEER: LISA Y. REYES
CITY ENGINEER: HOLLY E. BARRETT, P.E.

PROJECT TITLE
SCOTTSVILLE ROAD - ELMWOOD AVENUE IMPROVEMENT PROJECT
(WEST CITY LINE - MT HOPE AVE)

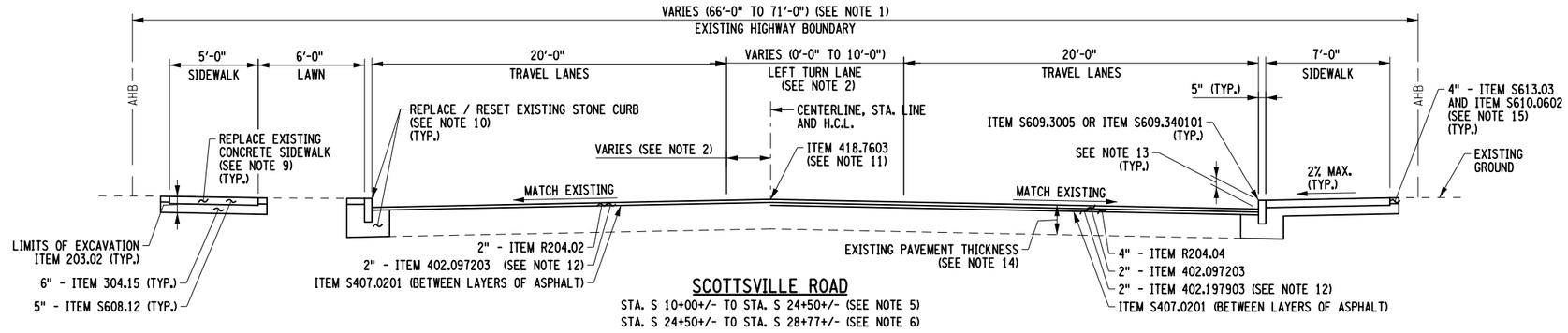
NO.	REVISION	BY	DATE

ISSUED 03/06/2019
CHECKED JWO
DRAWN JRM
DESIGN JRM
SCALE NONE
PROJECT NUMBER 17102

DRAWING TITLE
WORK ZONE TRAFFIC CONTROL NOTES
WZTC-3

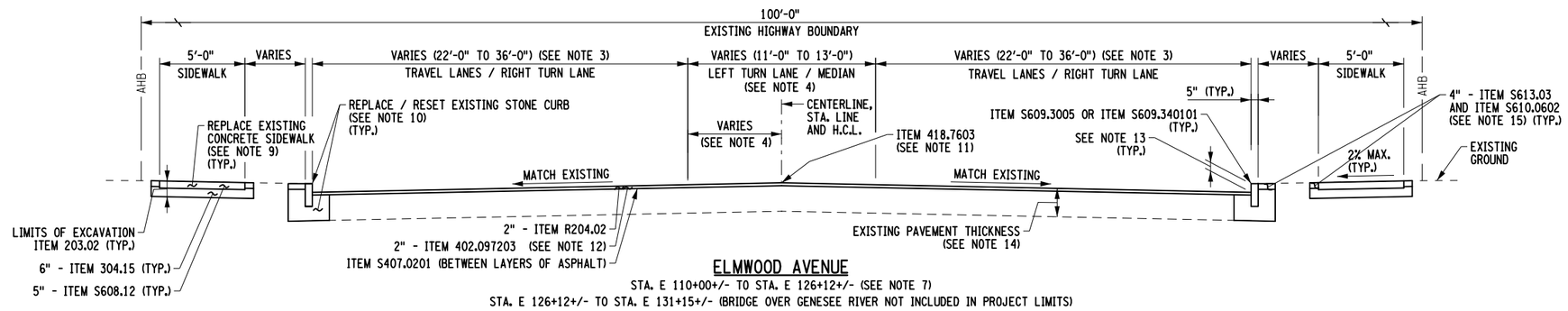
DRAWING NO. 13 OF 54

FILE NAME = Work Zone Traffic Control Notes.dgn
DATE/TIME = 06-MAR-2019 10:58
USER = JMessenger



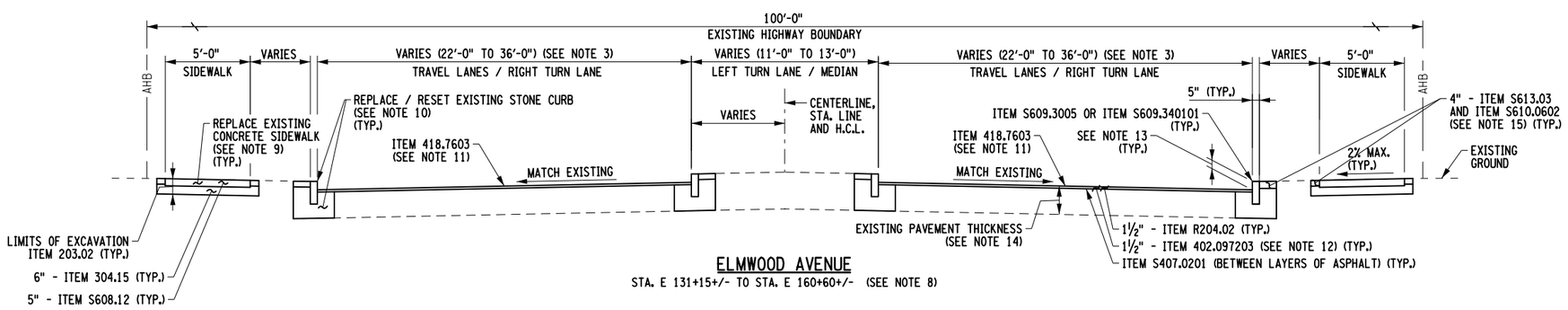
SCOTTSVILLE ROAD

STA. S 10+00+/- TO STA. S 24+50+/- (SEE NOTE 5)
STA. S 24+50+/- TO STA. S 28+77+/- (SEE NOTE 6)



ELMWOOD AVENUE

STA. E 110+00+/- TO STA. E 126+12+/- (SEE NOTE 7)
STA. E 126+12+/- TO STA. E 131+15+/- (BRIDGE OVER GENESEE RIVER NOT INCLUDED IN PROJECT LIMITS)



ELMWOOD AVENUE

STA. E 131+15+/- TO STA. E 160+60+/- (SEE NOTE 8)

ITEM	DESCRIPTION	UNIT
203.02	UNCLASSIFIED EXCAVATION	CY
R204.02	MILLING - 2" THICKNESS	SY
R204.04	MILLING - 4" THICKNESS	SY
304.15	SUBBASE COURSE, OPTIONAL TYPE	CY
402.097203	9.5 F2 TOP COURSE HMA, 70 SERIES COMPACTION	TON
402.197903	19 F9 BINDER COURSE HMA, 70 SERIES COMPACTION	TON
S407.0201	DILUTED TACK COAT	SY
418.7603	ASPAHALT PAVEMENT JOINT ADHESIVE	FT
S608.12	CONCRETE SIDEWALK AND DRIVEWAY	CY
S609.3005	5" STONE CURB (INCLUDING EXCAVATION, BACKFILL AND PAVEMENT BASE RESTORATION)	FT
S609.340101	RESET EXISTING STONE CURB (INCLUDING EXCAVATION, BACKFILL AND PAVEMENT BASE RESTORATION)	FT
S610.0602	SEEDING	SF
S613.03	TOPSOIL	CY

NOTES

1. REFER TO ROADWAY PLAN DWGS. PL-1 THRU PL-10 FOR THE WIDTH AND LOCATION OF EXISTING HIGHWAY BOUNDARY.
2. REFER TO PAVEMENT MARKING PLAN DWGS. PMP-1 THRU PMP-10 FOR LOCATIONS OF LEFT TURN LANES.
3. REFER TO PAVEMENT MARKING PLAN DWGS. PMP-1 THRU PMP-10 FOR THE WIDTH OF TRAVEL LANES AND THE WIDTH AND LOCATION OF RIGHT TURN LANES.
4. REFER TO PAVEMENT MARKING PLAN DWGS. PMP-1 THRU PMP-10 FOR THE WIDTH AND LOCATION OF LEFT TURN LANES AND CENTER CURBED OR FLUSH MEDIANS.
5. 4" MILL AND FILL LIMITS ARE FROM STA. S 10+00+/- TO STA. S 24+50+/- FROM CURB TO CURB.
6. 2" MILL AND FILL LIMITS ARE FROM STA. S 24+50+/- TO STA. S 28+77+/- FROM CURB TO CURB.
7. 2" MILL AND FILL LIMITS ARE FROM STA. E 110+00+/- TO STA. E 126+12+/- FROM CURB TO CURB.
8. 1½" MILL AND FILL LIMITS ARE FROM STA. E 131+15+/- TO STA. E 160+60+/- FROM CURB TO CURB.
9. REFER TO ROADWAY PLAN DWGS. PL-1 THRU PL-10 AND SIDEWALK REPAIR TABLE ON DWG. MT-6 FOR SIDEWALK REPLACEMENT LOCATIONS.
10. REFER TO CURB REPAIR TABLE ON DWG. MT-6 FOR CURB REPLACEMENT LOCATIONS AND TO CURB REPAIR DETAILS ON DWG. MD-2.
11. ALL PAVEMENT JOINTS/SEAMS INCLUDING BUT NOT LIMITED TO; ASPHALT CONSTRUCTION JOINTS, ASPHALT PAVING JOINTS AND PAVING WORK LIMITS SHALL BE SEALED WITH ITEM 418.7603, ASPHALT PAVEMENT JOINT ADHESIVE.
12. AFTER MILLING THE EXISTING PAVEMENT SURFACE, THE CITY AND/OR THE R.P.R. SHALL DETERMINE/DESIGNATE AREAS OF PAVEMENT REPAIR. REFER TO ROADWAY PLAN DWGS. PL-1 THRU PL-10 AND PAVEMENT REPAIR TABLE ON DWGS. MT-4 AND MT-5 FOR ANTICIPATED PAVEMENT REPLACEMENT LOCATIONS AND TO PAVEMENT REPAIR DETAILS ON DWG. MD-4.
13. EXISTING CURB TO REMAIN UNDISTURBED UNLESS OTHERWISE NOTED IN THE CONTRACT DOCUMENTS. MAINTAIN EXISTING CURB REVEAL.
14. THE CONTRACTOR SHALL REFER TO THE RECORD PLANS AND GEOTECHNICAL EXPLORATION REPORT FOR THE PAVEMENT CORE DATA WITHIN THE SUPPLEMENTAL INFORMATION
15. LIMITS OF WORK FOR ITEM S613.03 & S610.0602 SHALL BE DETERMINED BY THE R.P.R. IN THE FIELD.

FILE NAME = Typical Sections.dgn
DATE/TIME = 12-MAR-2019 11:20
USER = JMessenger

Lu Engineers
ENVIRONMENTAL • TRANSPORTATION • CIVIL
PROJECT MANAGER: J. OTTMAN
DRAWN BY: J. MESSENGER

Department of Environmental Services
Architecture and Engineering Services
City of Rochester, New York
ASSISTANT MANAGING ENGINEER: LISA Y. REYES
CITY ENGINEER: HOLLY E. BARRETT, P.E.

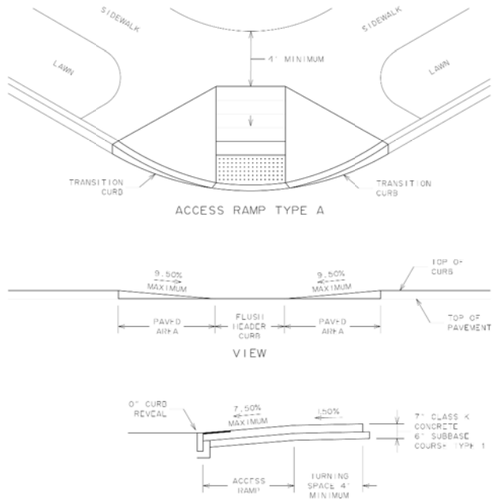
PROJECT TITLE
SCOTTSVILLE ROAD - ELMWOOD AVENUE
IMPROVEMENT PROJECT
(WEST CITY LINE - MT HOPE AVE)

NO.	REVISION	BY	DATE

ISSUED 03/06/2019
CHECKED JMO
DRAWN JRM
DESIGN JRM
SCALE NONE
PROJECT NUMBER 17102

DRAWING TITLE
TYPICAL SECTIONS
TS-1

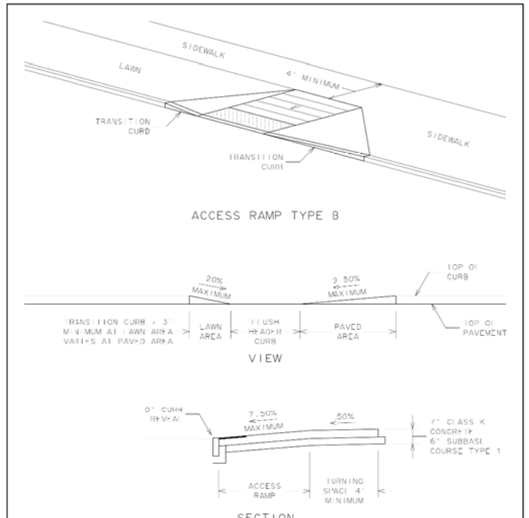
DRAWING NO.
14
OF
54



NOTES:

1. MINIMUM WIDTH OF ACCESS RAMP IS 4 FEET.
2. SURFACE OF ACCESS RAMP TO BE FIRM, STABLE AND SLIP RESISTANT. TEXTURE SURFACE WITH COARSE BROOM RUNNING TRANSVERSE TO SLOPE OF ACCESS RAMP.
3. ENTIRE AREA OF ACCESS RAMP AND SIDE FLARES TO BE 7 INCH THICK CONCRETE.

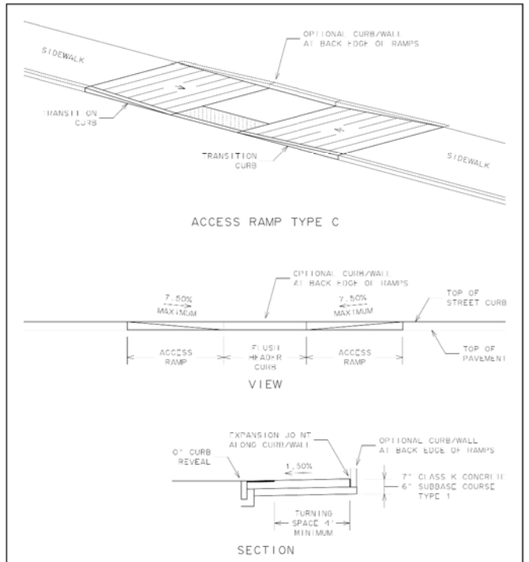
CITY OF ROCHESTER		
SIDEWALK ACCESS RAMP TYPE A		
ISSUED	9 2 91	STANDARD
REVISED	7 1 17	DWG.NO.R608-1



NOTES:

1. MINIMUM WIDTH OF ACCESS RAMP IS 4 FEET.
2. SURFACE OF ACCESS RAMP TO BE FIRM, STABLE AND SLIP RESISTANT. TEXTURE SURFACE WITH COARSE BROOM RUNNING TRANSVERSE TO SLOPE OF ACCESS RAMP.
3. ENTIRE AREA OF ACCESS RAMP AND SIDE FLARES TO BE 7 INCH THICK CONCRETE.

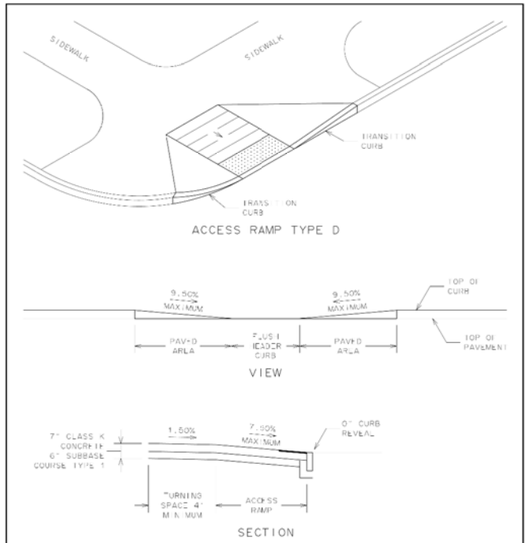
CITY OF ROCHESTER		
SIDEWALK ACCESS RAMP TYPE B		
ISSUED	9 2 91	STANDARD
REVISED	7 1 17	DWG.NO.R608-2



NOTES:

1. MINIMUM WIDTH OF ACCESS RAMPS AND TURNING SPACE IS 4 FEET.
2. SURFACE OF ACCESS RAMP TO BE FIRM, STABLE AND SLIP RESISTANT. TEXTURE SURFACE WITH COARSE BROOM RUNNING TRANSVERSE TO SLOPE OF ACCESS RAMP.
3. ENTIRE AREA OF ACCESS RAMPS AND TURNING SPACE TO BE 7 INCH THICK CONCRETE.

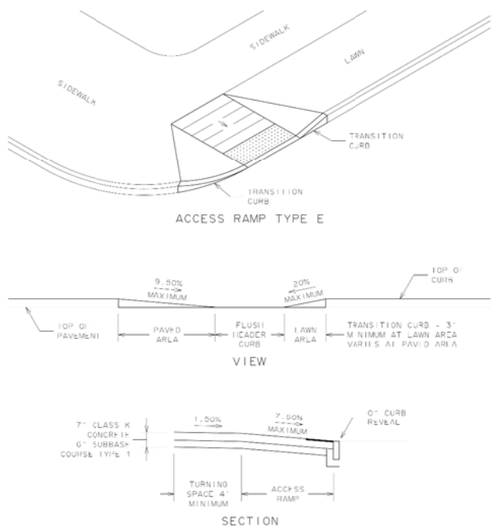
CITY OF ROCHESTER		
SIDEWALK ACCESS RAMP TYPE C		
ISSUED	9 2 91	STANDARD
REVISED	7 1 17	DWG.NO.R608-3



NOTES:

1. MINIMUM WIDTH OF ACCESS RAMP IS 4 FEET.
2. SURFACE OF ACCESS RAMP TO BE FIRM, STABLE AND SLIP RESISTANT. TEXTURE SURFACE WITH COARSE BROOM RUNNING TRANSVERSE TO SLOPE OF ACCESS RAMP.
3. ENTIRE AREA OF ACCESS RAMP AND SIDE FLARES TO BE 7 INCH THICK CONCRETE.

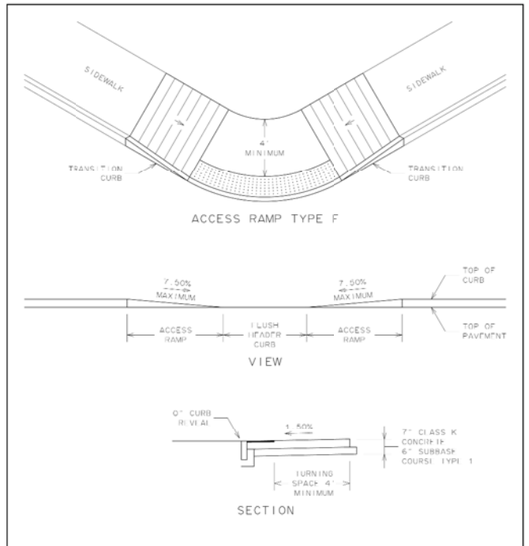
CITY OF ROCHESTER		
SIDEWALK ACCESS RAMP TYPE D		
ISSUED	9 2 91	STANDARD
REVISED	7 1 17	DWG.NO.R608-4



NOTES:

1. MINIMUM WIDTH OF ACCESS RAMP IS 4 FEET.
2. SURFACE OF ACCESS RAMP TO BE FIRM, STABLE AND SLIP RESISTANT. TEXTURE SURFACE WITH COARSE BROOM RUNNING TRANSVERSE TO SLOPE OF ACCESS RAMP.
3. ENTIRE AREA OF ACCESS RAMP AND SIDE FLARES TO BE 7 INCH THICK CONCRETE.

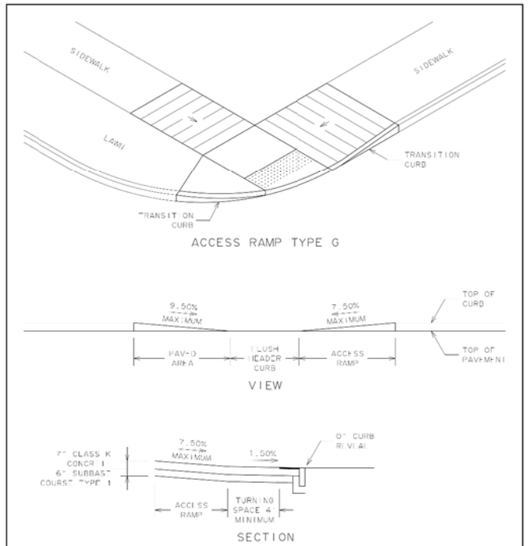
CITY OF ROCHESTER		
SIDEWALK ACCESS RAMP TYPE E		
ISSUED	3 21 94	NON-STANDARD
REVISED	7 1 17	DWG.NO.S608-13



NOTES:

1. MINIMUM WIDTH OF ACCESS RAMPS AND TURNING SPACE IS 4 FEET.
2. SURFACE OF ACCESS RAMP TO BE FIRM, STABLE AND SLIP RESISTANT. TEXTURE SURFACE WITH COARSE BROOM RUNNING TRANSVERSE TO SLOPE OF ACCESS RAMP.
3. ENTIRE AREA OF ACCESS RAMPS AND TURNING SPACE TO BE 7 INCH THICK CONCRETE.

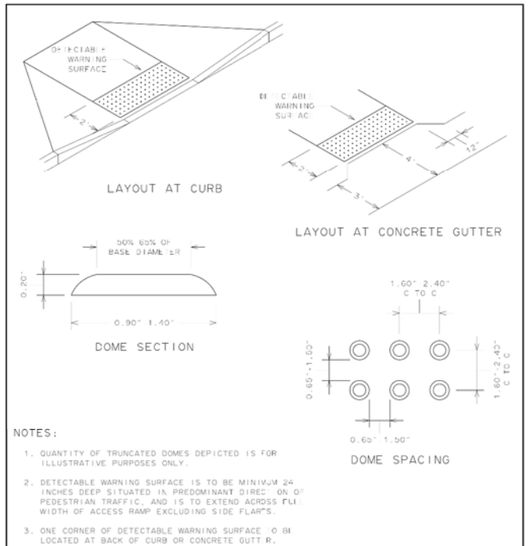
CITY OF ROCHESTER		
SIDEWALK ACCESS RAMP TYPE F		
ISSUED	3 21 94	NON-STANDARD
REVISED	7 1 17	DWG.NO.S608-14



NOTES:

1. MINIMUM WIDTH OF ACCESS RAMPS AND TURNING SPACE IS 4 FEET.
2. SURFACE OF ACCESS RAMP TO BE FIRM, STABLE AND SLIP RESISTANT. TEXTURE SURFACE WITH COARSE BROOM RUNNING TRANSVERSE TO SLOPE OF ACCESS RAMP.
3. ENTIRE AREA OF ACCESS RAMPS, TURNING SPACE AND SIDE FLARE TO BE 7 INCH THICK CONCRETE.

CITY OF ROCHESTER		
SIDEWALK ACCESS RAMP TYPE G		
ISSUED	3 21 94	NON-STANDARD
REVISED	7 1 17	DWG.NO.S608-15



NOTES:

1. QUANTITY OF TRUNCATED DOMES DEPICTED IS FOR ILLUSTRATIVE PURPOSES ONLY.
2. DETECTABLE WARNING SURFACE IS TO BE MINIMUM 24 INCHES DEEP SITUATED IN PREDOMINANT DIRECTION OF PEDESTRIAN TRAFFIC, AND IS TO EXTEND ACROSS FULL WIDTH OF ACCESS RAMP EXCLUDING SIDE FLARES.
3. ONE CORNER OF DETECTABLE WARNING SURFACE TO BE LOCATED AT BACK OF CURB OR CONCRETE GUTTER.
4. ON GRADE (RUNNING SLOPE) OF 5% OR GREATER, ROWS OF TRUNCATED DOMES ARE TO BE ALIGNED PERPENDICULAR OR RADIAL TO LOWER GRADE BREAK. WHERE GRADE (RUNNING SLOPE) IS THAN 5%, ORIENTATION IS LESS CRITICAL AND TRUNCATED DOMES MAY DIFFER FROM PERPENDICULAR OR RADIAL ALIGNMENT TO LOWER GRADE BREAK.
5. DETECTABLE WARNING SURFACE IS TO PROVIDE DARK ON LIGHT OR LIGHT-ON-DARK CONTRAST WITH SURROUNDING SURFACE. DEFAULT COLOR FOR ALL AREAS IS #33338 TRAFFIC YELLOW, OR APPROVED EQUIVALENT. COLOR IS TO BE IN ACCORDANCE WITH FEDERAL STANDARD 595C.

CITY OF ROCHESTER		
DETECTABLE WARNING SURFACE TRUNCATED DOMES		
ISSUED	1 23 03	NON-STANDARD
REVISED	7 1 17	DWG.NO.S608-28

Lu Engineers
ENVIRONMENTAL • TRANSPORTATION • CIVIL

PROJECT MANAGER: J. OTTMAN
DRAWN BY: J. MESSENGER

Department of Environmental Services
Architecture and Engineering Services
City of Rochester, New York

ASSISTANT MANAGING ENGINEER: LISA Y. REYES
CITY ENGINEER: HOLLY E. BARRETT, P.E.

PROJECT TITLE
SCOTTSVILLE ROAD - ELMWOOD AVENUE
IMPROVEMENT PROJECT
(WEST CITY LINE - MT HOPE AVE)

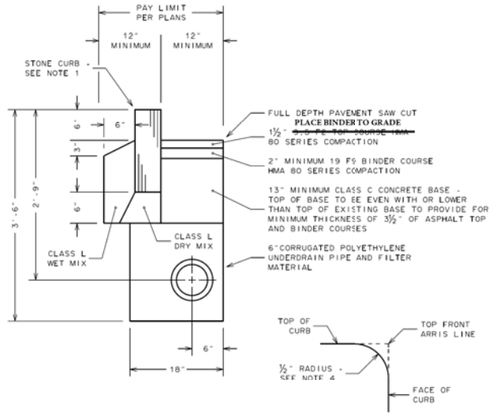
NO.	REVISION	BY	DATE

ISSUED	03/06/2019
CHECKED	JWO
DRAWN	JRM
DESIGN	JRM
SCALE	AS SHOWN
PROJECT NUMBER	17102

DRAWING TITLE
MISCELLANEOUS DETAILS
MD-1

DRAWING NO.
15
OF
54

FILE NAME = Miscellaneous Details.dgn
DATE/TIME = 06-MAR-2019 10:58
USER = JMessenger



CURB FOR DRIVEWAY

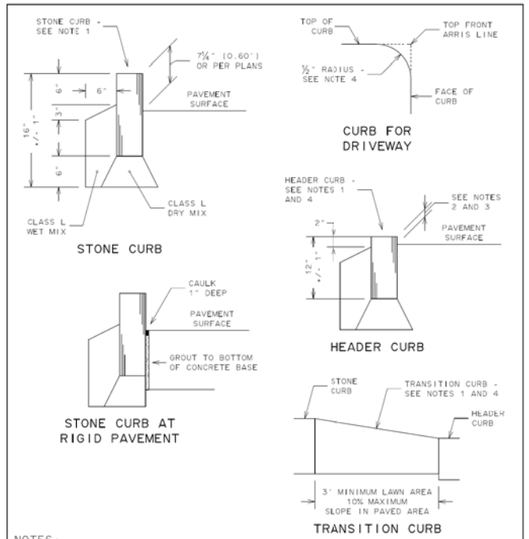
NOTES:

1. WIDTH OF CURB IS TO BE AS REQUIRED IN CONTRACT DOCUMENTS.
2. SIDEWALK ACCESS RAMP HEADER CURB IS TO BE FLUSH WITH PAVEMENT SURFACE.
3. DRIVEWAY HEADER CURB IS TO HAVE CURB REVEAL OF 1½ INCHES.
4. ONLY TRANSITION AND HEADER CURB PIECES USED FOR DRIVEWAYS ARE TO BE ROUNDED.
5. TACK COAT TO BE APPLIED BETWEEN ALL LIFTS OF HMA COURSES.
6. PAVEMENT SEAMS TO BE SEALED WITH BITUMINOUS SEALER IN ACCORDANCE WITH NYSDOT MATERIAL DESIGNATION 702-0700.

CITY OF ROCHESTER

STONE CURB REPLACEMENT - CONCRETE BASE

ISSUED	12-30-11	NON-STANDARD
REVISED	12-1-15	DWG.NO.S609-17



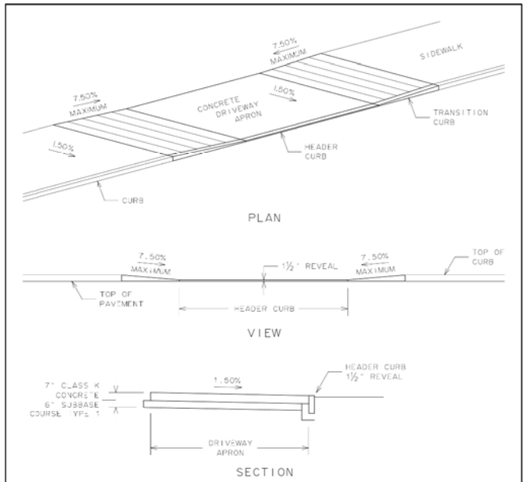
NOTES:

1. WIDTH OF CURB IS TO BE AS REQUIRED IN CONTRACT DOCUMENTS.
2. SIDEWALK ACCESS RAMP HEADER CURB IS TO BE FLUSH WITH PAVEMENT SURFACE.
3. DRIVEWAY HEADER CURB IS TO HAVE CURB REVEAL OF 1½ INCHES.
4. ONLY TRANSITION AND HEADER CURB PIECES USED FOR DRIVEWAYS ARE TO BE ROUNDED.
5. TACK COAT UNEXPOSED SURFACE OF CURB.

CITY OF ROCHESTER

STONE CURB

ISSUED	9-2-91	STANDARD
REVISED	3-1-18	DWG.NO.R609-1



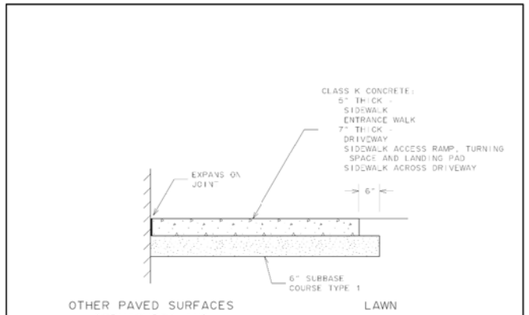
NOTES:

1. NOMINAL HEADER CURB LENGTH IS BETWEEN 10 AND 15 FEET FOR RESIDENTIAL DRIVEWAY, 24 FEET MAXIMUM FOR TWO-WAY COMMERCIAL DRIVEWAY, 16 FEET MAXIMUM FOR ONE-WAY COMMERCIAL DRIVEWAY. ACTUAL WIDTH WILL BE AS APPROVED BY CITY.
2. SURFACE OF DRIVEWAY APRON AND SIDE FLARES TO BE FIRM, STABLE AND SLIP-RESISTANT. TEXTURE SURFACE WITH COARSE BROOM RUNNING TRANSVERSE TO SLOPE OF DRIVEWAY APRON.
3. FULL DEPTH ½ INCH WIDE EXPANSION JOINT ACROSS SIDEWALK AT BOTH ENDS AND AROUND OUTER LIMITS OF DRIVEWAY APRON WHERE ABUTS OTHER PAVED SURFACES.
4. CROSS SLOPE OF DRIVEWAY APRON IS 1.50%.

CITY OF ROCHESTER

DEPRESSED SIDEWALK AND DRIVEWAY APRON ATTACHED SIDEWALK

ISSUED	2-10-00	NON-STANDARD
REVISED	7-1-17	DWG.NO.S608-22



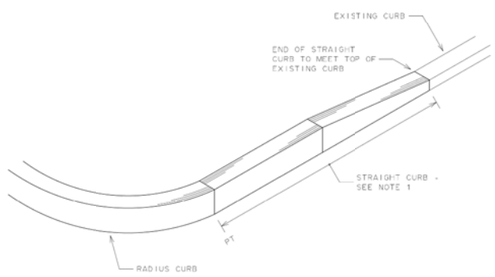
NOTES:

1. MINIMUM CLEAR WIDTH FOR SIDEWALK IS 5 FEET AS MEASURED FROM BACK EDGE OF CURB OR CONCRETE GUTTER.
2. CROSS SLOPE OF SIDEWALK TO BE 1.50%.
3. SURFACE TO BE FIRM, STABLE AND SLIP-RESISTANT.
4. SIDEWALK ACCESS RAMP AND LANDING PAD TO BE 7 INCH THICK CLASS K CONCRETE.
5. PROVIDE ½ INCH WIDE EXPANSION JOINT BETWEEN NEW CONCRETE CONSTRUCTION AND OTHER PAVED SURFACES, STRUCTURES AND APPURTENANCES.
6. EXPANSION JOINT, ½ INCH WIDE PREMOLDED EXPANSION JOINT MATERIAL, SEAL WITH CAULKING SEALANT ½ INCH WIDE ⅝ INCH DEEP.
7. SIDEWALK SURFACE, GRADE AND CROSS SLOPE ARE TO BE MAINTAINED ACROSS DRIVEWAYS.

CITY OF ROCHESTER

CONCRETE SIDEWALK AND DRIVEWAY

ISSUED	9-2-91	STANDARD
REVISED	7-1-17	DWG.NO.R608-6



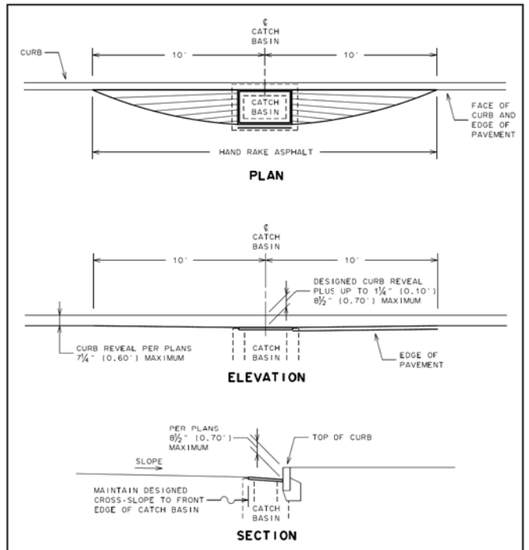
NOTES:

1. LENGTH OF STRAIGHT CURB WILL BE AS REQUIRED IN CONTRACT DOCUMENTS, WITH 6 FEET MINIMUM.
2. TRANSITION LAST 3 TO 5 FEET OF STRAIGHT CURB FROM DOWN TO TOP OF EXISTING CURB.

CITY OF ROCHESTER

NEW CURB TRANSITION TO EXISTING CURB

ISSUED	9-2-91	STANDARD
REVISED	12-1-15	DWG.NO.R609-7



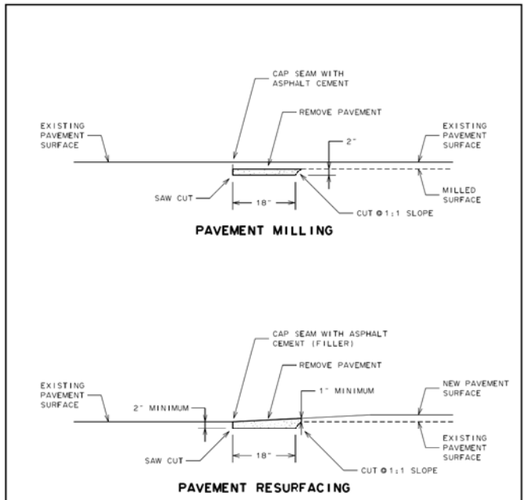
NOTES:

1. DEPRESSED PAVING IS TO BE USED ONLY WHERE GRADES ARE LESS THAN 0.40%, AND AT LOW POINTS WHERE BOTH INCOMING GRADES ARE LESS THAN 0.50%.
2. BACKEDGE OF CATCH BASIN FRAME AND GRATE TO BE DEPRESSED AT CURB.
3. PAVEMENT IS TO BE FLUSH WITH CATCH BASIN.
4. TOP OF CURB IS TO REMAIN TRUE TO LINE AND GRADE. CURB IS NOT TO BE DEPRESSED.

CITY OF ROCHESTER

DEPRESSED PAVING AT CATCH BASIN

ISSUED	9-2-91	STANDARD
REVISED	7-31-12	DWG.NO.R400-3



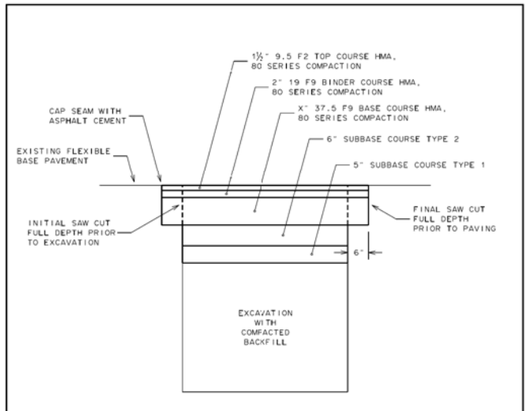
NOTES:

1. PAVEMENT KEYS ARE TO BE CONSTRUCTED SAME DAY AS NEW ASPHALT TOP COURSE IS PLACED, AND ARE NOT TO BE LEFT OPEN OVERNIGHT.
2. SEAMS BETWEEN EXISTING AND NEW PAVEMENT ARE TO BE SEALED WITH ASPHALT CEMENT 702-0700 IN ACCORDANCE WITH NYSDOT SECTION 702 BITUMINOUS MATERIALS.

CITY OF ROCHESTER

PAVEMENT KEY

ISSUED	1-13-06	NON-STANDARD
REVISED	7-31-12	DWG.NO.S406-1



NOTES:

1. FINAL SAW CUT IS TO BE FULL DEPTH TO OBTAIN STRAIGHT AND NEAT EDGE FOR PAVING. SAW CUT IS TO BE MADE AFTER TRENCH IS BACKFILLED.
2. BOTTOM OF ASPHALT BASE COURSE IS TO BE AT OR BELOW BOTTOM OF EXISTING ASPHALT PAVEMENT SECTION TO PROVIDE FOR REQUIRED MINIMUM THICKNESSES OF ASPHALT COURSES. MINIMUM THICKNESS OF ASPHALT BASE COURSE IS:
   HEAVY-DUTY PAVEMENT - 8 INCHES
   MEDIUM-DUTY PAVEMENT - 3 INCHES
   LIGHT-DUTY PAVEMENT - 0 INCHES
3. TACK COAT IS TO BE APPLIED BETWEEN ALL LIFTS OF HMA PAVEMENT COURSES.
4. SEAMS BETWEEN EXISTING AND NEW PAVEMENT ARE TO BE SEALED WITH ASPHALT CEMENT 702-0700 IN ACCORDANCE WITH NYSDOT SECTION 702 BITUMINOUS MATERIALS.

CITY OF ROCHESTER

TRENCH RESTORATION ASPHALT BASE

ISSUED	9-2-91	STANDARD
REVISED	7-31-12	DWG.NO.R206-1

Lu Engineers
ENVIRONMENTAL • TRANSPORTATION • CIVIL

PROJECT MANAGER: J. OTTMAN
DRAWN BY: J. MESSENGER

Department of Environmental Services
Architecture and Engineering Services
City of Rochester, New York

ASSISTANT MANAGING ENGINEER: LISA Y. REYES
CITY ENGINEER: HOLLY E. BARRETT, P.E.

PROJECT TITLE
SCOTTSVILLE ROAD - ELMWOOD AVENUE IMPROVEMENT PROJECT
(WEST CITY LINE - MT HOPE AVE)

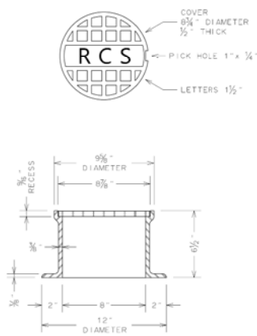
NO.	REVISION	BY	DATE

ISSUED	03/06/2019
CHECKED	JWO
DRAWN	JRM
DESIGN	JRM
SCALE	AS SHOWN
PROJECT NUMBER	17102

DRAWING TITLE
MISCELLANEOUS DETAILS MD-2

DRAWING NO. 16 OF 54

FILE NAME = Miscellaneous Details.dgn
DATE/TIME = 06-MAR-2019 10:59
USER = JMessenger



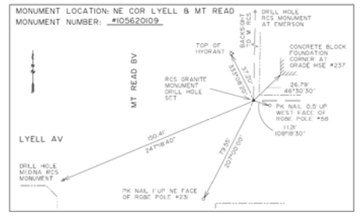
NOTES:

1. FRAME AND COVER TO BE SET FLUSH WITH FINISHED GRADE AND CENTERED OVER SURVEY MONUMENT.
2. FRAME AND COVER IS TO HAVE FULL AND EVEN BEARING ON UNDERLYING SURFACE AND IS TO BE NON-ROCKING WHEN IN PLACE.
3. INTERIOR OF FRAME TO BE VOID OF EXCESS MATERIALS.
4. HEAVY-DUTY FRAME AND COVER TO BE IN ACCORDANCE WITH EJ #4178A AND #4178Z AS MANUFACTURED BY EJ GROUP OR APPROVED EQUIVALENT.
5. FOR SURVEY MONUMENT SEE DETAIL R626-1.

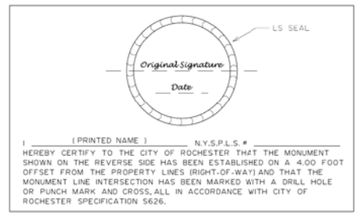
CITY OF ROCHESTER

SURVEY MONUMENT FRAME AND COVER

ISSUED	1-13-06	NON-STANDARD
REVISED	1-1-17	DWG.NO.S626-7



FRONT OF CARD



BACK OF CARD

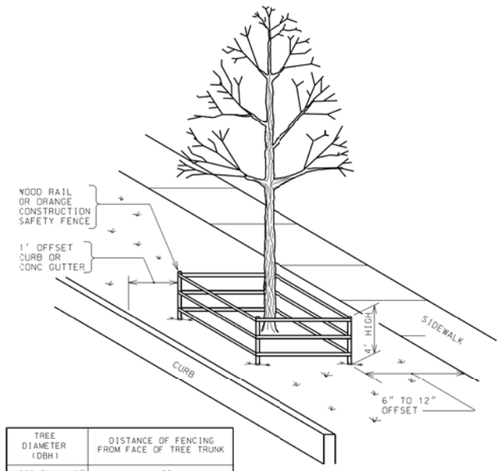
NOTES:

1. CERTIFICATION CARD TO BE 3 INCH x 5 INCH STANDARD CARD STOCK.
2. PROVIDE MINIMUM OF FIVE SWING ANGLE AND DISTANCE TIES TO SUBSTANTIAL, WELL DEFINED AND DESCRIBED PERMANENT POINTS.
3. CERTIFICATION CARD TO BE SUBMITTED COMPLETE WITH FIELD NOTES.

CITY OF ROCHESTER

HORIZONTAL CONTROL SURVEY MONUMENT CERTIFICATION CARD

ISSUED	9-2-91	STANDARD
REVISED	1-1-17	DWG.NO.R626-2



TREE DIAMETER (DBH)	DISTANCE OF FENCING FROM FACE OF TREE TRUNK
LESS THAN 10"	6'
10" - 14"	10'
15" - 19"	12'
20" OR MORE	15'

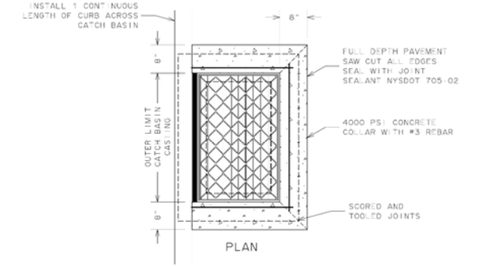
NOTE:

1. ALL TREES WITHIN THE PROJECT LIMITS THAT ARE TO REMAIN, ARE TO RECEIVE THIS TREATMENT.
2. DO NOT LEAVE CONSTRUCTION EQUIPMENT RUNNING (IDLING) UNDER TREE CANOPY.

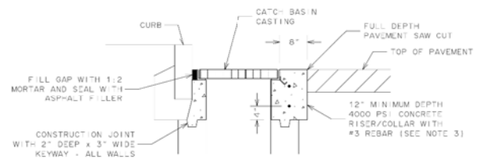
CITY OF ROCHESTER

TREE PROTECTION

ISSUED	11-20-92	NON-STANDARD
REVISED	7-1-93	DWG.NO.S617-1



PLAN



SECTION

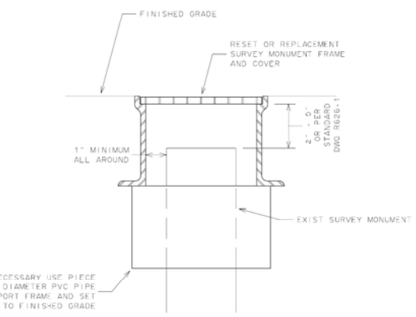
NOTES:

1. FRAME, GRATE AND CONCRETE COLLAR TO BE INSTALLED IN SAME HORIZONTAL PLANE AS SURROUNDING SURFACE, TRUE TO LINE AND GRADE, WITH GRATE HAVING FULL, CONTINUOUS AND UNIFORM BEARING CONTACT WITH CORRESPONDING FRAME.
2. TOP OF GRATE IS TO BE LEVEL WITH TOP OF FRAME, STABLE AND IMMOVABLE WHEN IN PLACE AND WHEN UNDER INFLUENCE OF TRAFFIC OR ANY OTHER TYPE OF LOAD BEARING STRESS.
3. DEPTH OF CONCRETE COLLAR TO BE MINIMUM 12 INCHES. ACTUAL DEPTH TO BE EITHER TO CONSTRUCTION JOINT OR SOUND CONCRETE/BRICK.

CITY OF ROCHESTER

TYPE A/B CATCH BASIN FRAME AND GRATE INSTALLATION AT CURB

ISSUED	2-1-19	NON-STANDARD
REVISED	2-22-19	DWG.NO.S604-92



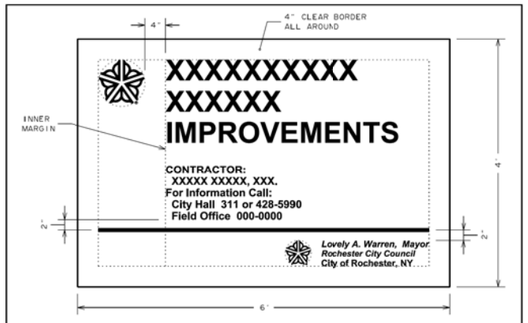
NOTES:

1. FRAME AND COVER TO BE SET FLUSH WITH FINISHED GRADE AND CENTERED OVER SURVEY MONUMENT.
2. FRAME AND COVER IS TO HAVE FULL AND EVEN BEARING ON UNDERLYING SURFACE AND IS TO BE NON-ROCKING WHEN IN PLACE.
3. MINIMUM OF 1½ INCHES AND MAXIMUM 6 INCHES BETWEEN FINISHED GRADE AND TOP OF SURVEY MONUMENT.
4. INTERIOR OF FRAME TO BE VOID OF EXCESS MATERIALS.

CITY OF ROCHESTER

RESET/REPLACE SURVEY MONUMENT FRAME AND COVER

ISSUED	1-13-06	NON-STANDARD
REVISED	1-1-17	DWG.NO.S626-6



LAYOUT:

- CLEAR BORDER 4" ALL AROUND
- INNER MARGIN 4" BETWEEN UPPER CITY MARK AND TEXT
- UPPER CITY MARK TO BE 9" DIAMETER AND FLUSH WITH CLEAR BORDER
- "CONTRACTOR" AND "FOR INFORMATION CALL" TEXT TO BE FLUSH WITH INNER MARGIN
- LOWER CITY MARK TO BE 5" DIAMETER AND BOTTOM TO BE FLUSH WITH CLEAR BORDER
- RIGHT SIDE MAYOR'S SIGNATURE TO BE FLUSH WITH CLEAR BORDER AND TOP TO LINE UP WITH TOP OF LOWER CITY MARK

LETTERS:

- PROJECT TITLE: SWISS 721 CONDENSED BOLD 4" LETTERS INITIAL CAP 2" LINE SPACING
- CONTRACTOR AND FOR INFORMATION CALL: SWISS 721 CONDENSED BOLD 1.50" LETTERS INITIAL CAP 0.75" LINE SPACING
- MAYOR'S SIGNATURE: SWISS 721 CONDENSED BOLD ITALIC 1.25" LETTERS INITIAL CAP 0.625" LINE SPACING
- ROCHESTER CITY COUNCIL: SWISS 721 CONDENSED BOLD ITALIC 1.1875" LETTERS INITIAL CAP 0.625" LINE SPACING
- CITY OF ROCHESTER: SWISS 721 CONDENSED BOLD 1.25" LETTERS INITIAL CAP

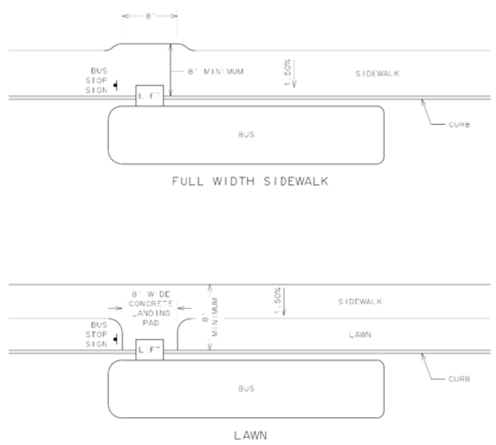
NOTES:

1. PRIOR TO SIGN FABRICATION, CONTRACTOR MUST OBTAIN WRITTEN APPROVAL FROM CITY OF SIGN LAYOUT.
2. BOARD TO BE 6 FOOT x 4 FOOT x ¾ INCH PLYWOOD PAINTED WITH 2 COATS WHITE EXTERIOR ENAMEL PAINT.
3. BOTTOM OF SIGN BOARD IS TO BE POSTED MINIMUM OF 5 FEET ABOVE GROUND.
4. CITY MARK AND ALL TEXT TO BE COLORED TO MATCH PMS COLOR 287C BLUE.
5. LINE TO BE 30 POINT RULE.
6. SIGN CAN BE REDUCED PROPORTIONALLY TO 3 FOOT x 2 FOOT AS REQUIRED IN CONTRACT DOCUMENTS.

CITY OF ROCHESTER

PROJECT SIGN

ISSUED	5-7-01	NON-STANDARD
REVISED	1-1-14	DWG.NO.S999-2



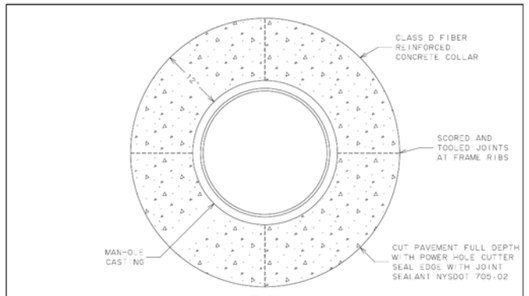
NOTES:

1. CROSS SLOPE OF LANDING PAD TO BE 1.50% MINIMUM TO 4.50% MAXIMUM.
2. SURFACE OF LANDING PAD TO BE FIRM, STABLE AND SLIP-RESISTANT. TEXTURE SURFACE WITH COARSE BROOM RUNNING TRANSVERSE TO SLOPE OF LANDING PAD.

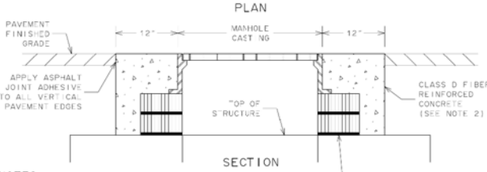
CITY OF ROCHESTER

BUS STOP LANDING PAD

ISSUED	12-2-96	NON-STANDARD
REVISED	7-1-17	DWG.NO.S608-19



PLAN



SECTION

NOTES:

1. CASTINGS ARE TO BE INSTALLED PLANAR, TRUE TO LINE AND GRADE OF SURROUNDING PAVEMENT. COVER TO HAVE FULL CONTINUOUS AND UNIFORM BEARING CONTACT WITH CORRESPONDING FRAME.
2. DEPTH OF CONCRETE COLLAR TO BE MINIMUM 8 INCHES. ACTUAL DEPTH TO BE EITHER TO EXISTING CONSTRUCTION JOINT, TOP SLAB OR SOUND CONCRETE/BRICK.
3. TOP OF COVER IS TO BE LEVEL WITH TOP OF CORRESPONDING FRAME. IF COVER IS BELOW/ABOVE TOP OF FRAME GREATER THAN ¼ INCH, IT IS DEFICIENT CONDITION THAT MUST BE MITIGATED WITHIN 30 CALENDAR DAYS OF HAVING BEEN IDENTIFIED.
4. COVER IS TO BE STABLE AND IMMOVABLE WHEN IN PLACE AND WHEN UNDER INFLUENCE OF TRAFFIC OR ANY OTHER TYPE OF LOAD BEARING STRESS.
5. ROUND COLLAR IS REQUIRED HOWEVER SQUARE COLLAR MAY BE ALLOWED AT DISCRETION OF CITY ENGINEER.

CITY OF ROCHESTER

SEWER MANHOLE INSTALLATION - ROUND COLLAR

ISSUED	11-14-18	NON-STANDARD
REVISED	3-7-19	DWG.NO.S604-9C

Lu Engineers
ENVIRONMENTAL • TRANSPORTATION • CIVIL

PROJECT MANAGER: J. OTTMAN
DRAWN BY: J. MESSENGER

Department of Environmental Services
Architecture and Engineering Services
City of Rochester, New York

ASSISTANT MANAGING ENGINEER: LISA Y. REYES
CITY ENGINEER: HOLLY E. BARRETT, P.E.

PROJECT TITLE
SCOTTSVILLE ROAD - ELMWOOD AVENUE IMPROVEMENT PROJECT
(WEST CITY LINE - MT HOPE AVE)

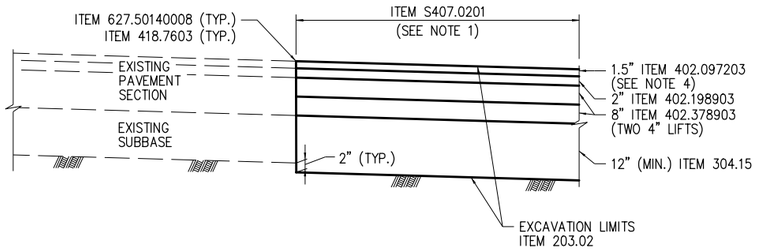
NO.	REVISION	BY	DATE

ISSUED 03/06/2019
CHECKED JWO
DRAWN JRM
DESIGN JRM
SCALE AS SHOWN
PROJECT NUMBER 17102

DRAWING TITLE
MISCELLANEOUS DETAILS MD-3

DRAWING NO. 17 OF 54

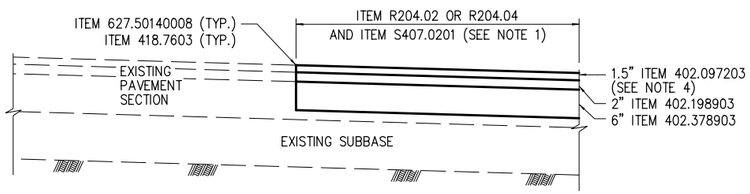
FILE NAME = Miscellaneous Details.dgn
DATE/TIME = 12-MAR-2019 11:21
USER = JMessenger



FULL DEPTH REPAIR SHALL BE USED AS ORDERED BY RESIDENT PROJECT REPRESENTATIVE FOR REPAIRS THAT ARE NECESSARY AFTER MILLING

FULL DEPTH REPAIR DETAIL

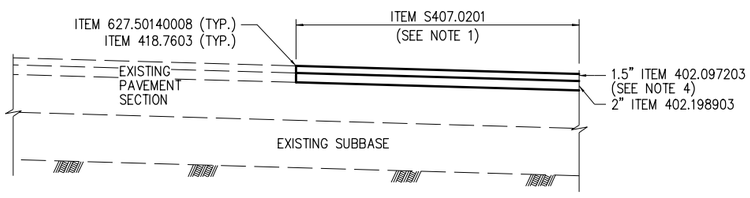
NOT TO SCALE



SEE PLAN DRAWINGS FOR APPROXIMATE LOCATIONS

DEEP REPAIR DETAIL

NOT TO SCALE



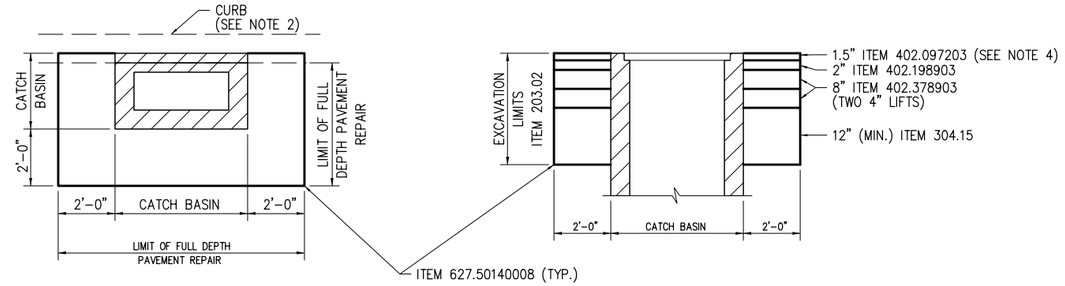
SEE PLAN DRAWINGS FOR APPROXIMATE LOCATIONS

TWO COURSE MILL AND OVERLAY DETAIL

NOT TO SCALE

REPAIR DETAIL NOTES:

1. DILUTED TACK COAT (ITEM S407.0201) SHALL BE USED ON MILLED SURFACES, EXISTING HMA PAVEMENT, VERTICAL SURFACES (CURB, DRAINAGE STRUCTURES, ETC.) AND BETWEEN ALL LIFTS OF NEW HMA PAVEMENT.
2. IF THE CONTRACTOR ELECTS TO "DROP THE MILL HEAD" TO BEGIN PAVEMENT REPAIR, NO PAYMENT WILL BE MADE FOR ITEM 627.50140008 (CUTTING PAVEMENT).
3. HMA PAVEMENT REPAIR LOCATIONS ARE TO BE APPROVED BY THE RESIDENT PROJECT REPRESENTATIVE AFTER MILLING OPERATIONS HAVE BEEN COMPLETED. THE R.P.R. SHALL VERIFY ALL PAVEMENT REPAIR AREAS AND THE TYPE OF REPAIR BEFORE THE CONTRACTOR BEGINS WORK.
4. ITEM 402.097203 SHALL BE 2" ON SCOTTSVILLE ROAD.



PLAN **ELEVATION**

NOTES:

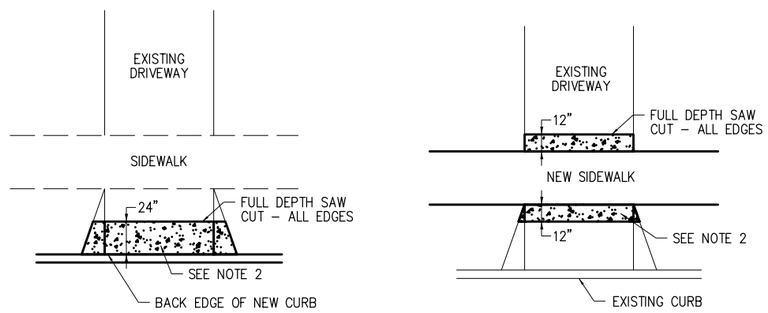
1. ASPHALT TOP (ITEM 402.097203) SHALL BE PLACED DURING ROADWAY OVERLAY OPERATION.
2. CATCH BASIN ADJACENT TO CURB SHOWN. REPAIR RETAIL ALSO APPLIES TO MANHOLE OR OTHER DRAINAGE STRUCTURES AWAY FROM CURBING.
3. TACK COAT (ITEM S407.0201) SHALL BE USED ON MILLED SURFACES, EXISTING HMA PAVEMENT, VERTICAL SURFACES (CURBS, DRAINAGE STRUCTURES, ETC.) AND BETWEEN ALL LIFTS OF NEW HMA PAVEMENT.
4. ITEM 402.097203 SHALL BE 2" ON SCOTTSVILLE ROAD.

FULL DEPTH PAVEMENT REPAIR AROUND CATCH BASIN

NOT TO SCALE

FULL DEPTH REPAIR SHALL BE USED AS ORDERED BY RESIDENT PROJECT REPRESENTATIVE FOR REPAIRS THAT ARE NECESSARY AFTER MILLING

ITEM NO.	DESCRIPTION	UNITS
203.02	UNCLASSIFIED EXCAVATION AND DISPOSAL	CY
R204.02	MILLING - 2" THICKNESS	SY
R204.04	MILLING - 4" THICKNESS	SY
304.15	SUBBASE COURSE, OPTIONAL TYPE	CY
402.097203	9.5 F2 TOP COURSE HMA, 70 SERIES COMPACTION	TON
402.198903	19 F9 BINDER COURSE HMA, 80 SERIES COMPACTION	TON
402.378903	37.5 F9 BASE COURSE HMA, 80 SERIES COMPACTION	TON
S407.0201	DILUTED TACK COAT	SY
418.7603	ASPHALT PAVEMENT JOINT ADHESIVE	LF
S608.12	CONCRETE SIDEWALK AND DRIVEWAY	CY
S608.15	ASPHALT DRIVEWAY - LIGHT DUTY	SF
627.50140008	CUTTING PAVEMENT	LF



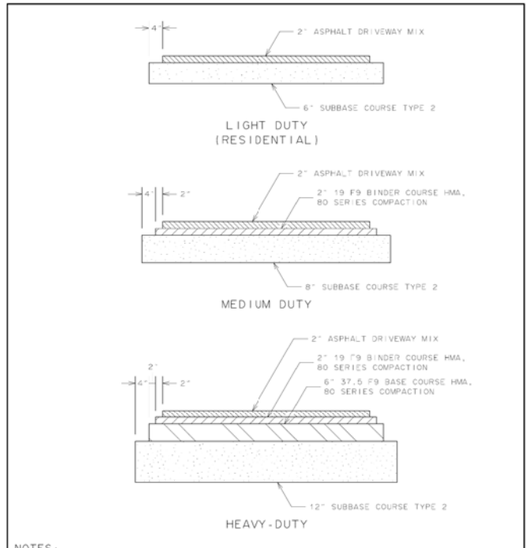
CURB REPLACEMENT **FULL SIDEWALK REPLACEMENT**

NOTES:

1. SAW CUTS ARE TO BE MADE USING POWER SAW WITH DIAMOND OR ABRASIVE BLADE AND ARE TO BE STRAIGHT. PAYMENT INCLUDED IN DRIVEWAY RESTORATION ITEM (ITEM S608.12 OR ITEM S608.15).
2. REMOVE EXISTING PAVEMENT AS REQUIRED AND REPLACE WITH NEW PAVEMENT MATERIALS TO MATCH EXISTING.
3. FINISHED SURFACE OF DRIVEWAY IS TO BE FLUSH WITH ADJACENT SURFACES.
4. JOINTS ARE TO BE SEALED WITH ASPHALT FILLER MEETING NYSDOT MATERIAL DESIGNATION 702–0700.

DRIVEWAY RESTORATION AT NEW SIDEWALK

NOT TO SCALE



NOTES:

1. ELIMINATE STEPPED EDGE TREATMENT WHEN ADJACENT TO OTHER PAVED SURFACES.
2. SURFACE TO BE FIRM AND STABLE.
3. PROVIDE ½ INCH WIDE EXPANSION JOINT BETWEEN NEW ASPHALT CONSTRUCTION AND OTHER PAVED SURFACES, STRUCTURES AND APPURTENANCES.
4. EXPANSION JOINT: ½ INCH WIDE PREMOLDED EXPANSION JOINT MATERIAL SEALED TOP WITH CAULKING SEALANT ½ WIDE ⅜ INCH DEEP.

CITY OF ROCHESTER

ASPHALT DRIVEWAY

ISSUED 9-2-91 STANDARD
REVISED 7-1-17 DWG.NO.R608-7

Lu Engineers
ENVIRONMENTAL • TRANSPORTATION • CIVIL

PROJECT MANAGER: J. OTTMAN
DRAWN BY: J. MESSENGER

Department of Environmental Services
Architecture and Engineering Services
City of Rochester, New York

ASSISTANT MANAGING ENGINEER: LISA Y. REYES
CITY ENGINEER: HOLLY E. BARRETT, P.E.

PROJECT TITLE
SCOTTSVILLE ROAD - ELMWOOD AVENUE IMPROVEMENT PROJECT
(WEST CITY LINE - MT HOPE AVE)

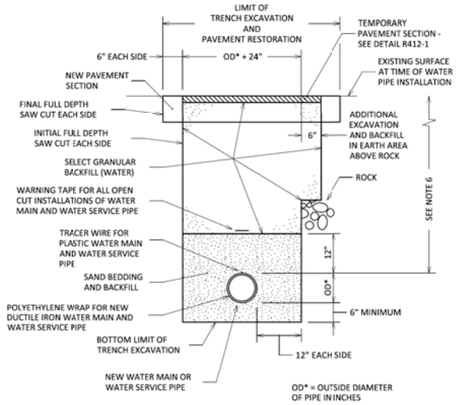
NO.	REVISION	BY	DATE

ISSUED 03/06/2019
CHECKED JWO
DRAWN JRM
DESIGN JRM
SCALE AS SHOWN
PROJECT NUMBER 17102

DRAWING TITLE
MISCELLANEOUS DETAILS MD-4

DRAWING NO. 18 OF 54

FILE NAME = Miscellaneous Details.dgn
DATE/TIME = 12-MAR-2019 11:24
USER = JMessenger

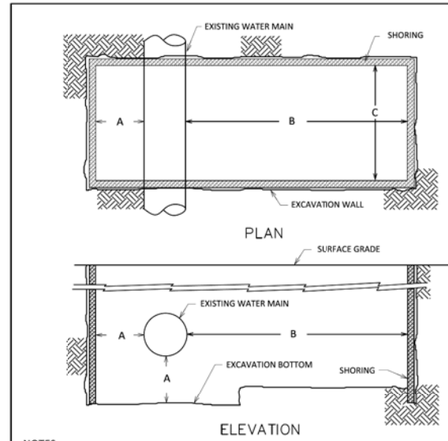


NOTES:

1. LIMITS SHOWN ARE MAXIMUM ALLOWED AND MINIMUM REQUIRED FOR EXCAVATION AND BACKFILL.
2. UPPER LIMIT FOR TRENCH EXCAVATION IS TOP OF EXISTING SURFACE AT TIME OF TRENCH EXCAVATION.
3. UPPER LIMIT FOR ROCK EXCAVATION IS TOP SURFACE OF ROCK.
4. UPPER LIMIT FOR SELECT GRANULAR BACKFILL (WATER) IS BOTTOM OF TEMPORARY PAVEMENT, IF USED, OTHERWISE TOP OF SURFACE.
5. TEMPORARY OR PERMANENT BLOCKS OR ANY OTHER TYPE OF PIPE SUPPORT IS NOT TO BE USED DURING PIPE INSTALLATION.
6. MINIMUM DEPTH OF COVER FOR WATER PIPE FROM TOP OF PROPOSED GRADE IS 4.50 FEET FOR DOMESTIC WATER PIPE, AND 5 FEET FOR HOLLY WATER PIPE, UNLESS OTHERWISE NOTED ON PLANS OR AS DIRECTED BY PROJECT MANAGER.

CITY OF ROCHESTER
WATER TRENCH PAVEMENT REHABILITATION

ISSUED	1-13-06	NON-STANDARD
REVISED	11-22-10	DWG.NO.S900-2



NOTES:

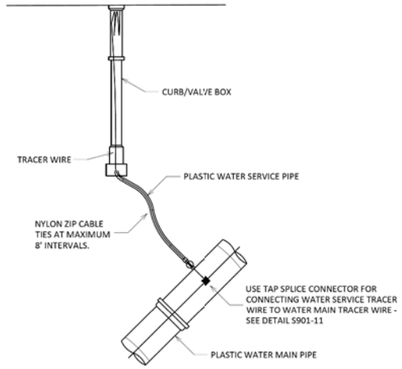
1. THE CONTRACTOR IS RESPONSIBLE FOR ENSURING THAT ALL EXCAVATIONS ARE ADEQUATELY PROTECTED IN ACCORDANCE WITH OCCUPATIONAL SAFETY AND HEALTH ADMINISTRATION (OSHA) REGULATIONS.
2. SHORING MUST BE FURNISHED BY THE CONTRACTOR IN ACCORDANCE WITH OSHA REGULATIONS, AND ADEQUATE CLEARANCES PROVIDED, WHEREVER WATER BUREAU PERSONNEL MUST ENTER AN EXCAVATION. EXCEPTIONS TO THIS POLICY CAN ONLY BE MADE BY THE DIRECTOR OF WATER OR HIS REPRESENTATIVE.
3. THE CONTRACTOR SHALL MAKE AND SHORE THE EXCAVATION, CLEAN THE AREA OF THE WATER MAIN FOR THE TAP, ATTACH THE TAPPING SLEEVE AND VALVE TO THE MAIN FOR SERVICES 4-INCH DIAMETER AND LARGER OR ATTACH THE TAPPING SADDLE (WHERE REQUIRED) TO THE MAIN FOR SERVICES 2-INCH DIAMETER AND SMALLER AND INSURE THAT THE WORK AREA IS READY FOR INSPECTION BY BUREAU PERSONNEL PRIOR TO MAKING THE TAP.

MINIMUM CLEARANCE DIMENSIONS

EXCAVATION FOR	A	B	C
SMALL SERVICE TAP (2-INCH AND SMALLER)	0'-6"	5'-0"	4'-0"
LARGE SERVICE TAP (4-INCH AND LARGER)	1'-0"	7'-0"	4'-0"

CITY OF ROCHESTER
WATER SERVICE TAP EXCAVATION

ISSUED	4-29-11	NON-STANDARD
REVISED		DWG.NO.S900-12

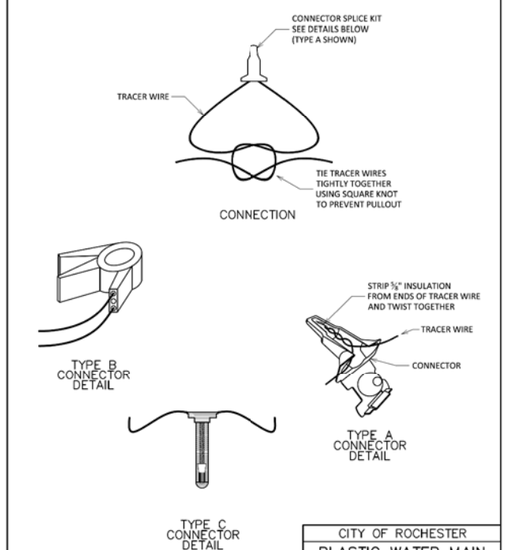


NOTES:

1. FOR OPEN CUT INSTALLATION, TRACER WIRE IS TO BE #12 AWG SOLID COPPER WITH 30 MIL BLUE HMWPE INSULATION. FOR TRENCHLESS INSTALLATION, TRACER WIRE IS TO BE #12 AWG SOLID COPPER CLAD STEEL CORE WITH 45 MIL BLUE HDPE INSULATION.
2. FOR NEW PLASTIC WATER SERVICE PIPE CONNECTED TO PLASTIC WATER MAIN PIPE, WATER SERVICE TRACER WIRE IS TO BE SPLICED TO WATER MAIN TRACER WIRE AND EXTENDED UP TO TOP OF CURB/VALVE BOX. WATER SERVICE TRACER WIRE IS TO RUN ALONG TOP OF AND BE SECURED TO PLASTIC WATER SERVICE PIPE WITH NYLON ZIP CABLE TIES PLACED AT MAXIMUM 8 FEET INTERVALS.
3. TRACER WIRE SHALL TRAVEL UP THE INSIDE THE WATER SERVICE CURB/VALVE BOX WITH ENOUGH EXTRA TRACER WIRE TO EXTEND A DISTANCE OF 4 FEET BEYOND THE TOP OF THE CURB/VALVE BOX. THE EXTRA TRACER WIRE SHALL BE COILED AND STORED ON THE UNDERSIDE OF THE CURB BOX COVER WITHIN THE CURB BOX.
4. FOR NEW PLASTIC WATER SERVICE PIPE CONNECTED TO CAST/DUCTILE IRON WATER MAIN PIPE, WATER SERVICE TRACER WIRE IS TO BE CONNECTED TO CORPORATION STOP WITH BRASS THAW WIRE NUT COMPRESSION ASSEMBLY WITH SET SCREW.

CITY OF ROCHESTER
PLASTIC WATER SERVICE TRACER WIRE INSTALLATION

ISSUED	1-13-06	NON-STANDARD
REVISED	4-19-18	DWG.NO.S901-9

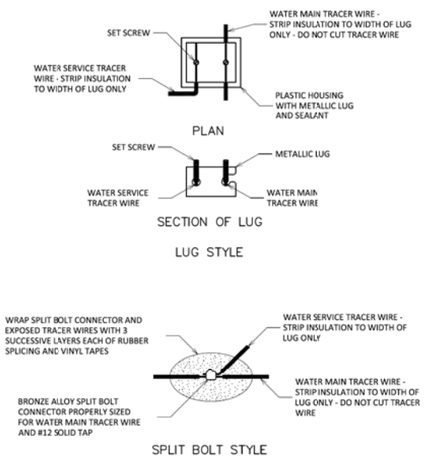


NOTE:

END TO END SPLICE CONNECTION IS TO BE USED ONLY FOR SPLICING TWO ENDS OF WATER MAIN TRACER WIRE TOGETHER WITH CONNECTION BEING MADE IN ACCORDANCE WITH MANUFACTURER'S INSTRUCTIONS.

CITY OF ROCHESTER
PLASTIC WATER MAIN TRACER WIRE END TO END SPLICE CONNECTION

ISSUED	6-1-09	NON-STANDARD
REVISED	2-19-15	DWG.NO.S901-10

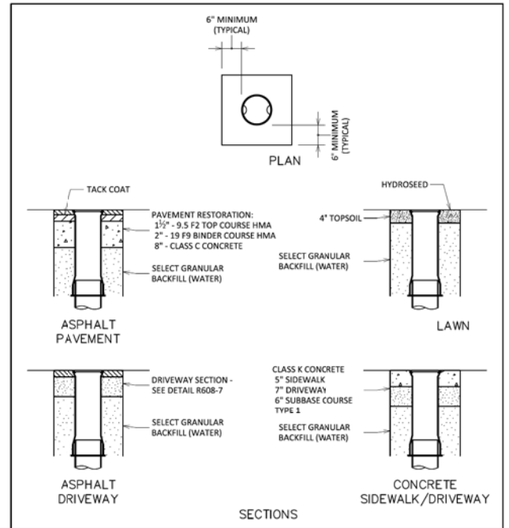


NOTE:

TAP SPLICE CONNECTION IS TO BE USED FOR CONNECTING WATER SERVICE TRACER WIRE TO WATER MAIN TRACER WIRE WITH CONNECTION BEING MADE IN ACCORDANCE WITH MANUFACTURER'S INSTRUCTIONS.

CITY OF ROCHESTER
PLASTIC WATER SERVICE TRACER WIRE TAP SPLICE CONNECTION

ISSUED	5-1-09	NON-STANDARD
REVISED		DWG.NO.S901-11

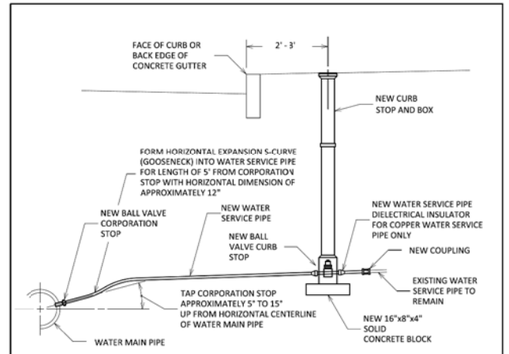


NOTES:

1. ADJUST EXISTING WATER VALVE BOX AND COVER, OR INSTALL NEW WATER VALVE BOX AND COVER. SET TOP TO GRADE.
2. IF ADJUSTING EXISTING WATER VALVE BOX AND COVER, EXISTING FLANGE IS TO REMAIN, DO NOT REMOVE.

CITY OF ROCHESTER
WATER VALVE BOX ADJUSTMENT OR REPLACEMENT

ISSUED	2-10-00	NON-STANDARD
REVISED	6-1-09	DWG.NO.S909-1



NOTES:

1. WATER SERVICE PIPE IS TO HAVE MINIMUM COVER DEPTH OF 4.50 FEET FROM FINISHED SURFACE.
2. WATER SERVICE TAP IS TO BE MADE ON CUSTOMER SIDE OF WATER MAIN PIPE. FOR PLASTIC WATER MAIN PIPE USE WIDE STRAP BRASS TAPPING SADDLE. FOR CAST/DUCTILE IRON WATER MAIN PIPE USE DOUBLE STRAP BRASS TAPPING SADDLE, SEE TABLE.
3. WATER SERVICE PIPE IS TO RUN PERPENDICULAR TO WATER MAIN PIPE.
4. FOR WATER SERVICE PIPE 1½ INCHES AND LARGER, ADD CURB BOX ARCH TO CURB BOX.
5. TRACER WIRE MUST BE INSTALLED FOR PLASTIC WATER SERVICE PIPE, SEE DETAILS S901-9 AND S901-11.
6. MAGNESIUM ANODE MUST BE INSTALLED FOR COPPER WATER SERVICE PIPE, SEE DETAIL S966-3.
7. WARNING TAPE MUST BE INSTALLED WITH ALL OPEN CUT TRENCHED WATER SERVICE PIPE INSTALLATIONS.
8. REFER TO WATER BUREAU'S APPROVED PRODUCT LIST FOR LIST OF APPROVED WATER SERVICE PRODUCTS.

SERVICE SADDLE REQUIREMENTS FOR TAPS ON CAST/DUCTILE IRON WATER MAIN PIPE

WATER MAIN PIPE SIZE (INCHES)	MAXIMUM TAP SIZE ALLOWED w/o SERVICE SADDLE (INCHES)
4" - 6"	ALL TAPS REQUIRE SADDLE
8" - 10"	¾"
12"	1"
16" AND LARGER	1½"

CITY OF ROCHESTER
WATER SERVICE PIPE 2 INCH AND SMALLER

ISSUED	9-2-91	STD. DWG.
REVISED	6-1-09	NO. R913-1

CITY OF ROCHESTER
WATER SERVICE INSTALLATION DATA

PROPERTY ADDRESS: _____

STREET SERVICE TAPPED ON (IF DIFFERENT): _____

DATE OF INSTALLATION: _____ DOMESTIC OR HOLLY SYSTEM? _____

TAP SIZE: _____ DISTANCE, MAIN TO ROW LINE: _____ SIDE OF ST. TAPPED: _____

CURB STOP/VALVE SIZE: _____ SERVICE LENGTH: _____ SERVICE MATERIAL: _____

CONTRACTOR/PLUMBER: _____

TAP INSTALLED BY: _____ INSPECTOR: _____

CURB OR OPERATING VALVE LOCATION *: _____

TAP OR GUARD VALVE LOCATION (IF DIFFERENT): _____

INSULATOR INSTALLED? _____ IF YES, WHERE: TAP _____ CURB STOP _____

ANODE INSTALLED? ____ IF YES, SIZE: ____ LBS., ATTACHED TO: TAP __, SERVICE __, MAIN __

INSIDE SERVICE MATERIAL & SIZE: _____

* MEASURED FROM ROW OF NEAREST INTERSECTING STREET OR HYDRANT, WHICHEVER IS CLOSER — OCT. 2013

NOTES:

1. WATER SERVICE CARD WILL BE SUPPLIED BY WATER BUREAU. REQUIRED INFORMATION IS TO BE TYPED OR NEATLY PRINTED ON WATER SERVICE CARD.
2. WATER SERVICE CARD FOR WATER SERVICE THAT IS ABANDONED AT TAP IS TO BE MARKED "ABANDONED AT TAP" ACROSS FACE OF WATER SERVICE CARD WITH CONTRACTOR'S NAME AND DATE ABANDONED BEING NOTED.
3. MEASUREMENT FOR LOCATION OF CURB STOP IS TO BE TAKEN FROM RIGHT-OF-WAY EXTENSION OF NEAREST INTERSECTING STREET AS MEASURED PARALLEL TO STREET ON WHICH WATER SERVICE IS INSTALLED, OR FROM NEAREST HYDRANT LOCATED OFF OF SAME WATER MAIN, WHICHEVER IS CLOSER (SEE NOTE 4 FOR EXCEPTION). WHENEVER POSSIBLE, MEASUREMENTS SHOULD BE LESS THAN 300 FEET.
4. IF NEAREST INTERSECTING STREET RIGHT-OF-WAY EXTENSION IS NOT PERPENDICULAR TO THE STREET RIGHT-OF-WAY ON WHICH THE CURB STOP IS LOCATED, THE CURB STOP MEASUREMENT SHOULD BE TAKEN FROM THE NEAREST HYDRANT ATTACHED TO THE WATER MAIN WHICH THE WATER SERVICE IS CONNECTED TO. IF SUCH A HYDRANT IS NOT AVAILABLE, THEN ANOTHER PERMANANT FEATURE, PRE-APPROVED BY THE WATER BUREAU, SHALL BE USED FOR THE CURB STOP MEASUREMENT.
5. MEASUREMENTS ARE TO BE ENTERED IN TENTHS OF FEET. DO NOT USE INCHES. ROUNDING TO THE NEAREST 0.5 FEET IS ACCEPTABLE.
6. IF SERVICE IS INSTALLED WITH HORIZONTAL BEND, DRAW A SKETCH ON THE BACK OF THE CARD SHOWING THE LOCATIONS OF THE MAIN, CORPORATION, SERVICE AND CURB STOP. THE DISTANCE BETWEEN THE BEND AND CURB STOP AND OFFSET DIMENSIONS BETWEEN THE BEND AND THE CORPORATION AND CURB STOP SHOULD BE SHOWN IN THE SKETCH.

CITY OF ROCHESTER
WATER SERVICE CARD

ISSUED	9-2-91	STD. DWG.
REVISED	10-23-13	NO. R913-3

Lu Engineers
ENVIRONMENTAL • TRANSPORTATION • CIVIL
PROJECT MANAGER: J. OTTMAN
DRAWN BY: J. MESSENGER

Department of Environmental Services
Architecture and Engineering Services
City of Rochester, New York
ASSISTANT MANAGING ENGINEER: LISA Y. REYES
CITY ENGINEER: HOLLY E. BARRETT, P.E.

PROJECT TITLE
SCOTTSVILLE ROAD - ELMWOOD AVENUE IMPROVEMENT PROJECT (WEST CITY LINE - MT HOPE AVE)

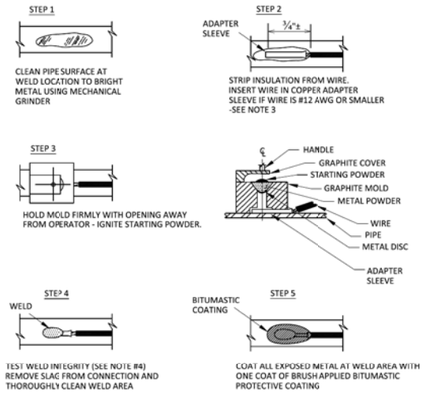
NO.	REVISION	BY	DATE

ISSUED	03/06/2019
CHECKED	JWO
DRAWN	JRM
DESIGN	JRM
SCALE	AS SHOWN
PROJECT NUMBER	17102

DRAWING TITLE
MISCELLANEOUS DETAILS MD-5

DRAWING NO. 19 OF 54

FILE NAME = Miscellaneous Details.dgn
DATE/TIME = 06-MAR-2019 11:06
USER = JMessenger



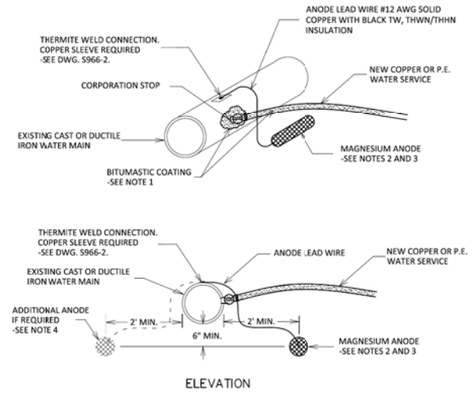
NOTES:

1. FOLLOW MANUFACTURER'S PROCEDURES AND RECOMMENDATIONS WHEN THERMITE WELDING.
2. USE APPROPRIATE WELD MOLDS AND WELD METALS FOR SPECIFIC SIZE AND MATERIAL OF PIPE THAT WIRE IS BEING ATTACHED TO.
3. WHEN THERMITE WELDING #12 AWG WIRES OR SMALLER, INSERT END OF WIRE INTO AN APPROVED COPPER SLEEVE PRIOR TO THERMITE WELDING AND CRIMP SLEEVE ON WIRE.
4. TEST WELD INTEGRITY BY STRIKING WELD WITH A HAMMER AFTER WELD HAS COOLED. AVOID STRIKING WIRE.

CITY OF ROCHESTER

THERMITE WELD DETAILS

ISSUED 10-17-08 NON-STANDARD
REVISED 12-28-10 DWG.NO.S966-2



NOTES:

1. APPLY ONE COAT OF BITUMASTIC TO MAIN AT SERVICE CONNECTION, CORPORATION STOP AND, FOR COPPER WATER SERVICES ONLY, SECTION OF SERVICE WITHIN 3 FEET OF MAIN.
2. USE HIGH POTENTIAL MAGNESIUM ANODE IN PREPACKAGED CLOTH BAG WITH BACKFILL. BARE WEIGHT OF ANODE INGOT (EXCLUDING BACKFILL) AND NUMBER OF ANODES AS NOTED ON PLANS OR AS DIRECTED BY PROJECT MANAGER.
3. ANODE IS TO BE PLACED IN TRENCH, WITH ANODE CENTERLINE 6 INCHES MINIMUM BELOW BOTTOM OF MAIN AND 2 FEET MINIMUM FROM SIDE WALL OF MAIN. ANODE TO BE SURROUNDED WITH NATIVE BACKFILL.
4. WHEN TWO ANODES ARE REQUIRED AT NEW WATER SERVICE, THE SECOND ANODE SHALL BE LOCATED ON OPPOSITE SIDE OF WATER MAIN. SECOND ANODE LEAD WIRE SHALL BE THERMITE WELDED TO TOP OF WATER MAIN AT LEAST 1 FOOT FROM THERMITE WELD FOR FIRST ANODE LEAD WIRE.

CITY OF ROCHESTER

ANODE AT NEW WATER SERVICE ON EXISTING CAST/DUCTILE IRON WATER MAIN

ISSUED 11-26-08 NON-STANDARD
REVISED 12-28-10 DWG.NO.S966-4

Lu Engineers
ENVIRONMENTAL • TRANSPORTATION • CIVIL

PROJECT MANAGER: J. OTTMAN
DRAWN BY: J. MESSENGER

Department of Environmental Services
Architecture and Engineering Services
City of Rochester, New York

ASSISTANT MANAGING ENGINEER: LISA Y. REYES
CITY ENGINEER: HOLLY E. BARRETT, P.E.

PROJECT TITLE
SCOTTSVILLE ROAD - ELMWOOD AVENUE IMPROVEMENT PROJECT (WEST CITY LINE - MT HOPE AVE)

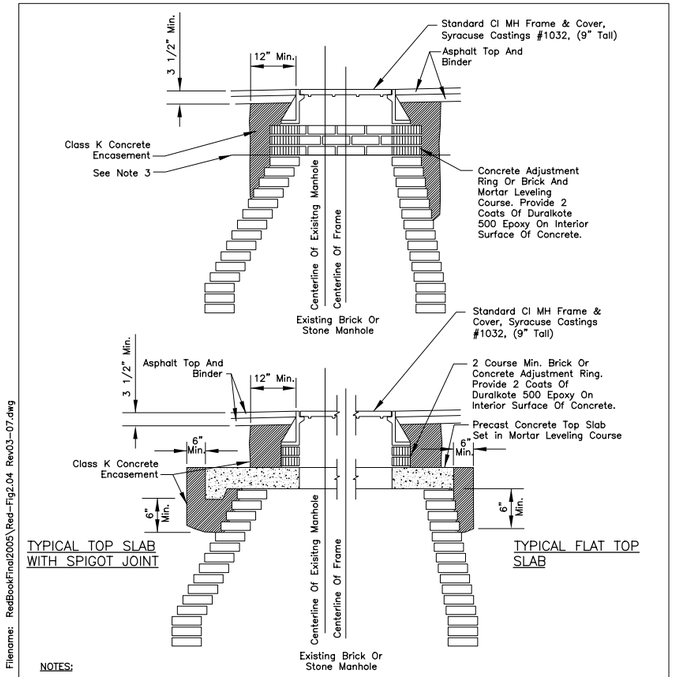
NO.	REVISION	BY	DATE

ISSUED 03/06/2019
CHECKED JWO
DRAWN JRM
DESIGN JRM
SCALE AS SHOWN
PROJECT NUMBER 17102

DRAWING TITLE
MISCELLANEOUS DETAILS MD-6

DRAWING NO. 20 OF 54

FILE NAME = Miscellaneous Details.dgn
DATE/TIME = 06-MAR-2019 11:07
USER = JMessenger



NOTES:

1. Provide appropriate measures to prevent surface water, mud, and foreign material from entering the sewer system during construction, by plugging, pumping, bailing, and other work as necessary.
2. Remove/Add courses of brick/block, or install concrete adjustment ring to retrofit manhole frame and cover onto existing manhole riser at the required finished grade.
3. The lower limit of the Class K concrete encasement shall be at, or below, the lowest portion of the leveling course.
4. Brick for frame leveling course, ASTM C32, Grade SS.

Typical Detail for City Of Rochester Street Improvement Projects. Other Use Requires RPWD Approval.

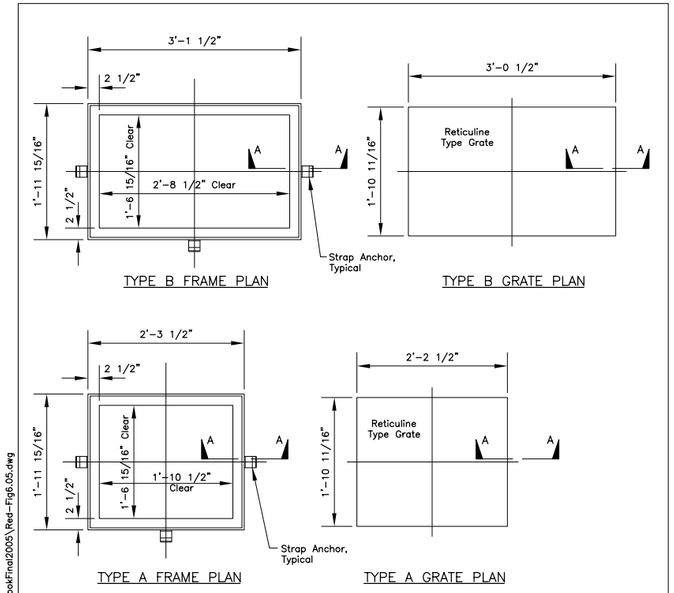
Former City of Rochester Std. Dwg. No. R604–10

MONROE COUNTY PURE WATERS

MARCH 2007	MANHOLE FRAME AND COVER ADJUSTMENT FOR BRICK AND STONE STRUCTURES	FIGURE 2.04

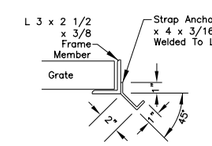
Filename: RedBookFinal2005\Red-Fig2.04 Rev03-07.dwg

Time Printed: Wed Mar 7 10:05 2007



NOTES:

1. Frames and grates shall be galvanized steel reticuline type, Syracuse Castings Model NYSDOT #1 for Type A, and Model NYSDOT #3 for Type B, or approved equal. Units shall be designed for H–20 loading. Complete unit shall be hot dip galvanized after fabrication, ASTM A123.



SECTION A–A
ANCHOR DETAIL
(For Reticuline Grate, 3 Required)

Typical Detail for City Of Rochester Street Improvement Projects. Other Use Requires RPWD Approval.

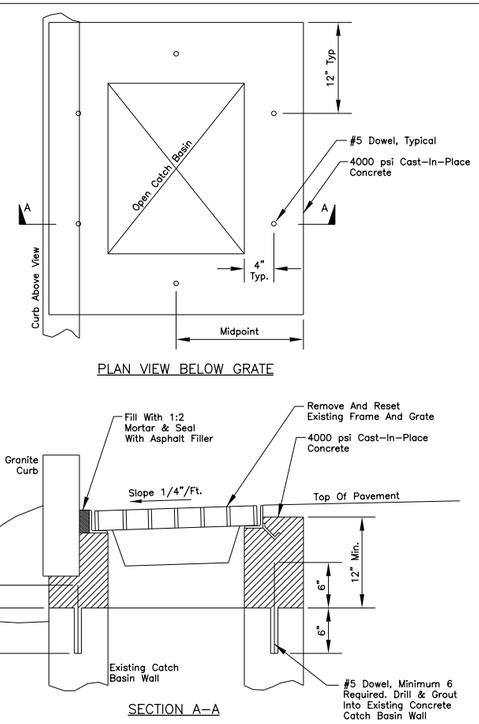
Former City of Rochester Std. Dwg. No. R655–1

MONROE COUNTY PURE WATERS

Feb 2005	TYPE A & B CATCH BASIN FRAME AND GRATE	FIGURE 6.05

Filename: RedBookFinal2005\Red-Fig6.05.dwg

Time Printed: Tues Feb 15 09:45 2005



NOTES:

1. Scarify and/or remove top of existing catch basin walls to sufficient depth to accommodate new concrete cap. Coat top surface with epoxy polysulfide grout per NYSDOT Sub–Section 721–03 prior to pouring new cap.
2. Coat interior of new concrete construction with 2 coats of Duralkote 500 Epoxy or approved equal.
3. Coat exposed exterior of new concrete construction with 2 coats of bituminous coating.

Typical Detail for City Of Rochester Street Improvement Projects. Other Use Requires RPWD Approval.

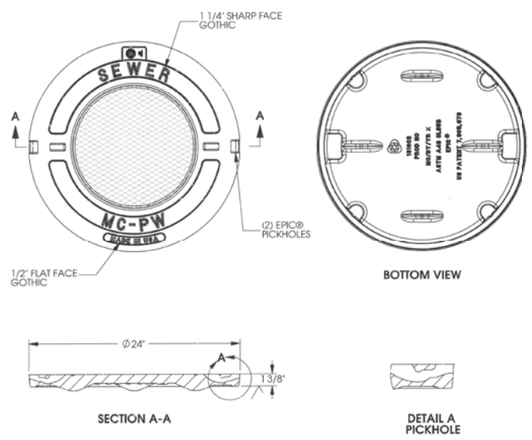
Former City of Rochester Std. Dwg. No. R604–7

MONROE COUNTY PURE WATERS

MARCH 2007	CATCH BASIN FRAME AND COVER ADJUSTMENT	FIGURE 6.10

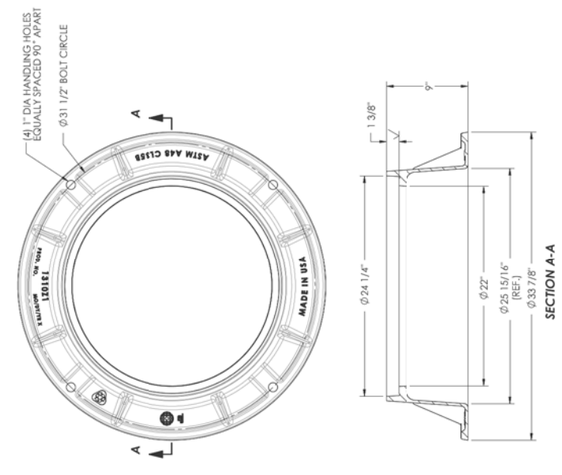
Filename: RedBookFinal2005\Red-Fig6.10 Rev03-07.dwg

Time Printed: Wed Mar 7 12:55 2007



MONROE COUNTY PURE WATERS
MANHOLE COVER DETAIL
NOT TO SCALE

NOTE:
1. EJ PRODUCT No. 00131094 OR APPROVED EQUAL.



MONROE COUNTY PURE WATERS
MANHOLE FRAME DETAIL
NOT TO SCALE

NOTE:
1. EJ PRODUCT No. 00131016 OR APPROVED EQUAL.

Lu Engineers
ENVIRONMENTAL • TRANSPORTATION • CIVIL

PROJECT MANAGER: J. OTTMAN
DRAWN BY: J. MESSENGER

Department of Environmental Services
Architecture and Engineering Services
City of Rochester, New York

ASSISTANT MANAGING ENGINEER: LISA Y. REYES
CITY ENGINEER: HOLLY E. BARRETT, P.E.

PROJECT TITLE
SCOTTSVILLE ROAD - ELMWOOD AVENUE IMPROVEMENT PROJECT
(WEST CITY LINE - MT HOPE AVE)

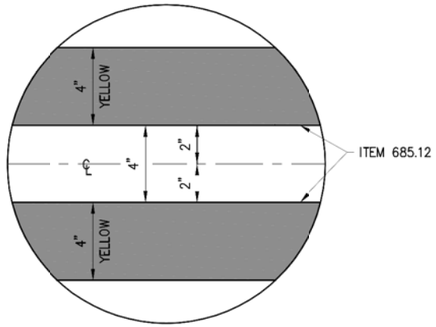
NO.	REVISION	BY	DATE

ISSUED	03/06/2019
CHECKED	JWO
DRAWN	JRM
DESIGN	JRM
SCALE	AS SHOWN
PROJECT NUMBER	17102

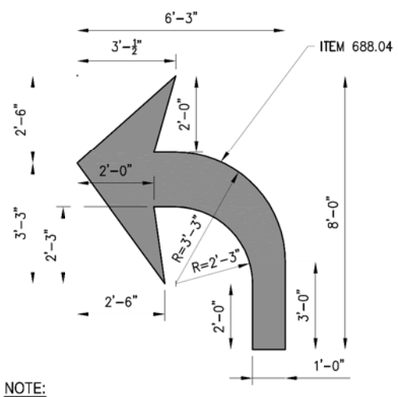
DRAWING TITLE
MISCELLANEOUS DETAILS MD-7

DRAWING NO. 21 OF 54

FILE NAME = Miscellanenous Details.dgn
DATE/TIME = 06-MAR-2019 11:07
USER = JMessenger



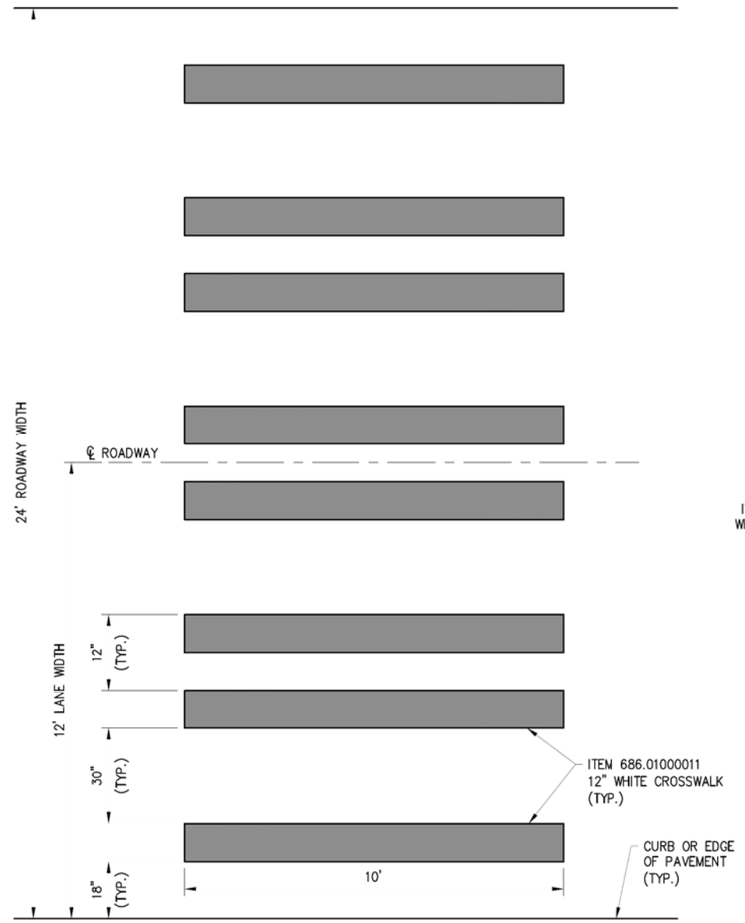
DOUBLE YELLOW CENTERLINE
N.T.S.



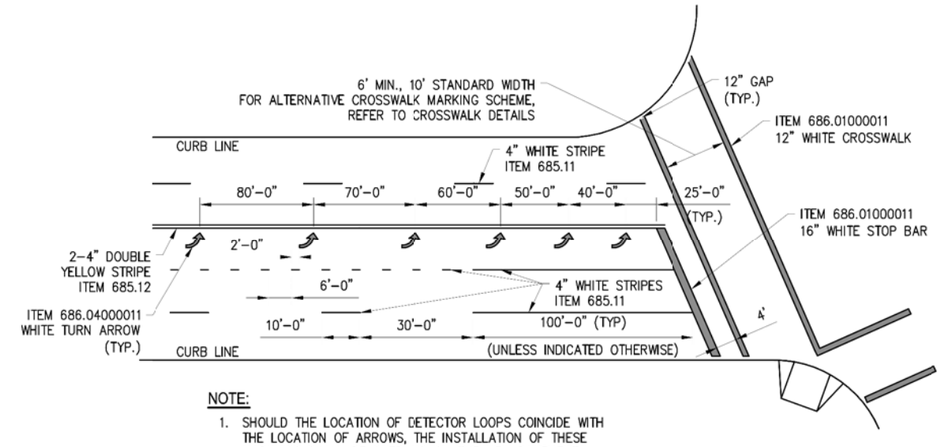
NOTE:

1. DIMENSIONS ARE THE SAME FOR RIGHT ARROW REVERSED.

LEFT OR RIGHT TURN ARROW
N.T.S.



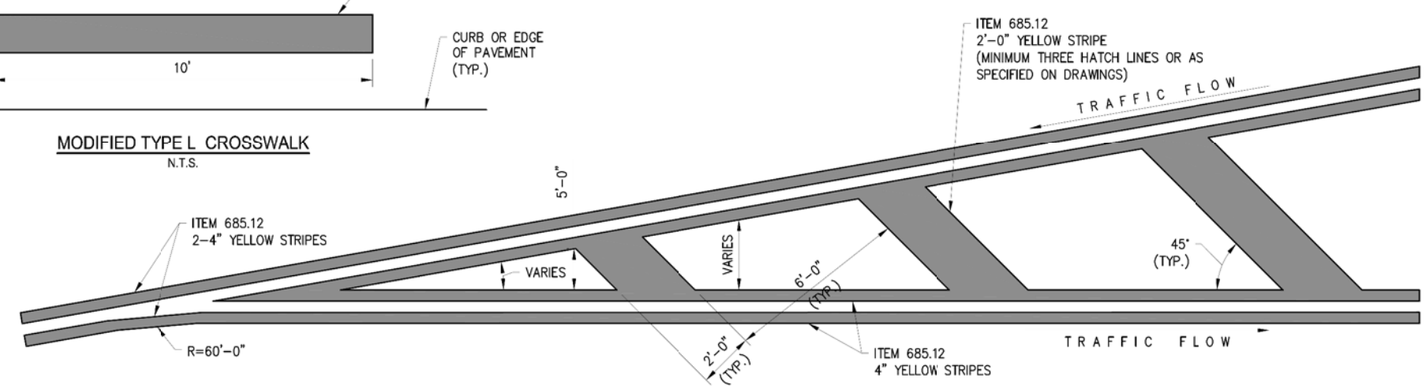
MODIFIED TYPE L CROSSWALK
N.T.S.



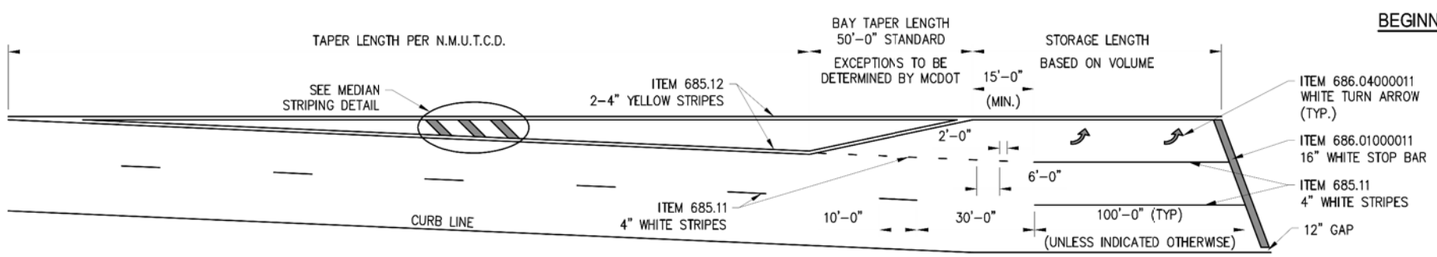
NOTE:

1. SHOULD THE LOCATION OF DETECTOR LOOPS COINCIDE WITH THE LOCATION OF ARROWS, THE INSTALLATION OF THESE MARKINGS SHALL BE OVERLAID ON THE NEW PAVEMENT AFTER THE DETECTOR LOOPS HAVE BEEN INSTALLED.

PAVEMENT MARKINGS AT INTERSECTION
N.T.S.



BEGINNING OF MEDIAN STRIPING DETAIL
N.T.S.



LEFT TURN LANE PAVEMENT MARKINGS
N.T.S.

FILE NAME = Miscellaneous Details.dgn
DATE/TIME = 06-MAR-2019 11:08
USER = JMessenger

Lu Engineers
ENVIRONMENTAL • TRANSPORTATION • CIVIL

PROJECT MANAGER: J. OTTMAN
DRAWN BY: J. MESSENGER

Department of Environmental Services
Architecture and Engineering Services
City of Rochester, New York

ASSISTANT MANAGING ENGINEER: LISA Y. REYES
CITY ENGINEER: HOLLY E. BARRETT, P.E.

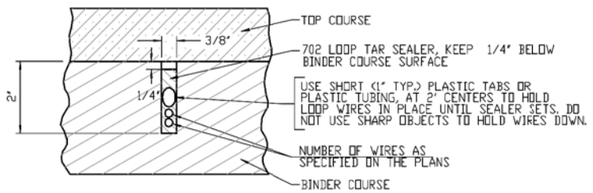
PROJECT TITLE
SCOTTSVILLE ROAD - ELMWOOD AVENUE
IMPROVEMENT PROJECT
(WEST CITY LINE - MT HOPE AVE)

NO.	REVISION	BY	DATE

ISSUED 03/06/2019
CHECKED JWO
DRAWN JRM
DESIGN JRM
SCALE AS SHOWN
PROJECT NUMBER 17102

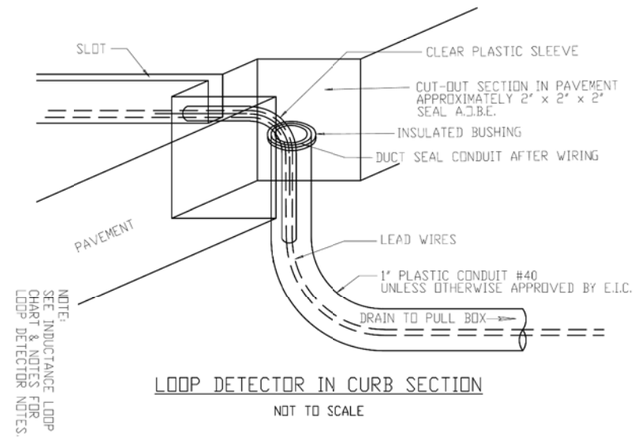
DRAWING TITLE
MISCELLANEOUS
DETAILS
MD-8

DRAWING NO.
22
OF
54



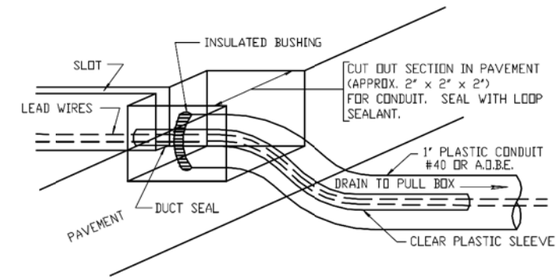
INDUCTANCE LOOP SAW CUT IN BINDER ASPHALT

NOT TO SCALE



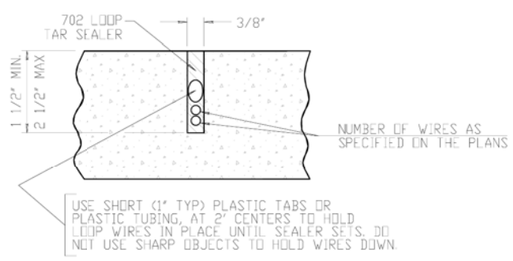
LOOP DETECTOR IN CURB SECTION

NOT TO SCALE



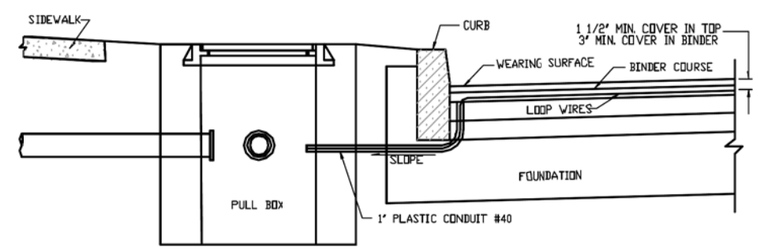
LOOP DETECTOR IN SHOULDER SECTION

NOT TO SCALE



INDUCTANCE LOOP SAW CUT IN CONCRETE OR ASPHALT TOP

NOT TO SCALE



LOOP DETECTOR INSTALLATION TO PULL BOX

NOT TO SCALE

Lu Engineers
ENVIRONMENTAL • TRANSPORTATION • CIVIL

PROJECT MANAGER: J. OTTMAN
DRAWN BY: J. MESSENGER

Department of Environmental Services
Architecture and Engineering Services
City of Rochester, New York

ASSISTANT MANAGING ENGINEER: LISA Y. REYES
CITY ENGINEER: HOLLY E. BARRETT, P.E.

PROJECT TITLE
SCOTTSVILLE ROAD - ELMWOOD AVENUE IMPROVEMENT PROJECT
(WEST CITY LINE - MT HOPE AVE)

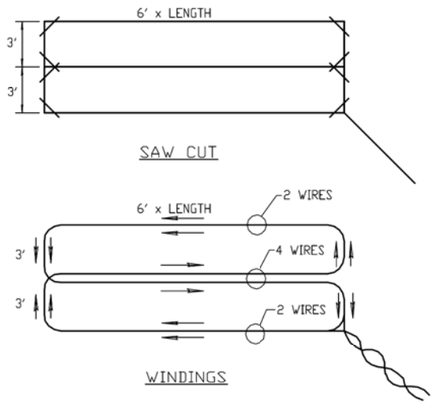
NO.	REVISION	BY	DATE

ISSUED 03/06/2019
CHECKED JWO
DRAWN JRM
DESIGN JRM
SCALE AS SHOWN
PROJECT NUMBER 17102

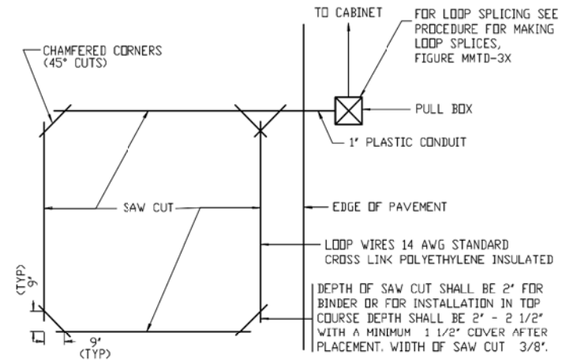
DRAWING TITLE
MISCELLANEOUS DETAILS
MD-9

DRAWING NO.
23
OF
54

FILE NAME = Miscellaneous Details.dgn
DATE/TIME = 06-MAR-2019 11:09
USER = JMessenger



QUADRUPLE LOOP INSTALLATION
NOT TO SCALE



INDUCTANCE LOOP INSTALLATION
NOT TO SCALE

PROCEDURE FOR MAKING LOOP SPLICES

1. BARE WIRE ENDS TO BE SPLICED WITH INSULATED BUTT SPLICE
2. RUBBER TAPE EACH SPLICE ONCE(1).
3. INSTALL WIRES IN SPLICE KIT 3-M SLIC 5 COMMUNICATIONS KIT OR APPROVED EQUIVALENT.

LOOP DETECTOR NOTES

1. ALL SAWED CHANNELS SHALL BE STRAIGHT AND TO THE DEPTH AND LENGTHS INDICATED OR CALLED FOR BY THE ENGINEER OR DRAWING.
2. CORNER CUTS SHALL BE MADE ON A 45° ANGLE WITH 2" LEGS ON THE LOOP.
3. EACH LOOP SHALL CONSIST OF ONE (1) CONTINUOUS LENGTH OF WIRE RUNNING AROUND THE LOOP FROM AND RETURNING TO THE PULL BOX.
4. THE NUMBER OF TURNS AROUND THE LOOP WILL BE A.O.B.E.
5. THE CONTRACTOR MUST PROVIDE A MANUFACTURER'S CERTIFICATION THAT THE EQUIPMENT FURNISHED WILL OPERATE THE SPECIFIED SIZE OF LOOPS AND LEAD-IN LENGTHS SHOWN.
6. ALL LOOP SEALANT MATERIAL SHALL BE APPROVED BY THE ENGINEER.
7. ALL LOOPS SHALL BE PLACED IN THE BINDER COURSE OF ASPHALT A.O.B.E.

INDUCTANCE LOOP CHART & NOTES

SIZE (6') x ____ = NUMBER OF TURNS OF LOOP WIRE

LOOP LENGTH	6'	8'	10'	12'	14'	16'	18'	20'	22'	24'	26'	28'	30'	35'	40'	45'	50'	55'	60'	65'	& ABOVE
TURNS	3	3	3	3	3	3	3	3	3	2	2	2	2	2	2	2	2	2	2	2	2

ALL LOOPS DIRECTLY WIRED TO A DETECTOR CHANNEL, NOTHING IN PARALLEL, OR SERIAL.

Lu Engineers
ENVIRONMENTAL • TRANSPORTATION • CIVIL

PROJECT MANAGER: J. OTTMAN
DRAWN BY: J. MESSENGER

Department of Environmental Services
Architecture and Engineering Services
City of Rochester, New York

ASSISTANT MANAGING ENGINEER: LISA Y. REYES
CITY ENGINEER: HOLLY E. BARRETT, P.E.

PROJECT TITLE
SCOTTSVILLE ROAD - ELMWOOD AVENUE IMPROVEMENT PROJECT
(WEST CITY LINE - MT HOPE AVE)

NO.	REVISION	BY	DATE

ISSUED 03/06/2019
CHECKED JWO
DRAWN JRM
DESIGN JRM
SCALE AS SHOWN
PROJECT NUMBER 17102

DRAWING TITLE
MISCELLANEOUS DETAILS MD-10

DRAWING NO. 24
OF 54

FILE NAME = Miscellanenous Details.dgn
DATE/TIME = 06-MAR-2019 11:10
USER = JMessenger

SIDEWALK CURB RAMPS

ITEM NO.	DESCRIPTION
203.02	UNCLASSIFIED EXCAVATION AND DISPOSAL
304.15	SUBBASE COURSE, OPTIONAL TYPE
402.198903	19 F9 BINDER COURSE HMA, 80 SERIES COMPACTION
418.7603	ASPHALT PAVEMENT JOINT ADHESIVE
S608.12	CONCRETE SIDEWALKS AND DRIVEWAYS
S608.380602	EMBEDDED DETECTABLE WARNING SURFACE
S608.380702	EMBEDDED DETECTABLE WARNING SURFACE AT EXISTING LOCATION
S609.3005	5" STONE CURB (INCLUDING EXCAVATION, BACKFILL AND PAVEMENT BASE RESTORATION)

RAMP NO.	LOCATION		DESCRIPTION	WORK DESCRIPTION	ACCESS RAMP TYPE	203.02 (CY)	304.15 (CY)	402.198903 (TON)	418.7603 (FT)	S608.12 (CY)	S608.380602 (SF)	S608.380702 (SF)	S609.3005 (FT)
	STATION	SIDE											
CR-1	S 12+05	LT	SW CORNER AT KINGSBORO ROAD	REPLACE RAMP	E	5.4	2.7	0.2	18.3	2.7	10.0		16.3
CR-2	S 12+45	LT	NW CORNER AT KINGSBORO ROAD	REPLACE RAMP	E	8.1	4.2	0.4	31.4	3.9	10.0		28.8
CR-3	S 12+70	LT	NW CORNER AT KINGSBORO ROAD	REPLACE RAMP	D	8.7	4.5	0.3	22.6	4.2	10.0		20.1
CR-4	S 12+70	RT	NE CORNER AT KINGSBORO ROAD	REPLACE RAMP	C	5.1	2.4	0.3	21.0	2.8	10.0		19.0
CR-5	S 15+65	RT	SE CORNER AT VIXETTE STREET	REPLACE RAMP	F	7.5	3.5	0.4	30.8	4.0	20.0		28.8
CR-6	S 16+05	RT	NE CORNER AT VIXETTE STREET	REPLACE RAMP	F	6.6	3.1	0.4	29.1	3.6	20.0		27.1
CR-7	S 18+55	LT	SW CORNER AT PIONEER STREET	REPLACE RAMP	A	9.8	5.2	0.4	30.2	4.7	10.0		27.1
CR-8	S 18+55	RT	SE CORNER AT MINEOLA STREET	REPLACE RAMP	F	6.0	2.8	0.3	25.9	3.2	20.0		23.9
CR-9	S 18+95	LT	NW CORNER AT PIONEER STREET	REPLACE RAMP	A	7.8	4.1	0.4	33.5	3.8	10.0		30.3
CR-10	S 18+95	RT	NE CORNER AT MINEOLA STREET	REPLACE RAMP	F	6.8	3.2		26.0	3.7	20.0		26.0
CR-11	S 24+55	LT	SW CORNER ATWINBOURNE ROAD	REPLACE RAMP	A	7.7	4.0	0.4	27.6	3.7	10.0		24.7
CR-12	S 24+95	LT	NW CORNER AT WINBOURNE ROAD	REPLACE RAMP	A	9.6	5.0	0.4	32.1	4.6	10.0		28.9
CR-13	S 27+45	RT	SE CORNER AT GENESEE STREET	INSTALL DWU ONLY	-							10.0	
CR-14	S 28+15	RT	NE CORNER AT GENESEE STREET	REPLACE RAMP	E	3.0	1.4	0.2	17.4	1.6	10.0		15.4
CR-15	S 28+40	LT	SW CORNER AT GENESEE STREET	REPLACE RAMP	F	4.9	2.3	0.3	20.7	2.7	10.0		18.7
CR-16	E 110+30	RT	NE CORNER AT ELMWOOD CROSSING AT GENESEE STREET	REPLACE RAMP	F	6.1	2.9	0.3	20.7	3.3	10.0		18.7
CR-17	E 110+45	LT	SW MEDIAN CORNER AT GENESEE STREET	REPLACE RAMP	D	3.7	1.8	0.3	18.6	2.0	10.0		16.4
CR-18	E 110+50	LT	NW MEDIAN CORNER AT ELMWOOD CROSSING AT GENESEE STREET	INSTALL DWU ONLY	-							12.0	
CR-19	E 110+55	LT	NE MEDIAN CORNER AT ELMWOOD WB RT TURN LN AT GENESEE ST.	INSTALL DWU ONLY	-							10.0	
CR-20	E 110+70	LT	NW CORNER AT ELMWOOD WB RT TURN LN AT GENESEE ST.	INSTALL DWU ONLY	-							14.0	
CR-21	E 120+55	RT	SE CORNER AT GENESEE VALLEY LOT ENTRANCE	REPLACE RAMP	G	5.0	2.6	0.2	17.3	2.5	12.0		15.3
CR-22	E 121+10	RT	NE CORNER AT GENESEE VALLEY LOT ENTRANCE	INSTALL DWU ONLY	-							10.0	
CR-23	E 122+00	LT	SW CORNER AT ELMWOOD CROSSING AT PLYMOUTH AVENUE	REPLACE RAMP	A	14.4	7.7	0.4	35.3	6.8	10.0		32.7
CR-24	E 122+00	RT	SE CORNER AT ELMWOOD CROSSING AT PLYMOUTH AVENUE	REPLACE RAMP	B MOD.	12.3	5.9	0.3	24.0	6.5	10.0		22.0
CR-25	E 122+45	RT	SE CORNER AT GENESEE VALLEY LOT EXIT	REPLACE RAMP	1	3.0	1.5	0.2	14.0	1.6	10.0		11.5
CR-26	E 122+85	LT	NW CORNER AT PLYMOUTH AVENUE	REPLACE RAMP	A	9.3	4.8	0.3	26.2	4.5	10.0		23.8
CR-27	E 122+85	RT	NE CORNER AT GENESEE VALLEY LOT EXIT	REPLACE RAMP	A	7.8	4.0	0.3	27.1	3.8	10.0		24.0
CR-28	E 131+40	LT	SW CORNER AT WILSON BOULEVARD	REPLACE RAMP	A	5.7	2.9	0.3	22.6	2.9	10.0		19.8
CR-29	E 131+40	RT	SE CORNER AT MOORE ROAD	(SEE NOTE 1)	-								
CR-30	E 131+85	RT	SE CORNER AT MOORE ROAD	REPLACE RAMP	F	7.0	3.3	0.3	25.5	3.8	10.0		23.5
CR-31	E 132+20	LT	NW CORNER AT WILSON BOULEVARD	NO WORK	-								
CR-32	E 132+45	RT	NE CORNER AT MOORE ROAD	NO WORK	-								
CR-33	E 137+70	RT	SE CORNER AT ELMWOOD EB RT TURN AT KENDRICK ROAD	REPLACE RAMP	B MOD.	3.1	1.6	0.2	12.0	1.6	10.0		10.0
CR-34	E 137+80	RT	NE CORNER AT ELMWOOD EB RT TURN AT KENDRICK ROAD	REPLACE RAMP	12 MOD.	2.9	1.5	0.4	29.5	1.5	10.0		27.5
CR-35	E 137+90	RT	SE CORNER AT ELMWOOD WB LT TURN AT KENDRICK ROAD	REPLACE RAMP	12 MOD.	2.9	1.5	0.4	28.0	1.5	10.0		26.0
					SUBTOTAL	**180.2**	**90.4**	**8.3**	**667.4**	**91.5**	**312.0**	**56.0**	**606.3**

NOTES:

1. THE RAMP REFERED TO WILL BE REMOVED SEPERATLY UNDER THE "ELMWOOD AVENUE COLLEGE TOWN CYCLE TRACK" CONTRACT D034682 IN THE SPRING OF 2019.
2. THE CONTRACTOR SHALL REFER TO NYSDOT STANDARD SHEET SERIES 608-01 FOR RAMP TYPES (1 & 12 MOD.) AS SHOWN IN THE TABLE.

FILE NAME = Miscellaneous Tables.dgn
DATE/TIME = 12-MAR-2019 11:29
USER = JMessenger

Lu Engineers
ENVIRONMENTAL • TRANSPORTATION • CIVIL

PROJECT MANAGER: J. OTTMAN
DRAWN BY: J. MESSENGER

Department of Environmental Services
Architecture and Engineering Services
City of Rochester, New York

ASSISTANT MANAGING ENGINEER LISA Y. REYES
CITY ENGINEER HOLLY E. BARRETT, P.E.

PROJECT TITLE
SCOTTSVILLE ROAD - ELMWOOD AVENUE
IMPROVEMENT PROJECT
(WEST CITY LINE - MT HOPE AVE)

NO.	REVISION	BY	DATE

ISSUED 03/06/2019
CHECKED JWO
DRAWN JRM
DESIGN JRM
SCALE NONE
PROJECT NUMBER 17102

DRAWING TITLE
MISCELLANEOUS TABLES MT-1

DRAWING NO.
25
OF
54

SIDEWALK CURB RAMPS

ITEM NO.	DESCRIPTION
203.02	UNCLASSIFIED EXCAVATION AND DISPOSAL
304.15	SUBBASE COURSE, OPTIONAL TYPE
402.198903	19 F9 BINDER COURSE HMA, 80 SERIES COMPACTION
418.7603	ASPHALT PAVEMENT JOINT ADHESIVE
S608.12	CONCRETE SIDEWALKS AND DRIVEWAYS
S608.380602	EMBEDDED DETECTABLE WARNING SURFACE
S608.380702	EMBEDDED DETECTABLE WARNING SURFACE AT EXISTING LOCATION
S609.3005	5" STONE CURB (INCLUDING EXCAVATION, BACKFILL AND PAVEMENT BASE RESTORATION)

RAMP NO.	LOCATION		DESCRIPTION	WORK DESCRIPTION	REPLACE RAMP TYPE	203.02 (CY)	304.15 (CY)	402.198903 (TON)	418.7603 (FT)	S608.12 (CY)	S608.380602 (SF)	S608.380702 (SF)	S609.3005 (FT)
	STATION	SIDE											
CR-36	E 138+10	RT	NE CORNER AT ELMWOOD WB LT TURN AT KENDRICK ROAD	REPLACE RAMP	12 MOD.	1.6	0.8	0.3	21.5	0.8	10.0		19.5
CR-37	E 138+15	RT	SE CORNER AT KENDRICK ROAD LT TURN LANE	REPLACE RAMP	12 MOD.	1.6	0.8	0.3	21.5	0.8	10.0		19.5
CR-38	E 138+40	RT	NE CORNER AT KENDRICK ROAD LT TURN LANE	REPLACE RAMP	12 MOD.	4.5	2.3	0.2	14.0	2.2	10.0		12.0
CR-39	E 138+45	LT	NW CORNER AT ELMWOOD CROSSING AT KENDRICK ROAD	(SEE NOTE 1)	-								
CR-40	E 138+45	RT	NE CORNER AT ELMWOOD CROSSING AT KENDRICK ROAD	REMOVE RAMP	-			0.4	27.0				25.0
CR-41	E 138+55	RT	SE CORNER AT KENDRICK ROAD RT TURN LANE	REPLACE RAMP	12 MOD.	4.5	2.3	0.2	14.0	2.2	10.0		12.0
CR-42	E 138+70	RT	NE CORNER AT KENDRICK ROAD RT TURN LANE	REPLACE RAMP	B MOD.	2.5	1.2	0.2	12.9	1.4	10.0		10.9
CR-43	E 142+65	RT	SE CORNER AT UR SCHOOL OF MEDICINE (SOM) ENTRANCE	INSTALL DWU ONLY	-							10.0	
CR-44	E 143+15	LT	NW CORNER AT ELMWOOD CROSSING AT UR SOM ENTRANCE	NO WORK	-								
CR-45	E 143+15	RT	NE CORNER AT UR SCHOOL OF MEDICINE (SOM) ENTRANCE	REPLACE RAMP	E	4.3	2.2	0.2	18.7	2.2	10.0		15.9
CR-46	E 143+30	RT	NE CORNER AT ELMWOOD CROSSING AT UR SOM ENTRANCE	REPLACE RAMP	E	6.9	3.6	0.2	18.7	3.4	10.0		15.9
CR-47	E 145+75	RT	SE CORNER AT UR STRONG HOSPITAL LOADING ENTRANCE	INSTALL DWU ONLY	-							10.0	
CR-48	E 146+10	RT	NE CORNER AT UR STRONG HOSPITAL LOADING ENTRANCE	REPLACE RAMP	1	2.8	1.4	0.2	16.2	1.5	10.0		14.2
CR-49	E 148+10	RT	SE CORNER AT UR SCHOOL OF MEDICINE LOT 6 ENTRANCE	REPLACE RAMP	1	2.8	1.4	0.2	16.2	1.5	10.0		14.2
CR-50	E 148+25	LT	SW CORNER AT UR SCHOOL OF MEDICINE LOT 7 ENTRANCE	NO WORK	-								
CR-51	E 148+55	RT	NE CORNER AT UR SCHOOL OF MEDICINE LOT 6 ENTRANCE	REPLACE RAMP	1	2.8	1.4	0.2	16.2	1.5	10.0		14.2
CR-52	E 148+65	LT	NW CORNER AT UR SCHOOL OF MEDICINE LOT 7 ENTRANCE	NO WORK	-								
CR-53	E 151+25	RT	SE CORNER AT UR STRONG HOSPITAL AMBULANCE ENTR.	REPLACE RAMP	1	2.8	1.4	0.2	16.2	1.5	10.0		14.2
CR-54	E 151+50	LT	SW CORNER AT MT. HOPE CEMETERY ENTRANCE	NO WORK	-								
CR-55	E 151+70	RT	NE CORNER AT UR STRONG HOSPITAL AMBULANCE ENTR.	REPLACE RAMP	1	2.8	1.4	0.2	16.2	1.5	10.0		14.2
CR-56	E 151+85	LT	NW CORNER AT MT. HOPE CEMETERY ENTRANCE	NO WORK	-								
CR-57	E 154+10	RT	SE CORNER AT UR STRONG HOSPITAL MAIN ENTRANCE	REPLACE RAMP	A	5.7	2.9	0.3	26.1	2.9	10.0		23.2
CR-58	E 154+95	LT	NW CORNER AT ELMWOOD CROSSING AT UR HOSPITAL ENTR.	NO WORK	-								
CR-59	E 154+95	RT	NE CORNER AT ELMWOOD CROSSING AT UR HOSPITAL ENTR.	REPLACE RAMP	A	5.6	2.8	0.3	26.1	2.8	10.0		23.2
CR-60	E 159+75	LT	SW CORNER AT ELMWOOD CROSSING AT EAST DRIVE	NO WORK	-								
CR-61	E 159+75	RT	SE CORNER AT ELMWOOD CROSSING AT EAST DRIVE	NO WORK	E	5.2	2.7	0.2	17.4	2.6			15.4
CR-62	E 15+85	RT	SE CORNER AT EAST DRIVE	NO WORK	E	5.2	2.7	0.2	17.4	2.6			15.4
CR-63	E 160+40	RT	NE CORNER AT EAST DRIVE	NO WORK	-								
CR-64	E 170+25	RT	SE CORNER AT MT. HOPE AVENUE	OUTSIDE OF PAVING LIMITS	-								
CR-65	E 170+85	LT	SW CORNER AT MT. HOPE AVENUE	OUTSIDE OF PAVING LIMITS	-								
CR-66	E 170+95	LT	SW CORNER AT MT. HOPE AVENUE	OUTSIDE OF PAVING LIMITS	-								
CR-67	E 171+25	RT	NE CORNER AT MT. HOPE AVENUE	OUTSIDE OF PAVING LIMITS	-								
CR-68	E 171+45	RT	NE CORNER AT MT. HOPE AVENUE	OUTSIDE OF PAVING LIMITS	-								
CR-69	E 171+75	LT	NW CORNER AT MT. HOPE AVENUE	OUTSIDE OF PAVING LIMITS	-								
CR-70	E 171+80	LT	NW CORNER AT MT. HOPE AVENUE	OUTSIDE OF PAVING LIMITS	-								
					SUBTOTAL	61.6	31.3	4.0	316.3	31.4	140.0	20.0	278.9
					TOTAL	241.8	121.7	12.3	983.7	122.9	452.0	76.0	885.2

NOTES:

1. THE RAMP REFERED TO WILL BE REMOVED SEPERATLY UNDER THE "ELMWOOD AVENUE COLLEGE TOWN CYCLE TRACK" CONTRACT D034682 IN THE SPRING OF 2019.
2. THE CONTRACTOR SHALL REFER TO NYSDOT STANDARD SHEET SERIES 608-01 FOR RAMP TYPES (1 & 12 MOD.) AS SHOWN IN THE TABLE.

FILE NAME = Miscellaneous Tables.dgn
DATE/TIME = 12-MAR-2019 11:30
USER = JMessenger

Lu Engineers
ENVIRONMENTAL • TRANSPORTATION • CIVIL

PROJECT MANAGER: J. OTTMAN
DRAWN BY: J. MESSENGER

Department of Environmental Services
Architecture and Engineering Services
City of Rochester, New York

ASSISTANT MANAGING ENGINEER: LISA Y. REYES
CITY ENGINEER: HOLLY E. BARRETT, P.E.

PROJECT TITLE
SCOTTSVILLE ROAD - ELMWOOD AVENUE
IMPROVEMENT PROJECT
(WEST CITY LINE - MT HOPE AVE)

NO.	REVISION	BY	DATE

ISSUED 03/06/2019
CHECKED JWO
DRAWN JRM
DESIGN JRM
SCALE NONE
PROJECT NUMBER 17102

DRAWING TITLE
MISCELLANEOUS TABLES MT-2

DRAWING NO.
26

MILLING / PAVING

ITEM NO.	DESCRIPTION
R204.02	MILLING - 2" THICKNESS
R204.04	MILLING - 4" THICKNESS
402.097203	9.5 F2 TOP COURSE HMA, 70 SERIES COMPACTION
402.197903	19 F9 BINDER COURSE HMA, 70 SERIES COMPACTION
R406.01	PAVEMENT KEY
S407.0201	DILUTED TACK COAT
418.7603	ASPHALT PAVEMENT JOINT ADHESIVE

STATION / LOCATION		MILLING DEPTH (IN)	TOP DEPTH (IN)	BINDER DEPTH (IN)	LENGTH (FT)	WIDTH (FT)	R204.02 (SY)	R204.04 (SY)	402.097203 (TON)	402.197903 (TON)	R406.01 (FT)	S407.0201 (SY)	418.7603 (FT)	REMARKS
FROM	TO													
S 10+00	S 24+50	4.0	2.0	2.0	1,450.0	40.0		6,569.5	827.8	813.0	40.0	13,138.9	4,390.0	MILL FULL WIDTH OF EXISTING ROADWAY PAVEMENT SAWCUT AT LIMITS OF MILLING OPERATIONS
S 12+30	KINGSBORO ROAD	2.0	2.0		50.0	24.0	168.8		21.3		24.0	168.8	48.0	MILL FULL WIDTH OF EXISTING SIDE STREET PAVEMENT FROM 20.0' LT. (EOP) TO 65.0' LT. (PAVING LIMITS) SAWCUT AT LIMITS OF MILLING OPERATIONS
S 15+85	VIXETTE STREET	2.0	2.0		20.0	20.0	59.8		7.6		20.0	59.8	40.0	MILL FULL WIDTH OF EXISTING SIDE STREET PAVEMENT FROM 20.0' RT. (EOP) TO 40.0' LT. (PAVING LIMITS) SAWCUT AT LIMITS OF MILLING OPERATIONS
S 18+75	PIONEER STREET	2.0	2.0		25.0	23.0	93.9		11.9		23.0	93.9	46.0	MILL FULL WIDTH OF EXISTING SIDE STREET PAVEMENT FROM 20.0' LT. (EOP) TO 45.0' LT. (PAVING LIMITS) SAWCUT AT LIMITS OF MILLING OPERATIONS
S 18+75	MINEOLA STREET	2.0	2.0		20.0	25.0	69.7		8.8		25.0	69.7	50.0	MILL FULL WIDTH OF EXISTING SIDE STREET PAVEMENT FROM 20.0' RT. (EOP) TO 40.0' RT. (PAVING LIMITS) SAWCUT AT LIMITS OF MILLING OPERATIONS
S 24+50	S 28+77	2.0	2.0		427.0	VARIES	2,464.5		310.6			2,464.5	1,281.0	MILL FULL WIDTH OF EXISTING ROADWAY PAVEMENT
S 24+75	WINBOURNE ROAD	2.0	2.0		20.0	28.0	90.2		11.4		28.0	90.2	56.0	MILL FULL WIDTH OF EXISTING SIDE STREET PAVEMENT FROM 20.0' LT. (EOP) TO 40.0' LT. (PAVING LIMITS) SAWCUT AT LIMITS OF MILLING OPERATIONS
S 28+00	GENESEE STREET	2.0	2.0		43.0	28.0	139.4		17.6		28.0	139.4	112.0	MILL FULL WIDTH OF EXISTING SIDE STREET PAVEMENT FROM 30.0' RT. (EOP) TO 70.0' RT. (PAVING LIMITS) SAWCUT AT LIMITS OF MILLING OPERATIONS
E 110+00	E 126+12	2.0	2.0		1,612.0	55.0	10,042.3		1,265.4		55.0	10,042.3	6,503.0	MILL FULL WIDTH OF EXISTING ROADWAY PAVEMENT SAWCUT AT LIMITS OF MILLING OPERATIONS
E 110+00	GENESEE STREET	2.0	2.0		80.0	66.0	693.8		87.5		66.0	693.8	330.0	MILL FULL WIDTH OF EXISTING SIDE STREET PAVEMENT FROM 24.0' LT. (EOP) TO 115.0' LT. (PAVING LIMITS) SAWCUT AT LIMITS OF MILLING OPERATIONS
E 120+85	GV SPORTS COMPLEX	2.0	2.0		24.0	38.0	138.5		17.5		38.0	138.5	76.0	MILL FULL WIDTH OF EXISTING SIDE STREET PAVEMENT FROM 28.0' RT. (EOP) TO 52.0' RT. (PAVING LIMITS) SAWCUT AT LIMITS OF MILLING OPERATIONS
E 122+45	S. PLYMOUTH AVENUE	2.0	2.0		66.0	44.0	467.8		59.0		44.0	467.8	176.0	MILL FULL WIDTH OF EXISTING SIDE STREET PAVEMENT FROM 28.0' LT. (EOP) TO 94.0' LT. (PAVING LIMITS) SAWCUT AT LIMITS OF MILLING OPERATIONS
E 122+60	GV SPORTS COMPLEX	2.0	2.0		30.0	29.0	118.9		15.0		29.0	118.9	58.0	MILL FULL WIDTH OF EXISTING SIDE STREET PAVEMENT FROM 28.0' RT. (EOP) TO 58.0' RT. (PAVING LIMITS) SAWCUT AT LIMITS OF MILLING OPERATIONS
E 131+15	E 160+60	1.5	1.5		2,945.0	VARIES	18,543.9		1,817.4		120.0	18,543.9	8,955.0	MILL FULL WIDTH OF EXISTING ROADWAY PAVEMENT SAWCUT AT LIMITS OF MILLING OPERATIONS
E 131+70	WILSON BOULEVARD	1.5	1.5		60.0	55.0	433.6		42.5		55.0	433.6	275.0	MILL FULL WIDTH OF EXISTING SIDE STREET PAVEMENT FROM 36.0' LT. (EOP) TO 96.0' LT. (PAVING LIMITS) SAWCUT AT LIMITS OF MILLING OPERATIONS
E 132+15	MOORE ROAD	1.5	1.5		36.0	37.0	206.7		20.3		37.0	206.7	111.0	MILL FULL WIDTH OF EXISTING SIDE STREET PAVEMENT FROM 29.0' RT. (EOP) TO 65.0' RT. (PAVING LIMITS) SAWCUT AT LIMITS OF MILLING OPERATIONS
E 138+05	KENDRICK ROAD	1.5	1.5		55.0	34.0	286.2		28.1		34.0	286.2	102.0	MILL FULL WIDTH OF EXISTING SIDE STREET PAVEMENT FROM 41.0' RT. (EOP) TO 96.0' RT. (PAVING LIMITS) SAWCUT AT LIMITS OF MILLING OPERATIONS
E 138+35	KENDRICK ROAD	1.5	1.5		55.0	30.0	265.4		26.1		30.0	265.4	90.0	MILL FULL WIDTH OF EXISTING SIDE STREET PAVEMENT FROM 41.0' RT. (EOP) TO 96.0' RT. (PAVING LIMITS) SAWCUT AT LIMITS OF MILLING OPERATIONS
E 146+00	U OF R ENTRANCE	1.5	1.5		18.0	26.0	80.7		8.0		26.0	80.7	52.0	MILL FULL WIDTH OF EXISTING ENTRANCE PAVEMENT FROM 32.0' RT. (EOP) TO 50.0' RT. (PAVING LIMITS) SAWCUT AT LIMITS OF MILLING OPERATIONS
E 148+30	U OF R ENTRANCE	1.5	1.5		18.0	30.0	97.0		9.6		30.0	97.0	60.0	MILL FULL WIDTH OF EXISTING ENTRANCE PAVEMENT FROM 32.0' RT. (EOP) TO 50.0' RT. (PAVING LIMITS) SAWCUT AT LIMITS OF MILLING OPERATIONS
E 151+50	U OF R ENTRANCE	1.5	1.5		18.0	35.0	112.3		11.0		35.0	112.3	70.0	MILL FULL WIDTH OF EXISTING ENTRANCE PAVEMENT FROM 32.0' RT. (EOP) TO 50.0' RT. (PAVING LIMITS) SAWCUT AT LIMITS OF MILLING OPERATIONS
E 154+50	STRONG HOSPITAL ENTRANCE	1.5	1.5		38.0	58.0	300.4		29.5		58.0	300.4	116.0	MILL FULL WIDTH OF EXISTING ENTRANCE PAVEMENT FROM 32.0' RT. (EOP) TO 70.0' RT. (PAVING LIMITS) SAWCUT AT LIMITS OF MILLING OPERATIONS
E 160+10	EAST DRIVE	1.5	1.5		50.0	48.0	301.2		29.6		48.0	301.2	96.0	MILL FULL WIDTH OF EXISTING ENTRANCE PAVEMENT FROM 32.0' RT. (EOP) TO 82.0' RT. (PAVING LIMITS) SAWCUT AT LIMITS OF MILLING OPERATIONS
						TOTAL	35,175.0	6,569.5	4,683.5	813.0	893.0	48,313.9	23,093.0	

FILE NAME = Miscellaneous Tables.dgn
DATE/TIME = 12-MAR-2019 11:44
USER = JMessenger

Lu Engineers
ENVIRONMENTAL • TRANSPORTATION • CIVIL

PROJECT MANAGER: J. OTTMAN
DRAWN BY: J. MESSENGER

Department of Environmental Services
Architecture and Engineering Services
City of Rochester, New York

ASSISTANT MANAGING ENGINEER LISA Y. REYES
CITY ENGINEER HOLLY E. BARRETT, P.E.

PROJECT TITLE
SCOTTSVILLE ROAD - ELMWOOD AVENUE IMPROVEMENT PROJECT
(WEST CITY LINE - MT HOPE AVE)

NO.	REVISION	BY	DATE

ISSUED 03/06/2019
CHECKED JWO
DRAWN JRM
DESIGN JRM
SCALE NONE
PROJECT NUMBER 17102

DRAWING TITLE
MISCELLANEOUS TABLES MT-3

DRAWING NO.
27
OF
54

PAVEMENT REPAIR

ITEM NO.	DESCRIPTION
203.02	UNCLASSIFIED EXCAVATION AND DISPOSAL
402.198903	19 F9 BINDER COURSE HMA, 80 SERIES COMPACTION
402.378903	37.5 F9 BASE COURSE HMA, 80 SERIES COMPACTION
S407.0201	DILUTED TACK COAT
418.7603	ASPHALT PAVEMENT JOINT ADHESIVE
627.50140008	CUTTING PAVEMENT

STATION		SIDE	REMOVAL DEPTH (IN)	TOP DEPTH (IN)	BINDER DEPTH (IN)	BASE DEPTH (IN)	LENGTH (FT)	WIDTH (FT)	203.02 (CY)	402.198903 (TON)	402.378903 (TON)	S407.0201 (SY)	418.7603 (FT)	627.50140008 (FT)	REMARKS
FROM	TO														
S 12+54	S 15+23	RT.	6.0	SEE NOTE 2	SEE NOTE 2	6.0	269.0	4.0	20.1		40.6		277.0	277.0	SEE NOTE 3
S 15+60	S 15+70	RT.	6.0	SEE NOTE 2	SEE NOTE 2	6.0	10.0	5.0	1.1		2.3		20.0	20.0	SEE NOTE 3
S 15+80	S 16+00	RT.	6.0	SEE NOTE 2	SEE NOTE 2	6.0	20.0	6.8	2.6		5.2		53.6	53.6	SEE NOTE 3
S 16+74	S 16+84	RT.	6.0	SEE NOTE 2	SEE NOTE 2	6.0	10.0	3.3	0.7		1.3		16.6	16.6	SEE NOTE 3
S 17+12	S 17+22	RT.	6.0	SEE NOTE 2	SEE NOTE 2	6.0	10.0	3.3	0.7		1.3		16.6	16.6	SEE NOTE 3
S 17+27	S 18+26	RT.	6.0	SEE NOTE 2	SEE NOTE 2	6.0	99.0	3.3	6.0		12.1		105.6	105.6	SEE NOTE 3
S 17+98	S 18+48	LT.	6.0	SEE NOTE 2	SEE NOTE 2	6.0	50.0	5.0	4.7		9.4		60.0	60.0	SEE NOTE 3
S 18+26	S 18+44	RT.	6.0	SEE NOTE 2	SEE NOTE 2	6.0	18.0	9.5	3.3		6.6		37.0	37.0	SEE NOTE 3
S 18+52	S 18+61	RT.	6.0	SEE NOTE 2	SEE NOTE 2	6.0	9.0	4.0	0.7		1.4		17.0	17.0	SEE NOTE 3
S 19+03	S 19+44	RT.	6.0	SEE NOTE 2	SEE NOTE 2	6.0	41.0	10.4	7.9		16.0		61.8	61.8	SEE NOTE 3
S 20+07	S 21+20	RT.	6.0	SEE NOTE 2	SEE NOTE 2	6.0	113.0	3.3	6.9		14.0		119.6	119.6	SEE NOTE 3
S 20+39	S 21+39	LT.	6.0	SEE NOTE 2	SEE NOTE 2	6.0	100.0	3.0	5.6		11.4		106.0	106.0	SEE NOTE 3
S 21+69	S 22+32	RT.	6.0	SEE NOTE 2	SEE NOTE 2	6.0	63.0	20.0	23.3		47.2		166.0	103.0	SEE NOTE 3
S 21+92	S 22+14	LT.	6.0	SEE NOTE 2	SEE NOTE 2	6.0	22.0	5.0	2.1		4.2		32.0	32.0	SEE NOTE 3
S 22+83	S 22+89	LT.	6.0	SEE NOTE 2	SEE NOTE 2	6.0	6.0	7.0	0.8		1.7		20.0	20.0	SEE NOTE 3
S 22+87	S 23+52	RT.	6.0	SEE NOTE 2	SEE NOTE 2	6.0	65.0	10.3	12.5		25.2		85.6	85.6	SEE NOTE 3
S 23+43	S 24+44	LT.	6.0	SEE NOTE 2	SEE NOTE 2	6.0	101.0	17.0	32.7		66.1		236.0	135.0	SEE NOTE 3
S 23+52	S 24+44	RT.	6.0	SEE NOTE 2	SEE NOTE 2	6.0	92.0	19.0	32.2		65.2		222.0	130.0	SEE NOTE 3
S 25+32	S 26+86	RT.	8.0	SEE NOTE 1	2.0	6.0	154.0	4.1	15.6	8.7	23.7	70.2	316.2	316.2	SEE NOTE 3
								SUBTOTAL	179.5	8.7	354.9	70.2	1,968.6	1,712.6	

NOTES:

1. HMA TOP COURSE TO BE REMOVED DURING MILLING OPERATIONS AND TO BE PAID FOR UNDER ITEM R204.02, MILLING - 2" THICKNESS.
2. HMA TOP AND BINDER COURSES TO BE REMOVED DURING MILLING OPERATIONS AND TO BE PAID FOR UNDER ITEM R204.04, MILLING - 4" THICKNESS.
3. HMA PAVEMENT REPAIR LOCATIONS ARE TO BE APPROVED BY THE RESIDENT PROJECT REPRESENTATIVE AFTER MILLING OPERATIONS HAVE BEEN COMPLETED. THE R.P.R. SHALL VERIFY ALL PAVEMENT REPAIR AREAS AND THE TYPE OF REPAIR BEFORE THE CONTRACTOR BEGINS WORK.
4. IF THE CONTRACTOR ELECTS TO "DROP THE MILL HEAD" TO BEGIN PAVEMENT REPAIR, NO PAYMENT WILL BE MADE FOR ITEM 627.50140008, CUTTING PAVEMENT.

FILE NAME = Miscellaneous Tables.dgn
DATE/TIME = 12-MAR-2019 11:44
USER = JMessenger

Lu Engineers
ENVIRONMENTAL • TRANSPORTATION • CIVIL
PROJECT MANAGER: J. OTTMAN
DRAWN BY: J. MESSENGER

Department of Environmental Services
Architecture and Engineering Services
City of Rochester, New York
ASSISTANT MANAGING ENGINEER LISA Y. REYES
CITY ENGINEER HOLLY E. BARRETT, P.E.

PROJECT TITLE
SCOTTSVILLE ROAD - ELMWOOD AVENUE
IMPROVEMENT PROJECT
(WEST CITY LINE - MT HOPE AVE)

NO.	REVISION	BY	DATE

ISSUED 03/06/2019
CHECKED JWO
DRAWN JRM
DESIGN JRM
SCALE NONE
PROJECT NUMBER 17102

DRAWING TITLE
MISCELLANEOUS TABLES MT-4

DRAWING NO.
28
OF
54

PAVEMENT REPAIR

ITEM NO.	DESCRIPTION
203.02	UNCLASSIFIED EXCAVATION AND DISPOSAL
402.198903	19 F9 BINDER COURSE HMA, 80 SERIES COMPACTION
402.378903	37.5 F9 BASE COURSE HMA, 80 SERIES COMPACTION
S407.0201	DILUTED TACK COAT
418.7603	ASPHALT PAVEMENT JOINT ADHESIVE
627.50140008	CUTTING PAVEMENT

STATION FROM	STATION TO	SIDE	REMOVAL DEPTH (IN)	TOP DEPTH (IN)	BINDER DEPTH (IN)	BASE DEPTH (IN)	LENGTH (FT)	WIDTH (FT)	203.02 (CY)	402.198903 (TON)	402.378903 (TON)	S407.0201 (SY)	418.7603 (FT)	627.50140008 (FT)	REMARKS
E 117+43	E 117+88	RT.	8.0	SEE NOTE 1	2.0	6.0	45.0	12.1	13.5	7.5	20.4	60.5	69.2	69.2	SEE NOTE 3
E 119+42	E 119+78	RT.	8.0	SEE NOTE 1	2.0	6.0	36.0	22.1	19.6	10.9	29.7	87.9	116.2	80.2	SEE NOTE 3
E 120+94	E 123+49	LT.	8.0	SEE NOTE 1	2.0	6.0	255.0	17.2	108.7	60.6	165.1	489.2	289.4	289.4	SEE NOTE 3
E 120+95	E 122+46	RT.	8.0	SEE NOTE 1	2.0	6.0	151.0	3.6	13.4	7.5	20.3	59.9	309.2	309.2	SEE NOTE 3
E 121+84	E 122+27	RT.	8.0	SEE NOTE 1	2.0	6.0	43.0	10.4	11.2	6.2	16.9	50.0	63.8	63.8	SEE NOTE 3
E 123+16	E 124+55	RT.	8.0	SEE NOTE 1	2.0	6.0	139.0	23.5	78.7	43.8	119.4	353.8	325.0	186.0	SEE NOTE 3
E 124+81	E 125+19	LT.	8.0	SEE NOTE 1	2.0	6.0	38.0	10.7	10.0	5.6	15.1	44.6	59.4	59.4	SEE NOTE 3
E 131+68	E 131+80	RT.	8.0	SEE NOTE 1	2.0	6.0	12.0	4.6	1.4	0.8	2.1	6.2	21.2	21.2	SEE NOTE 3
E 133+14	E 133+72	RT.	8.0	SEE NOTE 1	2.0	6.0	58.0	24.0	32.9	18.3	49.9	147.7	106.0	48.0	SEE NOTE 3
E 133+33	E 133+44	LT.	8.0	SEE NOTE 1	2.0	6.0	11.0	42.0	11.0	6.2	16.7	49.5	84.0	84.0	SEE NOTE 3
E 134+88	E 139+13	LT.	8.0	SEE NOTE 1	2.0	6.0	425.0	4.0	40.1	22.4	60.9	180.4	858.0	858.0	SEE NOTE 3
E 135+73	E 135+96	RT.	8.0	SEE NOTE 1	2.0	6.0	23.0	24.0	13.7	7.6	20.7	61.3	71.0	48.0	SEE NOTE 3
E 137+01	E 137+51	RT.	8.0	SEE NOTE 1	2.0	6.0	50.0	11.5	15.8	8.8	23.9	70.8	73.0	73.0	SEE NOTE 3
E 139+50	E 139+54	RT.	8.0	SEE NOTE 1	2.0	6.0	4.0	24.0	2.6	1.5	4.0	11.7	48.0	48.0	SEE NOTE 3
E 139+69	E 139+81	RT.	8.0	SEE NOTE 1	2.0	6.0	12.0	24.0	7.8	4.4	11.8	34.8	60.0	48.0	SEE NOTE 3
E 139+86	E 140+37	LT.	8.0	SEE NOTE 1	2.0	6.0	51.0	24.0	29.3	16.3	44.4	131.6	150.0	99.0	SEE NOTE 3
E 142+11	E 142+62	RT.	8.0	SEE NOTE 1	2.0	6.0	51.0	24.0	30.0	16.7	45.6	134.9	99.0	48.0	SEE NOTE 3
E 143+13	E 144+00	RT.	8.0	SEE NOTE 1	2.0	6.0	87.0	24.0	51.9	28.9	78.7	233.2	135.0	48.0	SEE NOTE 3
E 146+38	E 146+66	RT.	8.0	SEE NOTE 1	2.0	6.0	28.0	9.6	6.4	3.6	9.7	28.6	47.2	47.2	SEE NOTE 3
E 151+42	E 151+98	RT.	8.0	SEE NOTE 1	2.0	6.0	56.0	24.0	29.8	16.6	45.3	134.0	160.0	104.0	SEE NOTE 3
E 152+49	E 153+75	RT.	8.0	SEE NOTE 1	2.0	6.0	126.0	4.2	12.9	7.2	19.6	58.0	260.4	260.4	SEE NOTE 3
E 152+49	E 153+07	RT.	8.0	SEE NOTE 1	2.0	6.0	58.0	4.2	5.9	3.3	8.9	26.2	66.4	66.4	SEE NOTE 3
								SUBTOTAL	**546.6**	**304.7**	**829.1**	**2,454.8**	**3,471.4**	**2,958.4**	
								TOTAL	**726.1**	**313.4**	**1,184.0**	**2,525.0**	**5,440.0**	**4,671.0**	

NOTES:

1. HMA TOP COURSE TO BE REMOVED DURING MILLING OPERATIONS AND TO BE PAID FOR UNDER ITEM R204.02, MILLING - 2" THICKNESS.
2. HMA TOP AND BINDER COURSES TO BE REMOVED DURING MILLING OPERATIONS AND TO BE PAID FOR UNDER ITEM R204.04, MILLING - 4" THICKNESS.
3. HMA PAVEMENT REPAIR LOCATIONS ARE TO BE APPROVED BY THE RESIDENT PROJECT REPRESENTATIVE AFTER MILLING OPERATIONS HAVE BEEN COMPLETED. THE R.P.R. SHALL VERIFY ALL PAVEMENT REPAIR AREAS AND THE TYPE OF REPAIR BEFORE THE CONTRACTOR BEGINS WORK.
4. IF THE CONTRACTOR ELECTS TO "DROP THE MILL HEAD" TO BEGIN PAVEMENT REPAIR, NO PAYMENT WILL BE MADE FOR ITEM 627.50140008, CUTTING PAVEMENT.

Lu Engineers
ENVIRONMENTAL • TRANSPORTATION • CIVIL
PROJECT MANAGER: J. OTTMAN
DRAWN BY: J. MESSENGER

Department of Environmental Services
Architecture and Engineering Services
City of Rochester, New York
ASSISTANT MANAGING ENGINEER LISA Y. REYES
CITY ENGINEER HOLLY E. BARRETT, P.E.

PROJECT TITLE
SCOTTSVILLE ROAD - ELMWOOD AVENUE IMPROVEMENT PROJECT (WEST CITY LINE - MT HOPE AVE)

NO.	REVISION	BY	DATE

ISSUED 03/06/2019
CHECKED JWO
DRAWN JRM
DESIGN JRM
SCALE NONE
PROJECT NUMBER 17102

DRAWING TITLE
MISCELLANEOUS TABLES MT-5

DRAWING NO. 29 OF 54

FILE NAME = Miscellanenous Tables.dgn
DATE/TIME = 12-MAR-2019 11:45
USER = JMessenger

SIDEWALK REPAIR

ITEM NO.	DESCRIPTION
203.02	UNCLASSIFIED EXCAVATION AND DISPOSAL
304.15	SUBBASE COURSE, OPTIONAL TYPE
S608.12	CONCRETE SIDEWALK AND DRIVEWAY
S608.15	ASPHALT DRIVEWAY - LIGHT DUTY

TO STATION	FROM STATION	SIDE	LENGTH (FT)	WIDTH (FT)	ITEM 203.02 (CY)	ITEM 304.15 (CY)	ITEM S608.12 (CY)	ITEM S608.15 (SF)	REMARKS
S 10+12.5	S 10+22.2	RT	9.7	4.6	1.6	0.9	0.7		SIDEWALK CRACKING
S 10+51.2	S 10+56.2	RT	5.0	3.6	0.7	0.4	0.3		SIDEWALK CRACKING
S 11+31.8	S 11+41.8	RT	10.0	7.0	2.4	1.3	1.1		SIDEWALK CRACKING
S 11+60.7	S 11+80.7	RT	20.0	6.8	4.7	2.6	2.2		SIDEWALK CRACKING
S 12+26.5	S 12+36.5	RT	10.0	6.6	2.3	1.3	1.1		TRANSVERSE CRACK
S 12+65.2	S 12+72.5	RT	7.3	6.8	1.7	1.0	0.8		SIDEWALK CRACKING
S 13+02.9	S 13+42.4	LT	39.5	6.0	8.1	4.4	3.7		LONGITUDINAL CRACK
S 13+75.4	S 14+03.5	RT	28.1	6.6	6.4	3.5	2.9		SIDEWALK CRACKING
S 14+97.5	S 15+02.5	LT	5.0	5.0	0.9	0.5	0.4		SIDEWALK HEAVED AT TREE
S 15+01.5	S 15+11.5	RT	10.0	6.6	2.3	1.3	1.1		TRIP HAZARD
S 15+30.9	S 15+45.9	LT	15.0	5.2	2.7	1.5	1.3		SIDEWALK HEAVED
S 15+75.0	S 16+26.9	LT	51.9	4.9	8.7	4.8	4.0	24.0	SIDEWALK HEAVED (SEE NOTE 1)
S 16+89.8	S 16+95.2	LT	5.4	3.7	0.7	0.4	0.4		TRANSVERSE CRACK AT VALVE
S 17+03.2	S 17+28.9	RT	25.7	5.8	5.1	2.8	2.3		SIDEWALK CRACKING
S 17+48.2	S 17+58.2	RT	10.0	5.7	2.0	1.1	0.9		SIDEWALK CRACKING
S 18+08.7	S 18+13.7	LT	5.0	5.8	1.0	0.6	0.5		TRIP HAZARD AT TREE
S 19+26.1	S 19+36.6	RT	10.5	6.2	2.2	1.2	1.0		SIDEWALK CRACKING
S 19+66.8	S 20+06.7	RT	39.9	6.8	9.2	5.0	4.2	24.0	UNEVEN SIDEWALK (SEE NOTE 1)
S 20+51.2	S 20+61.5	RT	10.3	6.3	2.2	1.2	1.0		SIDEWALK CRACKING
S 21+05.9	S 21+10.9	LT	5.0	3.5	0.6	0.4	0.3		TRIP HAZARD
S 21+18.2	S 21+25.4	RT	7.2	6.2	1.6	0.9	0.7		SIDEWALK CRACKING
S 21+39.8	S 21+59.8	RT	20.0	5.9	4.0	2.2	1.9		SIDEWALK CRACKING
S 21+48.7	S 21+68.7	LT	20.0	4.5	3.1	1.7	1.5		SIDEWALK HEAVED AT TREE
S 22+24.1	S 22+49.1	LT	25.0	5.1	4.3	2.4	2.0		SIDEWALK HEAVED AT TREE
S 22+82.0	S 23+12.2	LT	30.2	4.2	4.3	2.4	2.0		SIDEWALK HEAVED AND CRACKED
S 22+99.5	S 23+20.0	RT	20.5	4.8	3.4	1.9	1.6	30.0	SIDEWALK CRACKING (SEE NOTE 1)
S 23+61.8	S 23+71.6	RT	9.8	7.1	2.4	1.3	1.1		SIDEWALK CRACKING
S 24+25.2	S 24+40.3	RT	15.1	6.6	3.4	1.9	1.6		SIDEWALK CRACKING
S 26+29.5	S 26+34.5	LT	5.0	6.7	1.2	0.7	0.6		CRACK AT MANHOLE COVER
E 110+70.4	E 110+80.2	LT	9.8	VARIES	2.6	1.4	1.2		TRIP HAZARD
E 116+39.0	E 116+54.0	RT	15.0	5.2	2.7	1.5	1.3		CRACKED DEPRESSED SIDEWALK
E 117+65.0	E 117+70.0	RT	5.0	4.4	0.8	0.5	0.4		TRANSVERSE CRACK
E 119+32.8	E 119+37.1	RT	4.3	10.0	1.5				REMOVE "OLD" BUS STOP ACCESS
E 120+23.8	E 120+31.8	LT	8.0	14.0	3.9	2.1	1.8		REPLACE BUS STOP PAD
E 122+94.0	E 123+02.8	RT	8.8	VARIES	3.1	1.7	1.4		CRACK AT MANHOLE COVER
E 123+19.0	E 123+38.7	LT	19.7	8.2	5.6	3.1	2.6		SIDEWALK CRACKING
E 134+91.2	E 135+09.1	LT	DETERIORATED SIDEWALK TO BE REMOVED/REPLACED UNDER THE CITY OF ROCHESTER ELMWOOD AVENUE / COLLEGE TOWN CYCLE TRACK PROJECT(PIN 4760.80, D034682)						
E 135+92.4	E 136+02.4	LT	10.0	5.0	1.7	1.0	0.8		CRACKED SIDEWALK
E 138+70.6	E 138+84.5	RT	13.9	5.8	4.4	2.4	2.0		CRACKED SIDEWALK
E 139+45.6	E 139+60.6	LT	DETERIORATED SIDEWALK TO BE REMOVED/REPLACED UNDER THE CITY OF ROCHESTER ELMWOOD AVENUE / COLLEGE TOWN CYCLE TRACK PROJECT(PIN 4760.80, D034682)						
E 142+16.2	E 142+28.2	RT	12.0	6.1	2.6	1.4	1.2		CRACKED DEPRESSED SIDEWALK
E 143+90.3	E 144+14.8	LT	DETERIORATED SIDEWALK TO BE REMOVED/REPLACED UNDER THE CITY OF ROCHESTER ELMWOOD AVENUE / COLLEGE TOWN CYCLE TRACK PROJECT(PIN 4760.80, D034682)						
E 144+33.6	E 144+58.1	LT	DETERIORATED SIDEWALK TO BE REMOVED/REPLACED UNDER THE CITY OF ROCHESTER ELMWOOD AVENUE / COLLEGE TOWN CYCLE TRACK PROJECT(PIN 4760.80, D034682)						
E 147+32.9	E 147+73.9	RT	41.0	5.5	7.5	4.1	3.4		UNEVEN SIDEWALK, TRIP HAZARDS
E 147+71.5	E 147+81.3	LT	DETERIORATED SIDEWALK TO BE REMOVED/REPLACED UNDER THE CITY OF ROCHESTER ELMWOOD AVENUE / COLLEGE TOWN CYCLE TRACK PROJECT(PIN 4760.80, D034682)						
				SUBTOTAL	129.6	70.8	59.3	78.0	

NOTE:

1. REFER TO THE DRIVEWAY RESTORATION AT NEW SIDEWALK DETAILS ON DWG. MD-4 FOR LIMITS OF DRIVEWAY WORK.

SIDEWALK REPAIR

ITEM NO.	DESCRIPTION
203.02	UNCLASSIFIED EXCAVATION AND DISPOSAL
304.15	SUBBASE COURSE, OPTIONAL TYPE
S608.12	CONCRETE SIDEWALK AND DRIVEWAY
S608.15	ASPHALT DRIVEWAY - LIGHT DUTY

TO STATION	FROM STATION	SIDE	LENGTH (FT)	WIDTH (FT)	ITEM 203.02 (CY)	ITEM 304.15 (CY)	ITEM S608.12 (CY)	ITEM S608.15 (SF)	REMARKS
E 149+96.6	E 150+06.4	LT	DETERIORATED SIDEWALK TO BE REMOVED/REPLACED UNDER THE CITY OF ROCHESTER ELMWOOD AVENUE / COLLEGE TOWN CYCLE TRACK PROJECT(PIN 4760.80, D034682)						
E 150+26.5	E 150+51.0	LT	DETERIORATED SIDEWALK TO BE REMOVED/REPLACED UNDER THE CITY OF ROCHESTER ELMWOOD AVENUE / COLLEGE TOWN CYCLE TRACK PROJECT(PIN 4760.80, D034682)						
E 155+45.6	E 155+50.6	RT	5.0	5.9	1.1	0.6	0.5		CRACK AT MANHOLE COVER
E 157+04.3	E 157+09.3	RT	5.0	5.9	1.1	0.6	0.5		UNEVEN SIDEWALK
E 160+70.7	E 160+95.8	RT	DETERIORATED SIDEWALK TO BE REMOVED/REPLACED UNDER THE CITY OF ROCHESTER ELMWOOD AVENUE / COLLEGE TOWN CYCLE TRACK PROJECT(PIN 4760.80, D034682)						
E 161+61.1	E 161+91.1	RT	DETERIORATED SIDEWALK TO BE REMOVED/REPLACED UNDER THE CITY OF ROCHESTER ELMWOOD AVENUE / COLLEGE TOWN CYCLE TRACK PROJECT(PIN 4760.80, D034682)						
E 162+24.7	E 162+34.7	LT	10.0	5.2	1.9	1.1	0.9		TRIP HAZARD
E 162+32.5	E 1163+62.5	RT	DETERIORATED SIDEWALK TO BE REMOVED/REPLACED UNDER THE CITY OF ROCHESTER ELMWOOD AVENUE / COLLEGE TOWN CYCLE TRACK PROJECT(PIN 4760.80, D034682)						
E 169+21.7	E 169+31.7	LT	10.0	6.5	2.3	1.3	1.1		TRIP HAZARD
E 169+72.5	E 169+82.5	LT	10.0	6.5	2.3	1.3	1.1		TRIP HAZARD
				SUBTOTAL	8.7	4.9	4.1		
				TOTAL	138.3	75.7	63.4	78.0	

CURB REPAIR

ITEM NO.	DESCRIPTION
402.198903	19 F9 BINDER COURSE HMA, 80 SERIES COMPACTION
418.7603	ASPHALT PAVEMENT JOINT ADHESIVE
S609.3005	5" STONE CURB (INCLUDING EXCAVATION, BACKFILL AND PAVEMENT BASE RESTORATION)
S609.340101	RESET EXISTING STONE CURB (INCLUDING EXCAVATION, BACKFILL AND PAVEMENT BASE RESTORATION)

STATION	SIDE	LENGTH (FT)	WIDTH (FT)	ITEM 402.198903 (TON)	ITEM 418.7603 (FT)	ITEM S609.3005 (FT)	ITEM S609.340101 (FT)	REMARKS
S 12+31.4	RT	6.0	1.0	0.1	8.0	6.0		REPLACE BROKEN CURB
S 12+70.6	LT							BROKEN CURB TO BE REPLACED WITH CR-3
S 13+07.0	RT	6.0	1.0	0.1	8.0	6.0		REPLACE BROKEN / MISSING CURB
S 13+58.4	RT	6.0	1.0	0.1	8.0		6.0	RESET SUNK CURB AT FIRE HYDRANT
S 15+97.9	RT							BROKEN CURB TO BE REPLACED WITH CR-6
S 16+17.0	LT	6.0	1.0	0.1	8.0		6.0	RESET OFFSET CURB
S 17+48.5	RT	6.0	1.0	0.1	8.0	6.0		REPLACE BROKEN / MISSING CURB
S 18+62.6	LT	6.0	1.0	0.1	8.0	6.0		REPLACE BROKEN CURB
S 18+85.9	LT							BROKEN CURB TO BE REPLACED WITH CR-9
S 20+02.2	LT	6.0	1.0	0.1	8.0		6.0	RESET OFFSET CURB
S 20+02.2	LT	6.0	1.0	0.1	8.0	6.0		REPLACE BROKEN CURB
S 23+81.5	LT	12.0	1.0	0.2	14.0		12.0	RESET SUNK CURB AT DRAINAGE STRUCTURE
E 110+77.6	LT	6.0	1.0	0.1	8.0	6.0		REPLACE BROKEN / MISSING CURB
E 110+85.7	LT	6.0	1.0	0.1	8.0	6.0		REPLACE BROKEN / MISSING CURB
E 111+14.8	LT	6.0	1.0	0.1	8.0	6.0		REPLACE BROKEN CURB
E 120+55.0	RT	6.0	1.0	0.1	8.0	6.0		REPLACE BROKEN / MISSING CURB
E 122+13.0	LT							BROKEN CURB TO BE REPLACED WITH CR-23
E 122+89.6	LT							BROKEN CURB TO BE REPLACED WITH CR-26
E 139+49.4	LT	6.0	1.0	0.1	8.0		6.0	RESET SUNK CURB
E 139+49.4	LT	6.0	1.0	0.1	8.0	6.0		REPLACE BROKEN CURB
E 142+63.7	RT	12.0	1.0	0.2	14.0		12.0	REST SUNK CURB
E 143+79.9	LT	6.0	1.0	0.1	8.0	6.0		REPLACE BROKEN CURB
E 142+96.2	LT	6.0	1.0	0.1	8.0	6.0		REPLACE BROKEN CURB
E 160+48.3	LT	6.0	1.0	0.1	8.0	6.0		REPLACE BROKEN CURB
E 170+46.1	LT	6.0	1.0	0.1	8.0	6.0		REPLACE BROKEN / MISSING CURB
			TOTAL	2.2	172.0	84.0	48.0	

FILE NAME = Miscellaneous Tables.dgn
DATE/TIME = 12-MAR-2019 11:46
USER = JMessenger

Lu Engineers
ENVIRONMENTAL • TRANSPORTATION • CIVIL

PROJECT MANAGER: J. OTTMAN
DRAWN BY: J. MESSENGER

Department of Environmental Services
Architecture and Engineering Services
City of Rochester, New York

ASSISTANT MANAGING ENGINEER: LISA Y. REYES
CITY ENGINEER: HOLLY E. BARRETT, P.E.

PROJECT TITLE
SCOTTSVILLE ROAD - ELMWOOD AVENUE IMPROVEMENT PROJECT
(WEST CITY LINE - MT HOPE AVE)

NO.	REVISION	BY	DATE

ISSUED 03/06/2019
CHECKED JWO
DRAWN JRM
DESIGN JRM
SCALE NONE
PROJECT NUMBER 17102

DRAWING TITLE
MISCELLANEOUS TABLES MT-6

DRAWING NO. 30 OF 54

ALTER EXISTING MANHOLES

ITEM NO.	DESCRIPTION
S604.60	ADJUST EXISTING SEWER MANHOLE FRAME AND COVER (INCLUDING EXCAVATION AND BACKFILL)
S604.65	CLEAN EXISTING MANHOLE
S604.70	SEWER MANHOLE PRECAST CONCRETE FLAT TOP SLAB, 48" DIAMETER
R655.03	MANHOLE FRAME AND COVER - CASTING

STATION	SIDE	UTILITY	S604.60 (EACH)	S604.65 (EACH)	S604.70 (EACH)	R655.03 (EACH)	REMARKS
S 10+05	LT	FIBER					TO BE ADJUSTED BY OTHERS
S 10+47	RT	RG&E					NO ADJUSTMENTS NEEDED
S 11+00	LT	MCPW	1				ADJUST MANHOLE FRAME AND COVER
S 11+20	LT	RG&E					REPLACE MANHOLE FRAME AND COVER BY OTHERS
S 13+83	LT	MCPW	1				ADJUST MANHOLE FRAME AND COVER
S 14+28	RT	RG&E					TO BE ADJUSTED BY OTHERS
S 14+77	CTR	MCPW	1				ADJUST MANHOLE FRAME AND COVER
S 15+80	CTR	MCPW	1				ADJUST MANHOLE FRAME AND COVER
S 16+87	LT	MCPW	1				ADJUST MANHOLE FRAME AND COVER
S 17+21	LT	RG&E					TO BE ADJUSTED BY OTHERS
S 18+45	RT	RG&E					TO BE ADJUSTED BY OTHERS
S 18+75	CTR	MCPW	1				ADJUST MANHOLE FRAME AND COVER
S 19+90	LT	MCPW	1				ADJUST MANHOLE FRAME AND COVER
S 22+30	CTR	MCPW	1				ADJUST MANHOLE FRAME AND COVER
S 22+89	LT	MCPW	1				ADJUST MANHOLE FRAME AND COVER
S 23+25	LT	TEL					TO BE ADJUSTED BY OTHERS
S 25+75	LT	N/A					TO BE DETERMINED IN FIELD
S 25+99	CTR	MCPW	1				ADJUST MANHOLE FRAME AND COVER
S 26+50	LT	MCPW		1		1	REPLACE MANHOLE FRAME AND COVER, RECORBEL BRICK INTERIOR
S 27+30	RT	RG&E					TO BE ADJUSTED BY OTHERS
E 111+30	LT	RG&E					NO ADJUSTMENTS NEEDED
E 115+53	LT	RG&E					NO ADJUSTMENTS NEEDED
E 119+45	LT	RG&E					NO ADJUSTMENTS NEEDED
E 120+96	LT	RG&E					REPLACE MANHOLE FRAME AND COVER BY OTHERS
E 122+00	LT	RG&E					NO ADJUSTMENTS NEEDED
E 122+45	LT	RG&E					REPLACE MANHOLE FRAME AND COVER BY OTHERS
E 122+88	LT	RG&E					NO ADJUSTMENTS NEEDED
E 125+95	CTR	RG&E					TO BE ADJUSTED BY OTHERS
E 126+05	LT	RG&E					NO ADJUSTMENTS NEEDED
E 131+08	RT	MCPW		1		1	REPLACE MANHOLE FRAME AND COVER
E 131+25	LT	RG&E					NO ADJUSTMENTS NEEDED
E 131+25	CTR	RG&E					NO ADJUSTMENTS NEEDED
E 131+46	RT	FIBER		1		1	REPLACE MANHOLE FRAME AND COVER
E 131+51	LT	RG&E					NO ADJUSTMENTS NEEDED
E 131+68	RT	MCPW		1		1	REPLACE MANHOLE FRAME AND COVER
E 131+70	RT	MCPW		1		1	REPLACE MANHOLE FRAME AND COVER
E 131+93	RT	MCPW		1		1	REPLACE MANHOLE FRAME AND COVER
E 132+40	LT	RG&E					REPLACE MANHOLE FRAME AND COVER BY OTHERS
E 134+12	LT	RG&E					REPLACE MANHOLE FRAME AND COVER BY OTHERS
E 135+75	RT	MCPW		1	1	1	REPLACE MANHOLE FRAME AND COVER, AND PLACE NEW TOP SLAB
E 135+75	RT	MCPW					NO ADJUSTMENTS NEEDED
E 137+10	LT	RG&E					REPLACE MANHOLE FRAME AND COVER BY OTHERS
E 138+33	RT	TEL		1		1	TO BE ADJUSTED BY OTHERS
E 143+07	CTR	MCPW	1				ADJUST MANHOLE FRAME AND COVER
E 143+18	CTR	MCPW		1		1	REPLACE MANHOLE FRAME AND COVER
E 154+10	RT	MCPW		1		1	REPLACE MANHOLE FRAME AND COVER
E 159+60	CTR	RG&E					NO ADJUSTMENTS NEEDED
G 10+45	CTR	MCPW		1		1	REPLACE MANHOLE FRAME AND COVER
		TOTAL	11	11	1	11	

ALTER EXISTING DRAINAGE STRUCTURES

ITEM NO.	DESCRIPTION
S604.5004	ADJUST EXISTING CATCH BASIN FRAME AND GRATE (INCLUDING EXCAVATION, BACKFILL AND PAVEMENT BASE RESTORATION)
S604.53	CLEAN EXISTING CATCH BASIN AND LATERAL PIPE
R655.06	TYPE A CATCH BASIN FRAME AND GRATE - FABRICATED
R655.07	TYPE B CATCH BASIN FRAME AND GRATE - FABRICATED

STATION	SIDE	S604.5004 (EACH)	S604.53 (EACH)	R655.06 (EACH)	R655.07 (EACH)
S 9+98.1	LT	1			
S 10+18.2	RT	1			
S 11+35.7	RT	1			
S 11+88.1	LT	1			
S 12+38.0	LT	1			
S 12+77.2	LT		1	1	
S 12+77.9	RT		1	1	
S 13+19.1	RT	1			
S 14+36.2	RT	1			
S 14+63.9	LT	1			
S 16+11.6	LT	1			
S 16+15.0	RT		1	1	
S 17+53.0	RT		1	1	
S 18+19.6	LT		1	1	
S 19+13.3	LT		1	1	
S 19+23.6	RT		1	1	
S 20+76.2	LT		1	1	
S 20+93.7	RT		1	1	
S 22+13.5	LT		1		1
S 23+16.4	RT		1	1	
S 23+81.1	LT		1	1	
S 25+18.7	LT	1			
S 25+33.6	RT	1			
S 26+34.4	LT	1			
E 112+42.2	RT	1			
E 112+46.5	LT	1			
E 114+98.5	LT	1			
E 114+99.1	RT	1			
E 116+67.3	LT	1			
E 116+67.0	RT	1			
E 118+38.7	LT	1			
E 118+38.5	RT	1			
E 120+08.1	LT	1			
E 120+07.5	RT	1			
E 121+71.8	LT	1			
E 122+43.3	RT	1			
E 122+99.7	LT	1			
E 123+03.3	RT	1			
E 124+01.2	LT	1		1	
E 131+73.1	RT	1			
SUBTOTAL		28	12	12	1

ALTER EXISTING DRAINAGE STRUCTURES

ITEM NO.	DESCRIPTION
S604.5004	ADJUST EXISTING CATCH BASIN FRAME AND GRATE (INCLUDING EXCAVATION, BACKFILL AND PAVEMENT BASE RESTORATION)
S604.53	CLEAN EXISTING CATCH BASIN AND LATERAL PIPE
R655.06	TYPE A CATCH BASIN FRAME AND GRATE - FABRICATED
R655.07	TYPE B CATCH BASIN FRAME AND GRATE - FABRICATED

STATION	SIDE	S604.5004 (EACH)	S604.53 (EACH)	R655.06 (EACH)	R655.07 (EACH)
E 133+71.3	RT		1		1
E 133+75.5	LT	1			
E 135+74.6	RT	1			
E 135+77.4	LT	1			
E 135+80.0	RT	1			
E 135+83.4	LT	1			
E 137+75.6	RT	1			
E 138+12.9	LT	1			
E 139+21.0	RT	1			
E 140+07.7	LT	1			
E 141+21.0	RT		1		1
E 142+02.8	LT	1			
E 143+70.6	CTR		1		1
E 143+76.7	RT	1			
E 145+09.8	RT	1			
E 145+66.4	CTR	1			
E 146+43.4	RT	1			
E 149+50.4	RT	1			
E 149+51.6	RT	1			
E 150+55.5	LT	1			
E 150+85.1	RT	1			
E 151+80.5	LT	1			
E 152+02.0	RT	1			
E 153+71.2	LT	1			
E 153+85.0	RT	1			
E 155+29.7	LT	1			
E 155+60.6	RT	1			
E 157+30.3	LT	1			
E 157+81.7	RT	1			
E 158+92.2	LT	1			
E 158+92.4	RT	1			
E 160+56.6	LT	1			
E 160+60.8	RT	1			
E 162+18.8	LT	1			
E 163+03.2	RT	1			
E 165+12.4	RT	1			
E 165+41.0	LT	1			
E 168+45.5	LT	1			
E 169+22.1	RT	1			
E 170+55.0	LT	1			
E 170+73.4	LT	1			
SUBTOTAL		38	3		3
TOTAL		66	15	12	4

ALTER DRAINAGE STRUCTURE AND MANHOLE TABLE NOTES:

1. ADJUST DRAINAGE STRUCTURE FRAME AND GRATE PRIOR TO PLACEMENT OF HMA TOP COURSE.
2. ADJUST MANHOLE FRAME AND COVER AFTER PLACEMENT OF HMA TOP COURSE.
3. ALL MANHOLE FRAMES SHALL BE 9". REFER TO MANHOLE FRAME DETAIL AND MCPW FIGURE 2.04 ON DWG. MD-7.
4. NEW COVERS SHALL BE STAMPED WITH THE INDENTIFICATION "SEWER & MCPW". REFER TO MANHOLE COVER DETAIL ON DWG. MD-7.

Lu Engineers
ENVIRONMENTAL • TRANSPORTATION • CIVIL
PROJECT MANAGER: J. OTTMAN
DRAWN BY: J. MESSENGER

Department of Environmental Services
Architecture and Engineering Services
City of Rochester, New York
ASSISTANT MANAGING ENGINEER: LISA Y. REYES
CITY ENGINEER: HOLLY E. BARRETT, P.E.

PROJECT TITLE
SCOTTSVILLE ROAD - ELMWOOD AVENUE IMPROVEMENT PROJECT
(WEST CITY LINE - MT HOPE AVE)

NO.	REVISION	BY	DATE

ISSUED 03/06/2019
CHECKED JWO
DRAWN JRM
DESIGN JRM
SCALE NONE
PROJECT NUMBER 17102

DRAWING TITLE
MISCELLANEOUS TABLES MT-7

DRAWING NO. 31 OF 54

FILE NAME = Miscellanenous Tables.dgn
DATE/TIME = 12-MAR-2019 11:54
USER = JMessenger

WATER SERVICES

ITEM NO.	DESCRIPTION
S912.030100	NEW 1" WATER SERVICE TAP AT WATER MAIN, CORPORATION STOP AND CONNECTION (INCLUDING EXCAVATION, BACKFILL AND SURFACE RESTORATION)
S913.130100	NEW 1" POLYETHYLENE OR CROSS-LINKED POLYETHYLENE WATER SERVICE (INCLUDING EXCAVATION, BACKFILL AND SURFACE RESTORATION)
S914.030100	FURNISH AND INSTALL NEW 1" CURB STOP AND BOX AT NEW WATER SERVICE (INCLUDING EXCAVATION, BACKFILL AND SURFACE RESTORATION)
S966.0332	32 POUND MAGNESIUM ANODE

HOUSE NUMBER	SIDE	EXISTING SERVICE			S912.030100 (EACH)	S913.130100 (FT)	S914.030100 (EACH)	S966.0332 (EACH)	REMARKS
		SIZE	MATERIAL	STATUS					
230 SCOTTSVILLE RD.	LT	5/8"	GALVANIZED	ACTIVE	1	38.0	1	1	SEE NOTE 1
202 - 204 SCOTTSVILLE RD.	LT	5/8"	GALVANIZED	ACTIVE	1	36.0	1	1	SEE NOTE 1
194 SCOTTSVILLE RD.	LT	5/8"	GALVANIZED	ACTIVE	1	36.0	1	1	SEE NOTE 1
191 SCOTTSVILLE RD.	RT	5/8"	LEAD	ACTIVE	1	16.0	1	1	SEE NOTE 1
188 SCOTTSVILLE RD.	LT	5/8"	GALVANIZED	ACTIVE	1	36.0	1	1	SEE NOTE 1
185 SCOTTSVILLE RD.	RT	5/8"	LEAD	ACTIVE	1	16.0	1	1	SEE NOTE 1
182 - 184 SCOTTSVILLE RD.	LT	5/8"	GALVANIZED	ACTIVE	1	35.0	1	1	SEE NOTE 1
178 SCOTTSVILLE RD.	LT	5/8"	GALVANIZED	ACTIVE	1	36.0	1	1	SEE NOTE 1
172 SCOTTSVILLE RD.	LT	5/8"	GALVANIZED	ACTIVE	1	38.5	1	1	SEE NOTE 1
171 SCOTTSVILLE RD.	RT	5/8"	GALVANIZED	ACTIVE	1	8.0	1	1	SEE NOTE 1
166 SCOTTSVILLE RD.	LT	5/8"	GALVANIZED	ACTIVE	1	36.0	1	1	SEE NOTE 1
160 SCOTTSVILLE RD.	LT	5/8"	GALVANIZED	ACTIVE	1	35.0	1	1	SEE NOTE 1
157 SCOTTSVILLE RD.	RT	5/8"	LEAD	ACTIVE	1	16.0	1	1	SEE NOTE 1
151 SCOTTSVILLE RD.	RT	5/8"	LEAD	ACTIVE	1	16.0	1	1	SEE NOTE 1
147 SCOTTSVILLE RD.	RT	5/8"	LEAD	ACTIVE	1	16.0	1	1	SEE NOTE 1
141 SCOTTSVILLE RD.	RT	5/8"	LEAD	ACTIVE	1	16.0	1	1	SEE NOTE 1
135 SCOTTSVILLE RD.	RT	5/8"	LEAD	ACTIVE	1	16.0	1	1	SEE NOTE 1
123 SCOTTSVILLE RD.	RT	5/8"	GALVANIZED	ACTIVE	1	16.0	1	1	SEE NOTE 1
119 SCOTTSVILLE RD.	RT	5/8"	LEAD	ACTIVE	1	16.0	1	1	SEE NOTE 1
113 SCOTTSVILLE RD.	RT	5/8"	LEAD	ACTIVE	1	16.0	1	1	SEE NOTE 1
112 SCOTTSVILLE RD.	LT	5/8"	LEAD	ACTIVE	1	37.0	1	1	SEE NOTE 1
107 SCOTTSVILLE RD.	RT	5/8"	GALVANIZED	ACTIVE	1	16.0	1	1	SEE NOTE 1
106 SCOTTSVILLE RD.	LT	5/8"	LEAD	ACTIVE	1	36.0	1	1	SEE NOTE 1
103 SCOTTSVILLE RD.	RT	5/8"	LEAD	ACTIVE	1	36.0	1	1	SEE NOTE 1
97 SCOTTSVILLE RD.	RT	5/8"	LEAD	ACTIVE	1	16.0	1	1	SEE NOTE 1
96 SCOTTSVILLE RD.	LT	5/8"	LEAD	ACTIVE	1	36.0	1	1	SEE NOTE 1
93 SCOTTSVILLE RD.	RT	5/8"	LEAD	ACTIVE	1	16.0	1	1	SEE NOTE 1
90 SCOTTSVILLE RD.	LT	5/8"	LEAD	ACTIVE	1	38.0	1	1	SEE NOTE 1
84 SCOTTSVILLE RD.	LT	5/8"	GALVANIZED	ACTIVE	1	39.0	1	1	SEE NOTE 1
83 SCOTTSVILLE RD.	RT	5/8"	GALVANIZED	ACTIVE	1	16.0	1	1	SEE NOTE 1
79 SCOTTSVILLE RD.	RT	5/8"	GALVANIZED	ACTIVE	1	16.0	1	1	SEE NOTE 1
69 SCOTTSVILLE RD.	RT	5/8"	LEAD	ACTIVE	1	16.0	1	1	SEE NOTE 1
59 SCOTTSVILLE RD.	RT	5/8"	GALVANIZED	ACTIVE	1	16.0	1	1	SEE NOTE 1
53 - 55 SCOTTSVILLE RD.	RT	5/8"	LEAD	ACTIVE	1	16.0	1	1	SEE NOTE 1
31 SCOTTSVILLE RD.	RT	5/8"	LEAD	ACTIVE	1	9.0	1	1	SEE NOTE 1
30 SCOTTSVILLE RD.	LT	5/8"	GALVANIZED	ACTIVE	1	35.0	1	1	SEE NOTE 1
2 SCOTTSVILLE RD.	LT	5/8"	LEAD	ACTIVE	1	35.0	1	1	SEE NOTE 1
				TOTAL	37	923.5	37	37	

WATER SERVICE TABLE NOTES:

1. REMOVE EXISTING CURB STOP AND BOX AND PROVIDE NEW WATER SERVICE TAP, WATER SERVICE, CURB STOP AND BOX.
2. PRIOR TO COMMENCING WORK, CONTRACTOR TO VERIFY LOCATION AND DISPOSITION OF ALL WATER SERVICE WORK.
3. TRACER WIRE IS REQUIRED ON ALL BURIED NON-METALLIC WATER SERVICES.
4. CONTRACTOR TO PROVIDE WATER SERVICE LOCATION CARD FOR ALL SERVICES.

ADJUST EXISTING VALVE BOX

ITEM NO.	DESCRIPTION
S909.09	ADJUST EXISTING WATER VALVE BOX TO GRADE - EXTENSION ADJUSTMENT (INCLUDING EXCAVATION AND BACKFILL AND SURFACE RESTORATION)

STATION	SIDE	TYPE	S909.09 (EA)	REMARKS
S 10+79	LT	WATER	1	ADJUST EXISTING WATER VALVE
S 11+30	RT	WATER	1	ADJUST EXISTING WATER VALVE
S 11+81	RT	WATER	1	ADJUST EXISTING WATER VALVE
S 12+17	RT	WATER	1	ADJUST EXISTING WATER VALVE
S 12+27	LT	WATER	1	ADJUST EXISTING WATER VALVE
S 12+33	LT	UNKOWN		ADJUST EXISTING UNKOWN VALVE
S 12+62	RT	WATER	1	ADJUST EXISTING WATER VALVE
S 13+59	RT	WATER	1	ADJUST EXISTING WATER VALVE
S 15+76	RT	WATER	1	ADJUST EXISTING WATER VALVE
S 18+81	RT	WATER	1	ADJUST EXISTING WATER VALVE
S 18+99	RT	WATER	1	ADJUST EXISTING WATER VALVE
S 21+77	RT	WATER	1	ADJUST EXISTING WATER VALVE
S 25+37	RT	WATER	1	ADJUST EXISTING WATER VALVE
S 26+15	RT	WATER	1	ADJUST EXISTING WATER VALVE
S 27+45	LT	UNKNOWN		OUTSIDE OF PAVING LIMITS
S 27+50	LT	WATER		OUTSIDE OF PAVING LIMITS
G 11+11	LT	WATER		OUTSIDE OF PAVING LIMITS
G 11+68	RT	WATER		OUTSIDE OF PAVING LIMITS
G 11+76	RT	GAS		OUTSIDE OF PAVING LIMITS
G 11+83	CTR	WATER		OUTSIDE OF PAVING LIMITS
G 12+41	RT	WATER		OUTSIDE OF PAVING LIMITS
E 112+14	LT	WATER	1	ADJUST EXISTING WATER VALVE
E 115+20	LT	WATER	1	ADJUST EXISTING WATER VALVE
E 118+36	LT	WATER	1	ADJUST EXISTING WATER VALVE
E 120+72	LT	WATER	1	ADJUST EXISTING WATER VALVE
E 120+93	LT	WATER	1	ADJUST EXISTING WATER VALVE
E 125+86	RT	GAS		ADJUST EXISTING GAS VALVE BY OTHERS
E 125+90	RT	GAS		ADJUST EXISTING GAS VALVE BY OTHERS
E 125+90	RT	GAS		ADJUST EXISTING GAS VALVE BY OTHERS
E 125+92	RT	GAS		ADJUST EXISTING GAS VALVE BY OTHERS
		SUBTOTAL	18	

ADJUST EXISTING VALVE BOX

ITEM NO.	DESCRIPTION
S909.09	ADJUST EXISTING WATER VALVE BOX TO GRADE - EXTENSION ADJUSTMENT (INCLUDING EXCAVATION AND BACKFILL AND SURFACE RESTORATION)

STATION	SIDE	TYPE	S909.09 (EA)	REMARKS
E 131+38	RT	GAS		ADJUST EXISTING GAS VALVE BY OTHERS
E 131+42	RT	GAS		ADJUST EXISTING GAS VALVE BY OTHERS
E 131+44	RT	GAS		ADJUST EXISTING GAS VALVE BY OTHERS
E 134+92	LT	WATER	1	ADJUST EXISTING WATER VALVE
E 134+92	LT	WATER	1	ADJUST EXISTING WATER VALVE
E 135+00	LT	WATER	1	ADJUST EXISTING WATER VALVE
E 135+08	LT	WATER	1	ADJUST EXISTING WATER VALVE
E 137+92	RT	WATER	1	ADJUST EXISTING WATER VALVE
E 140+10	LT	WATER	1	ADJUST EXISTING WATER VALVE
E 141+57	LT	WATER	1	ADJUST EXISTING WATER VALVE
E 143+79	LT	WATER	1	ADJUST EXISTING WATER VALVE
E 144+23	LT	WATER	1	ADJUST EXISTING WATER VALVE
E 144+30	LT	WATER	1	ADJUST EXISTING WATER VALVE
E 144+47	LT	WATER	1	ADJUST EXISTING WATER VALVE
E 144+58	LT	WATER	1	ADJUST EXISTING WATER VALVE
E 144+90	LT	WATER	1	ADJUST EXISTING WATER VALVE
E 147+13	LT	WATER	1	ADJUST EXISTING WATER VALVE
E 148+30	LT	WATER	1	ADJUST EXISTING WATER VALVE
E 156+13	LT	WATER	1	ADJUST EXISTING WATER VALVE
E 157+43	LT	WATER	1	ADJUST EXISTING WATER VALVE
E 157+89	LT	WATER	1	ADJUST EXISTING WATER VALVE
E 157+89	RT	WATER	1	ADJUST EXISTING WATER VALVE
E 157+92	LT	WATER	1	ADJUST EXISTING WATER VALVE
E 157+92	RT	WATER	1	ADJUST EXISTING WATER VALVE
E 169+53	LT	WATER		OUTSIDE OF PAVING LIMITS
E 164+40	LT	WATER		OUTSIDE OF PAVING LIMITS
E 166+68	LT	WATER		OUTSIDE OF PAVING LIMITS
E 170+88	LT	WATER		OUTSIDE OF PAVING LIMITS
		SUBTOTAL	21	
		TOTAL	39	

FILE NAME = Miscellaneous Tables.dgn
DATE/TIME = 06-MAR-2019 11:47
USER = JMessenger

Lu Engineers
ENVIRONMENTAL • TRANSPORTATION • CIVIL
PROJECT MANAGER: J. OTTMAN
DRAWN BY: J. MESSENGER

Department of Environmental Services
Architecture and Engineering Services
City of Rochester, New York
ASSISTANT MANAGING ENGINEER LISA Y. REYES
CITY ENGINEER HOLLY E. BARRETT, P.E.

PROJECT TITLE
SCOTTSVILLE ROAD - ELMWOOD AVENUE
IMPROVEMENT PROJECT
(WEST CITY LINE - MT HOPE AVE)

NO.	REVISION	BY	DATE

ISSUED 03/06/2019
CHECKED JWO
DRAWN JRM
DESIGN JRM
SCALE NONE
PROJECT NUMBER 17102

DRAWING TITLE
MISCELLANEOUS TABLES MT-8

DRAWING NO. 32 OF 54

SNOWPLOWING MARKERS

ITEM NO.	DESCRIPTION
646.40	FLEXIBLE DELINEATOR, SINGLE UNIT, ONE WAY
646.42	FLEXIBLE DELINEATOR, DOUBLE UNIT

STATION	SIDE	COLOR	MOUNTING SURFACE	646.40 (EACH)	646.42 (EACH)	REMARKS
G 10+52	RT	WHITE	CONCRETE		1	RT. SIDE OF TRAVELWAY
G 10+70	RT	YELLOW	CONCRETE	1		LT. SIDE OF TRAVELWAY
G 10+70	RT	YELLOW	CONCRETE		1	LT. SIDE OF TRAVELWAY
G 10+85	RT	WHITE	CONCRETE	1		RT. SIDE OF TRAVELWAY
G 11+70	RT	YELLOW	CONCRETE		1	LT. SIDE OF TRAVELWAY
G 11+70	RT	YELLOW	CONCRETE	1		LT. SIDE OF TRAVELWAY
E 110+45	LT	WHITE	CONCRETE	1		RT. SIDE OF TRAVELWAY
E 110+54	LT	YELLOW	CONCRETE	1		LT. SIDE OF TRAVELWAY
E 110+80	LT	WHITE	CONCRETE		1	RT. SIDE OF TRAVELWAY
E 110+82	LT	YELLOW	CONCRETE		1	LT. SIDE OF TRAVELWAY
E 132+75	RT	YELLOW	CONCRETE	1		LT. SIDE OF TRAVELWAY
E 132+75	RT	YELLOW	CONCRETE		1	LT. SIDE OF TRAVELWAY
E 134+48	RT	YELLOW	CONCRETE	1		LT. SIDE OF TRAVELWAY
E 134+48	RT	YELLOW	CONCRETE	1		LT. SIDE OF TRAVELWAY
E 135+00	RT	YELLOW	CONCRETE	1		LT. SIDE OF TRAVELWAY
E 135+00	RT	YELLOW	CONCRETE	1		LT. SIDE OF TRAVELWAY
E 137+00	LT	YELLOW	CONCRETE	1		LT. SIDE OF TRAVELWAY
E 137+00	RT	YELLOW	CONCRETE	1		LT. SIDE OF TRAVELWAY
E 137+67	RT	YELLOW	CONCRETE		1	LT. SIDE OF TRAVELWAY
E 137+67	RT	YELLOW	CONCRETE	1		LT. SIDE OF TRAVELWAY
E 137+70	RT	WHITE	CONCRETE		1	RT. SIDE OF TRAVELWAY
E 137+70	RT	YELLOW	CONCRETE		1	LT. SIDE OF TRAVELWAY
E 137+85	RT	WHITE	CONCRETE	1		RT. SIDE OF TRAVELWAY
E 137+88	RT	WHITE	CONCRETE		1	RT. SIDE OF TRAVELWAY
E 137+91	RT	YELLOW	CONCRETE	1		LT. SIDE OF TRAVELWAY
E 137+94	RT	WHITE	CONCRETE	1		RT. SIDE OF TRAVELWAY
E 138+12	RT	YELLOW	CONCRETE		1	LT. SIDE OF TRAVELWAY
E 138+14	RT	YELLOW	CONCRETE	1		LT. SIDE OF TRAVELWAY
E 138+18	RT	YELLOW	CONCRETE	1		LT. SIDE OF TRAVELWAY
E 138+24	RT	YELLOW	CONCRETE		1	LT. SIDE OF TRAVELWAY
E 138+40	RT	WHITE	CONCRETE	1		RT. SIDE OF TRAVELWAY
E 138+42	RT	WHITE	CONCRETE		1	RT. SIDE OF TRAVELWAY
E 138+43	RT	WHITE	CONCRETE		1	RT. SIDE OF TRAVELWAY
E 138+46	RT	YELLOW	CONCRETE		1	LT. SIDE OF TRAVELWAY
E 138+56	RT	YELLOW	CONCRETE	1		LT. SIDE OF TRAVELWAY
E 138+56	RT	YELLOW	CONCRETE		1	LT. SIDE OF TRAVELWAY
E 138+59	RT	WHITE	CONCRETE	1		RT. SIDE OF TRAVELWAY
E 138+60	RT	YELLOW	CONCRETE	1		LT. SIDE OF TRAVELWAY
E 139+10	RT	YELLOW	CONCRETE	1		LT. SIDE OF TRAVELWAY
E 139+10	RT	YELLOW	CONCRETE	1		LT. SIDE OF TRAVELWAY
E 139+60	RT	YELLOW	CONCRETE	1		LT. SIDE OF TRAVELWAY
E 139+60	RT	YELLOW	CONCRETE	1		LT. SIDE OF TRAVELWAY
E 140+63	LT	YELLOW	SOIL	1		LT. SIDE OF TRAVELWAY
E 142+52	LT	YELLOW	SOIL		1	LT. SIDE OF TRAVELWAY
E 142+52	LT	YELLOW	SOIL	1		LT. SIDE OF TRAVELWAY
E 143+32	RT	YELLOW	CONCRETE	1		LT. SIDE OF TRAVELWAY
E 143+32	RT	YELLOW	CONCRETE		1	LT. SIDE OF TRAVELWAY
E 144+50	RT	YELLOW	SOIL	1		LT. SIDE OF TRAVELWAY
E 145+11	LT	YELLOW	SOIL	1		LT. SIDE OF TRAVELWAY
E 145+67	LT	YELLOW	SOIL	1		LT. SIDE OF TRAVELWAY
			SUBTOTAL	32	18	

CITY OF ROCHESTER SURVEY MONUMENTS

ITEM NO.	DESCRIPTION
S626.07	RESET EXISTING SURVEY MONUMENT FRAME AND COVER
S626.10	REPLACE EXISTING HORIZONTAL CONTROL MONUMENT WITH NEW HORIZONTAL CONTROL MONUMENT (RCS)

STATION	OFFSET	SIDE	MONUMENT NUMBER	CONTROL NUMBER	TYPE	LOCATION	S626.07 (EACH)	S626.10 (EACH)	REMARKS
						SCOTTSVILLE ROAD			
S 12+55	28'	LT	135570105	5777	RCS	N.W. SCOTTSVILLE ROAD AND KINGSBORO ROAD	1		PROTECT MONUMENT
S 16+05	28'	RT	135570203	5782	RCS	N.E. SCOTTSVILLE ROAD AND VIXETTE ROAD		1	REPLACE MONUMENT, IF NEEDED
S 16+99	28'	LT	135570104	5776	RCS	NORTH SIDE SCOTTSVILLE ROAD 110'+/- EAST OF VIXELLE STREET		1	REPLACE MONUMENT,IF IMPACTED
S 18+95	28'	RT	135570202	5781	RCS	N.E. SCOTTSVILLE ROAD AND MINEOLA STREET		1	REPLACE MONUMENT,IF IMPACTED
S 26+25	28'	LT	135500111	5763	RCS	NORTH SIDE SCOTTSVILLE ROAD 140'+/- EAST OF WINBOURNE ROAD			PROTECT MONUMENT
S 25+60	28'	RT	135500113	5764	RCS	SOUTH SIDE SCOTTSVILLE ROAD 80'+/- EAST OF WINBOURNE ROAD			PROTECT MONUMENT
S 27+45	34'	RT	135500110	5762	RCS	S.W. SCOTTSVILLE ROAD AND GENESEE STREET			PROTECT MONUMENT
						ELMWOOD AVENUE			
S 28+45	43'	RT	135500208	5767	RCS	S.E. SCOTTSVILLE ROAD AND GENESEE STREET			PROTECT MONUMENT
E 110+30	40'	RT	135500209	5768	RCS	SOUTH SIDE ELMWOOD AVENUE EAST OF GENESEE STREET			PROTECT MONUMENT
E 117+40	46'	RT	135500205	759	USC&GS	SOUTH SIDE ELMWOOD AVENUE			USC&GS MONUMENT "LOOP", PROTECT
E 131+80	39'	RT	135590106	5791	RCS	S.W. ELMWOOD AVENUE AND MOORE ROAD	1		PROTECT MONUMENT
E 131+85	43'	RT	135590107	5792	RCS	S.W. ELMWOOD AVENUE AND MOORE ROAD	1		PROTECT MONUMENT
E 137+15	44'	LT	135590104	5789	RCS	NORTH SIDE P.C. ELMWOOD AVENUE WEST OF KENDRICK ROAD	1		PROTECT MONUMENT
E 139+10	43'	LT	135600101	5793	RCS	NORTH SIDE P.T. ELMWOOD AVENUE NORTH OF KENDRICK ROAD	1		PROTECT MONUMENT
E 169+95	50'	RT	136540108	6118	RCS	S.W. ELMWOOD AVENUE AND MT. HOPE	1		CHECK CONDITION OF MONUMENT
E 170+10	85'	RT	163540107	6117	RCS	S.W. MT. HOPE AND ELMWOOD AVENUE			PROTECT MONUMENT
						TOTAL	6	3	

SNOWPLOWING MARKERS

ITEM NO.	DESCRIPTION
646.40	FLEXIBLE DELINEATOR, SINGLE UNIT, ONE WAY
646.42	FLEXIBLE DELINEATOR, DOUBLE UNIT

STATION	SIDE	COLOR	MOUNTING SURFACE	646.40 (EACH)	646.42 (EACH)	REMARKS
E 145+67	RT	YELLOW	SOIL	1		LT. SIDE OF TRAVELWAY
E 147+84	LT	YELLOW	SOIL		1	LT. SIDE OF TRAVELWAY
E 147+84	RT	YELLOW	SOIL	1		LT. SIDE OF TRAVELWAY
E 148+92	RT	YELLOW	CONCRETE	1		LT. SIDE OF TRAVELWAY
E 148+92	RT	YELLOW	CONCRETE		1	LT. SIDE OF TRAVELWAY
E 149+72	RT	YELLOW	SOIL	1		LT. SIDE OF TRAVELWAY
E 150+52	LT	YELLOW	SOIL	1		LT. SIDE OF TRAVELWAY
E 150+78	LT	YELLOW	SOIL	1		LT. SIDE OF TRAVELWAY
E 151+05	LT	YELLOW	SOIL		1	LT. SIDE OF TRAVELWAY
E 151+05	RT	YELLOW	SOIL	1		LT. SIDE OF TRAVELWAY
E 151+88	LT	YELLOW	SOIL	1		LT. SIDE OF TRAVELWAY
E 151+88	RT	YELLOW	SOIL		1	LT. SIDE OF TRAVELWAY
E 153+98	LT	YELLOW	SOIL		1	LT. SIDE OF TRAVELWAY
E 153+98	RT	YELLOW	SOIL	1		LT. SIDE OF TRAVELWAY
E 155+06	RT	YELLOW	CONCRETE	1		LT. SIDE OF TRAVELWAY
E 155+06	RT	YELLOW	CONCRETE		1	LT. SIDE OF TRAVELWAY
E 157+28	RT	YELLOW	SOIL	1		LT. SIDE OF TRAVELWAY
E 157+28	RT	YELLOW	SOIL	1		LT. SIDE OF TRAVELWAY
E 158+26	LT	YELLOW	SOIL	1		LT. SIDE OF TRAVELWAY
E 159+60	LT	YELLOW	SOIL		1	LT. SIDE OF TRAVELWAY
E 159+60	RT	YELLOW	SOIL	1		LT. SIDE OF TRAVELWAY
			SUBTOTAL	14	7	

SNOWPLOWING MARKERS

ITEM NO.	DESCRIPTION
646.40	FLEXIBLE DELINEATOR, SINGLE UNIT, ONE WAY
646.42	FLEXIBLE DELINEATOR, DOUBLE UNIT

STATION	SIDE	COLOR	MOUNTING SURFACE	646.40 (EACH)	646.42 (EACH)	REMARKS
E 160+72	RT	YELLOW	CONCRETE	1		LT. SIDE OF TRAVELWAY
E 160+72	RT	YELLOW	CONCRETE		1	LT. SIDE OF TRAVELWAY
E 161+74	RT	YELLOW	CONCRETE	1		LT. SIDE OF TRAVELWAY
E 162+23	RT	YELLOW	CONCRETE	1		LT. SIDE OF TRAVELWAY
E 162+72	LT	YELLOW	SOIL	1		LT. SIDE OF TRAVELWAY
E 163+12	RT	YELLOW	SOIL	1		LT. SIDE OF TRAVELWAY
E 163+74	LT	YELLOW	SOIL	1		LT. SIDE OF TRAVELWAY
E 165+32	RT	YELLOW	CONCRETE	1		LT. SIDE OF TRAVELWAY
E 166+01	RT	YELLOW	CONCRETE	1		LT. SIDE OF TRAVELWAY
E 167+92	RT	YELLOW	CONCRETE	1		LT. SIDE OF TRAVELWAY
E 168+28	RT	YELLOW	CONCRETE	1		LT. SIDE OF TRAVELWAY
E 170+56	RT	YELLOW	CONCRETE		1	LT. SIDE OF TRAVELWAY
E 170+56	RT	YELLOW	CONCRETE	1		LT. SIDE OF TRAVELWAY
			SUBTOTAL	11	2	
			TOTAL	57	27	

NOTE:

1. THE CONTRACTOR SHALL REFER TO NYSDOT STANDARD SHEET SERIES 646 FOR SNOWPLOWING MARKER DETAILS AND NOTES.

Lu Engineers
ENVIRONMENTAL • TRANSPORTATION • CIVIL
PROJECT MANAGER: J. OTTMAN
DRAWN BY: J. MESSENGER

Department of Environmental Services
Architecture and Engineering Services
City of Rochester, New York
ASSISTANT MANAGING ENGINEER LISA Y. REYES
CITY ENGINEER HOLLY E. BARRETT, P.E.

PROJECT TITLE
SCOTTSVILLE ROAD - ELMWOOD AVENUE IMPROVEMENT PROJECT (WEST CITY LINE - MT HOPE AVE)

NO.	REVISION	BY	DATE

ISSUED 03/06/2019
CHECKED JWO
DRAWN JRM
DESIGN JRM
SCALE NONE
PROJECT NUMBER 17102

DRAWING TITLE
MISCELLANEOUS TABLES MT-9

DRAWING NO. 33 OF 54

FILE NAME = Miscellaneous Tables.dgn
DATE/TIME = 06-MAR-2019 11:47
USER = JMessenger

TRAFFIC SIGNAL LOOPS

ITEM NO.	DESCRIPTION
680.54	INDUCTANCE LOOP INSTALLATION
680.72	INDUCTANCE LOOP WIRE

ROAD NAME	LOOP No.	DIRECTION	LOOP LENGTH (FT)	LOOP WIDTH (FT)	DIST. FROM PULLBOX (FT)	NUMBER OF TURNS	680.54 (FT)	680.72 (FT)	REMARKS
KINGSBORO ROAD	1	SB, BACK	15	6	8	2-4-2	65	160	TO BE REPLACED
KINGSBORO ROAD	2	SB, FRONT	10	6	20	2-4-2	62	144	TO BE REPLACED AND EXTENDED
SCOTTSVILLE ROAD AND KINGSBORO ROAD INTERSECTION TOTAL							**127**	**304**	
SCOTTSVILLE ROAD	17001 A	EB, LEFT, FRONT	20	6	80	2-4-2	152	344	TO BE REPLACED
SCOTTSVILLE ROAD	17001 B	EB, LEFT, BACK	20	6	60	2-4-2	132	304	TO BE REPLACED
SCOTTSVILLE ROAD	17002 A	EB, THRU'S / RIGHT, FRONT	20	6	40	3	92	236	TO BE REPLACED
SCOTTSVILLE ROAD	17002 B	EB, THRU'S / RIGHT, MIDDLE	20	6	20	3	72	196	TO BE REPLACED
GENESEE STREET	17005 A	SB, LEFT / THRU-LEFT, FRONT	20	6	25	3	77	206	TO BE REPLACED
GENESEE STREET	17005 B	SB, LEFT / THRU-LEFT, BACK	20	6	30	3	82	216	TO BE REPLACED
GENESEE STREET	17006 A	SB, RIGHT, FRONT	20	6	16	2-4-2	88	216	TO BE REPLACED
GENESEE STREET	17006 B	SB, RIGHT, BACK	20	6	18	2-4-2	90	220	TO BE REPLACED
SCOTTSVILLE ROAD	213	EB, SYSTEM SENSOR, INNER LANE	6	6	20	3	44	112	TO BE REPLACED
SCOTTSVILLE ROAD	214	EB, SYSTEM SENSOR, OUTER LANE	6	6	10	3	34	92	TO BE REPLACED
SCOTTSVILLE ROAD AND GENESEE STREET INTERSECTION TOTAL							**863**	**2,142**	
ELMWOOD AVENUE	17003	WB, LEFT / THRU'S, FRONT	20	6	25	3	77	206	TO BE REPLACED AND EXTENDED
ELMWOOD AVENUE	17004	WB, LEFT / THRU'S, BACK	20	6	35	3	87	226	TO BE REPLACED AND EXTENDED
ELMWOOD AVENUE	408	WB, SYSTEM SENSOR, INNER LANE	6	6	36	3	60	144	TO BE REPLACED
ELMWOOD AVENUE	409	WB, SYSTEM SENSOR, MIDDLE LANE	6	6	25	3	49	122	TO BE REPLACED
ELMWOOD AVENUE	410	WB, SYSTEM SENSOR, OUTER LANE	6	6	12	3	36	96	TO BE REPLACED
ELMWOOD AVENUE AND GENESEE STREET INTERSECTION TOTAL							**309**	**794**	
GV SPORTS COMPLEX	2	NB, LEFT / THRU / RIGHT, FRONT	28	6	30	3	98	264	TO BE REPLACED
GV SPORTS COMPLEX	3	NB, LEFT / THRU / RIGHT, BACK	24	6	35	3	95	250	TO BE REPLACED
S. PLYMOUTH AVENUE	5	SB, THRU / RIGHT, FRONT	20	6	46	2-4-2	98	276	TO BE REPLACED
S. PLYMOUTH AVENUE	6	SB, THRU / RIGHT, BACK	20	6	55	2-4-2	107	294	TO BE REPLACED
ELMWOOD AVENUE	217	WB, SYSTEM SENSOR, INNER LANE	6	6	18	3	42	108	TO BE REPLACED
ELMWOOD AVENUE	218	EB, SYSTEM SENSOR, INNER LANE	6	6	24	3	48	120	TO BE REPLACED
ELMWOOD AVENUE AND S. PLYMOUTH AVENUE INTERSECTION TOTAL							**488**	**1,312**	
ELMWOOD AVENUE	1	WB, LEFT	40	6	35	2-4-2	167	414	TO BE REPLACED
WILSON BOULEVARD	2	SB, RIGHT, FRONT	30	6	30	2-4-2	132	324	TO BE REPLACED
WILSON BOULEVARD	3	SB, LEFT / THRU-LEFT, FRONT	18	6	65	3	113	274	TO BE REPLACED
WILSON BOULEVARD	4	SB, LEFT / THRU-LEFT, MIDDLE	18	6	65	3	113	274	TO BE REPLACED
WILSON BOULEVARD	5	SB, LEFT / THRU-LEFT / RIGHT, BACK	25	6	70	3	132	326	TO BE REPLACED
ELMWOOD AVENUE	6	EB, LEFT	40	6	40	2-4-2	172	424	TO BE REPLACED
MOORE ROAD	7	NB, LEFT / THRU / RIGHT, FRONT	30	6	20	3	92	256	TO BE REPLACED
MOORE ROAD	8	NB, LEFT / THRU / RIGHT, MIDDLE	20	6	35	3	87	226	TO BE REPLACED
MOORE ROAD	9	NB, LEFT / THRU / RIGHT, BACK	15	6	40	3	82	206	TO BE REPLACED
ELMWOOD AVENUE AND WILSON BOULEVARD / MOORE ROAD INTERSECTION TOTAL							**1,090**	**2,724**	
KENDRICK ROAD	17301	NB, LEFT, FRONT	20	6	25	2-4-2	97	234	TO BE REPLACED
KENDRICK ROAD	17302	NB, LEFT, FRONT	20	6	12	2-4-2	84	208	TO BE REPLACED
KENDRICK ROAD	17303	NB, LEFTS, BACK	18	6	30	3	78	204	TO BE REPLACED
ELMWOOD AVENUE	17304	WB, LEFT	34	6	35	2-4-2	149	366	TO BE REPLACED
KENDRICK ROAD	206	NB, SYSTEM SENSOR, LEFT LANE	6	6		3			TO REMAIN
KENDRICK ROAD	219	NB, SYSTEM SENSOR, RIGHT LANE	6	6		3			TO REMAIN
ELMWOOD AVENUE	220	WB, SYSTEM SENSOR, OUTER LANE	6	6	15	3	39	102	TO BE REPLACED
ELMWOOD AVENUE	221	EB, SYSTEM SENSOR, OUTER LANE	6	6	8	3	32	88	TO BE REPLACED
ELMWOOD AVENUE AND KENDRICK ROAD INTERSECTION TOTAL							**479**	**1,202**	
						SUBTOTAL	**3,356**	**8,478**	

TRAFFIC SIGNAL LOOPS

ITEM NO.	DESCRIPTION
680.54	INDUCTANCE LOOP INSTALLATION
680.72	INDUCTANCE LOOP WIRE

ROAD NAME	LOOP No.	DIRECTION	LOOP LENGTH (FT)	LOOP WIDTH (FT)	DIST. FROM PULLBOX (FT)	NUMBER OF TURNS	680.54 (FT)	680.72 (FT)	REMARKS
U of R SCHOOL OF MEDICAL DENTISTRY	1A	NB, LEFT, FRONT	6	6	30	3			TO REMAIN
U of R SCHOOL OF MEDICAL DENTISTRY	2A	NB, RIGHT, FRONT	6	6	20	3			TO REMAIN
U of R SCHOOL OF MEDICAL DENTISTRY	1B	NB, LEFT, BACK	20	6	25	2-4-2			TO REMAIN
U of R SCHOOL OF MEDICAL DENTISTRY	2B	NB, RIGHT, BACK	20	6	15	2-4-2			TO REMAIN
ELMWOOD AVENUE AND U of R SCHOOL OF MEDICAL DENTISTRY									
ELMWOOD AVENUE	1A	WB, LEFT, FRONT	20	6	40	2-4-2	132	264	TO BE REPLACED
ELMWOOD AVENUE	1B	WB, LEFT, BACK	30	6	40	2-4-2	142	344	TO BE REPLACED
THOMAS H. JACKSON DRIVE	2	NB, LEFT	30	6	50	2-4-2	152	364	TO BE REPLACED
THOMAS H. JACKSON DRIVE	3	NB, RIGHT	30	6	40	2-4-2	142	344	TO BE REPLACED
ELMWOOD AVENUE AND THOMAS H. JACKSON DRIVE INTERSECTION TOTAL							**568**	**1,316**	
ELMWOOD AVENUE	66401 A	WB, LEFT, FRONT	20	6	35	2-4-2			TO REMAIN
ELMWOOD AVENUE	66401 B	WB, LEFT, BACK	20	6	32	2-4-2			TO REMAIN
EAST DRIVE	66405 A	NB, LEFT, FRONT	20	6	36	2-4-2	108	256	TO BE REPLACED
EAST DRIVE	66405 B	NB, LEFT, BACK	20	6	36	2-4-2	108	256	TO BE REPLACED
EAST DRIVE	66406 A	NB, RIGHT, FRONT	20	6	30	2-4-2	102	244	TO BE REPLACED
EAST DRIVE	66406 B	NB, RIGHT, BACK	20	6	30	2-4-2	102	244	TO BE REPLACED
ELMWOOD AVENUE AND EAST DRIVE INTERSECTION TOTAL							**420**	**1,000**	
						SUBTOTAL	**988**	**2,316**	
						TOTAL	**4,344**	**10,794**	

FILE NAME = Miscellaneous Tables.dgn
DATE/TIME = 06-MAR-2019 11:48
USER = JMessenger

Lu Engineers
ENVIRONMENTAL • TRANSPORTATION • CIVIL

PROJECT MANAGER: J. OTTMAN
DRAWN BY: J. MESSENGER

Department of Environmental Services
Architecture and Engineering Services
City of Rochester, New York

ASSISTANT MANAGING ENGINEER: LISA Y. REYES
CITY ENGINEER: HOLLY E. BARRETT, P.E.

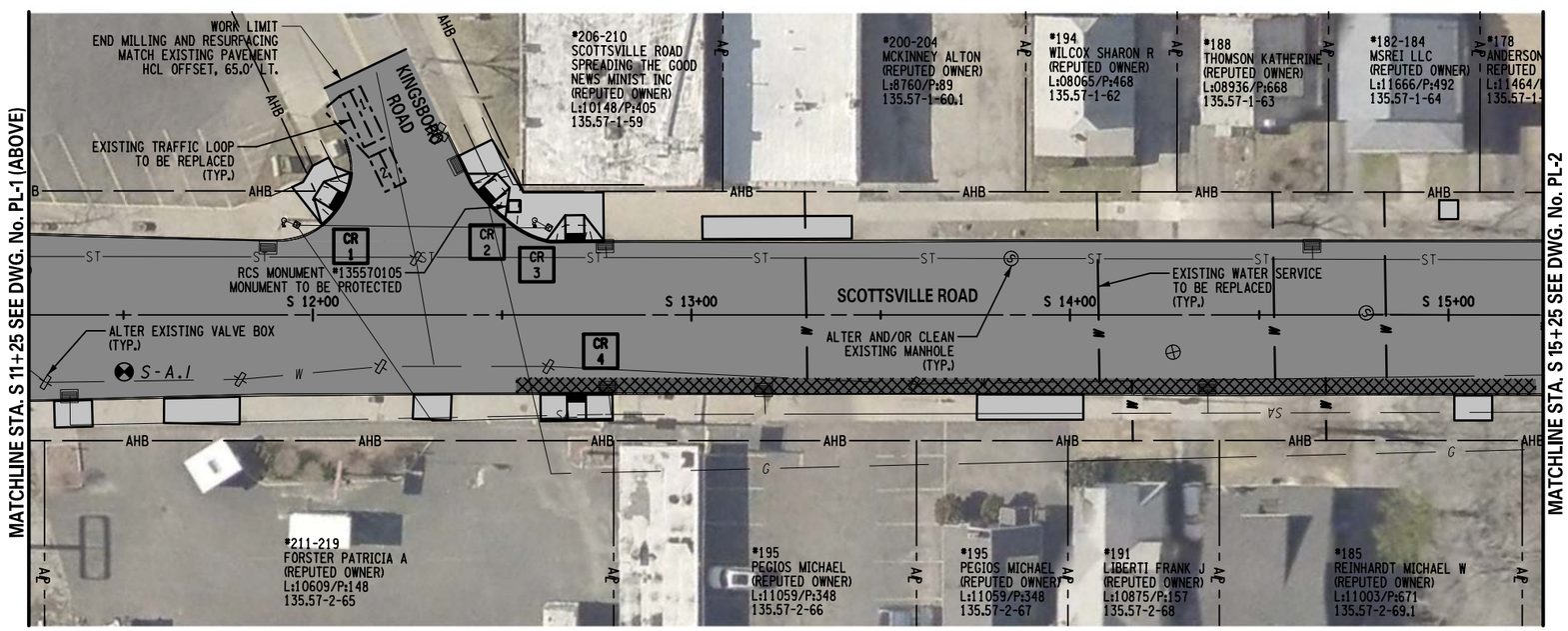
PROJECT TITLE
SCOTTSVILLE ROAD - ELMWOOD AVENUE IMPROVEMENT PROJECT
(WEST CITY LINE - MT HOPE AVE)

NO.	REVISION	BY	DATE

ISSUED 03/06/2019
CHECKED JWO
DRAWN JRM
DESIGN JRM
SCALE NONE
PROJECT NUMBER 17102

DRAWING TITLE
MISCELLANEOUS TABLES MT-10

DRAWING NO.
34
OF
54



ROADWAY PLAN NOTES:

1. THE CONTRACTOR SHALL REFER TO THE GENERAL NOTES ON DWGS. GN-1 THRU GN-3, MISCELLANEOUS DETAILS ON DWGS. MD-1 THRU MD-10 AND MISCELLANEOUS TABLES ON DWGS. MT-1 THRU MT-10 FOR ADDITIONAL INFORMATION ON THE FOLLOWING PROJECT WORK ITEMS:

A. CURB RAMP REPLACEMENT
B. STONE CURB REPAIR/REPLACEMENT
C. CONCRETE SIDEWALK REPLACEMENT
D. ROADWAY PAVEMENT REPAIR
E. ALTERING DRAINAGE STRUCTURES
F. ALTERING MANHOLES
G. REPLACEMENT OF WATER SERVICES
H. ADJUSTMENT OF VALVE BOXES WITHIN WORK LIMITS
I. REPLACEMENT OF TRAFFIC SIGNAL DETECTOR LOOPS
J. CITY OF ROCHESTER SURVEY MONUMENT LOCATIONS
K. PLACEMENT OF PAVEMENT MARKINGS
L. PLACEMENT OF SNOW PLOW MARKERS

ROADWAY PLAN LEGEND

- LIMITS OF SIDEWALK AND SIDEWALK CURB RAMP REPLACEMENTS. REFER TO SIDEWALK AND SIDEWALK CURB RAMP TABLES ON DWGS. MT-1, MT-2 AND MT-6 FOR ADDITIONAL INFORMATION.
- LIMITS OF HMA PAVEMENT MILLING AND RESURFACING. REFER TO MILLING AND PAVING TABLE ON DWG. MT-3 FOR ADDITIONAL INFORMATION.
- LIMITS OF FIELD OBSERVED SEVERE PAVEMENT DISTRESS IDENTIFIED FOR PAVEMENT REPAIRS. REFER TO PAVEMENT REPAIR TABLE ON DWGS. MT-4 AND MT-5 FOR ADDITIONAL INFORMATION.
- PAVEMENT CORE SYMBOL

SIDEWALK CURB RAMPS LEGEND

THE CONTRACTOR SHALL REFER TO THE SIDEWALK CURB RAMP TABLE ON DWGS. MT-1 AND MT-2 FOR ADDITIONAL INFORMATION CONCERNING THE DISPOSITION OF ALL SIDEALK CURB RAMPS WITHIN THE PROJECT LIMITS. THE CONTRACTOR SHALL REFER TO THE "PEDESTRIAN ACCESSIBILITY GUIDELINES" DWG. GN-3 FOR ADDITIONAL CONSTRUCTION NOTES.

CR XX — NEW DETECTABLE WARNING UNIT (DWU) TO BE INSTALLED.

CR XX — RAMP TO BE REPLACED

0 20 40 60

Lu Engineers
ENVIRONMENTAL • TRANSPORTATION • CIVIL

PROJECT MANAGER: J. OTTMAN
DRAWN BY: J. MESSENGER

Department of Environmental Services
Architecture and Engineering Services
City of Rochester, New York

ASSISTANT MANAGING ENGINEER: LISA Y. REYES
CITY ENGINEER: HOLLY E. BARRETT, P.E.

PROJECT TITLE
SCOTTSVILLE ROAD - ELMWOOD AVENUE IMPROVEMENT PROJECT (WEST CITY LINE - MT HOPE AVE)

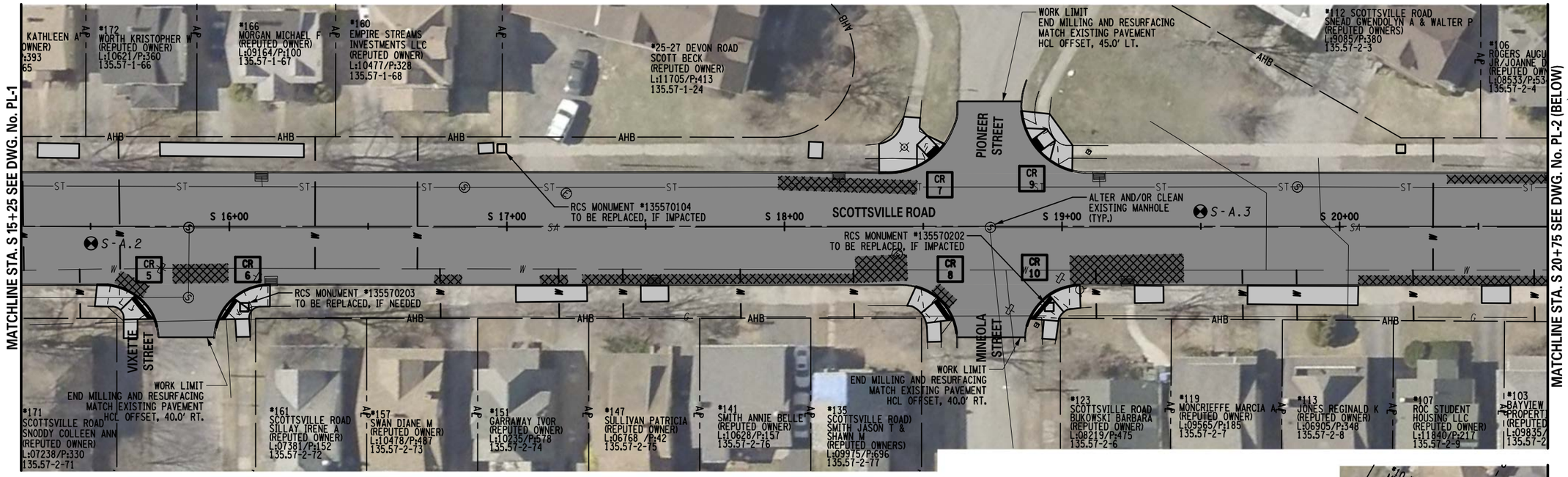
NO.	REVISION	BY	DATE

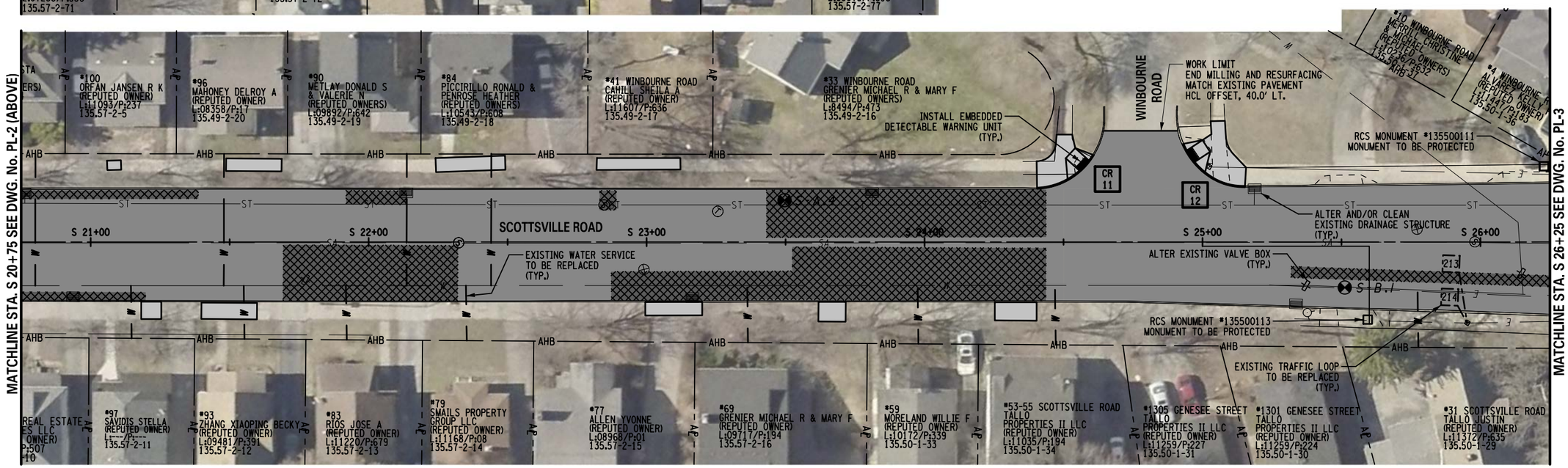
ISSUED 03/06/2019
CHECKED JWO
DRAWN JRM
DESIGN JRM
SCALE 1" = 20'
PROJECT NUMBER 17102

DRAWING TITLE
ROADWAY PLAN
PL-1

DRAWING NO. 35 OF 54

FILE NAME = Roadway Plans.dgn
DATE/TIME = 06-MAR-2019 11:52
USER = JMessenger





ROADWAY PLAN NOTES:

1. THE CONTRACTOR SHALL REFER TO THE GENERAL NOTES ON DWGS. GN-1 THRU GN-3, MISCELLANEOUS DETAILS ON DWGS. MD-1 THRU MD-10 AND MISCELLANEOUS TABLES ON DWGS. MT-1 THRU MT-10 FOR ADDITIONAL INFORMATION ON THE FOLLOWING PROJECT WORK ITEMS:

A. CURB RAMP REPLACEMENT
B. STONE CURB REPAIR/REPLACEMENT
C. CONCRETE SIDEWALK REPLACEMENT
D. ROADWAY PAVEMENT REPAIR
E. ALTERING DRAINAGE STRUCTURES
F. ALTERING MANHOLES
G. REPLACEMENT OF WATER SERVICES
H. ADJUSTMENT OF VALVE BOXES WITHIN WORK LIMITS
I. REPLACEMENT OF TRAFFIC SIGNAL DETECTOR LOOPS
J. CITY OF ROCHESTER SURVEY MONUMENT LOCATIONS
K. PLACEMENT OF PAVEMENT MARKINGS
L. PLACEMENT OF SNOW PLOW MARKERS

ROADWAY PLAN LEGEND

- LIMITS OF SIDEWALK AND SIDEWALK CURB RAMP REPLACEMENTS. REFER TO SIDEWALK AND SIDEWALK CURB RAMP TABLES ON DWGS. MT-1, MT-2 AND MT-6 FOR ADDITIONAL INFORMATION.
- LIMITS OF HMA PAVEMENT MILLING AND RESURFACING. REFER TO MILLING AND PAVING TABLE ON DWG. MT-3 FOR ADDITIONAL INFORMATION.
- LIMITS OF FIELD OBSERVED SEVERE PAVEMENT DISTRESS IDENTIFIED FOR PAVEMENT REPAIRS. REFER TO PAVEMENT REPAIR TABLE ON DWGS. MT-4 AND MT-5 FOR ADDITIONAL INFORMATION.
- PAVEMENT CORE SYMBOL

SIDEWALK CURB RAMPS LEGEND

THE CONTRACTOR SHALL REFER TO THE SIDEWALK CURB RAMP TABLE ON DWGS. MT-1 AND MT-2 FOR ADDITIONAL INFORMATION CONCERNING THE DISPOSITION OF ALL SIDEALK CURB RAMPS WITHIN THE PROJECT LIMITS. THE CONTRACTOR SHALL REFER TO THE "PEDESTRIAN ACCESSIBILITY GUIDELINES" DWG. GN-3 FOR ADDITIONAL CONSTRUCTION NOTES.

CR XX — NEW DETECTABLE WARNING UNIT (DWU) TO BE INSTALLED.

CR XX — RAMP TO BE REPLACED

0 20 40 60

FILE NAME = Roadway Plans.dgn
DATE/TIME = 06-MAR-2019 11:53
USER = JMessenger

Lu Engineers
ENVIRONMENTAL • TRANSPORTATION • CIVIL

PROJECT MANAGER: J. OTTMAN
DRAWN BY: J. MESSENGER

Department of Environmental Services
Architecture and Engineering Services
City of Rochester, New York

ASSISTANT MANAGING ENGINEER: LISA Y. REYES
CITY ENGINEER: HOLLY E. BARRETT, P.E.

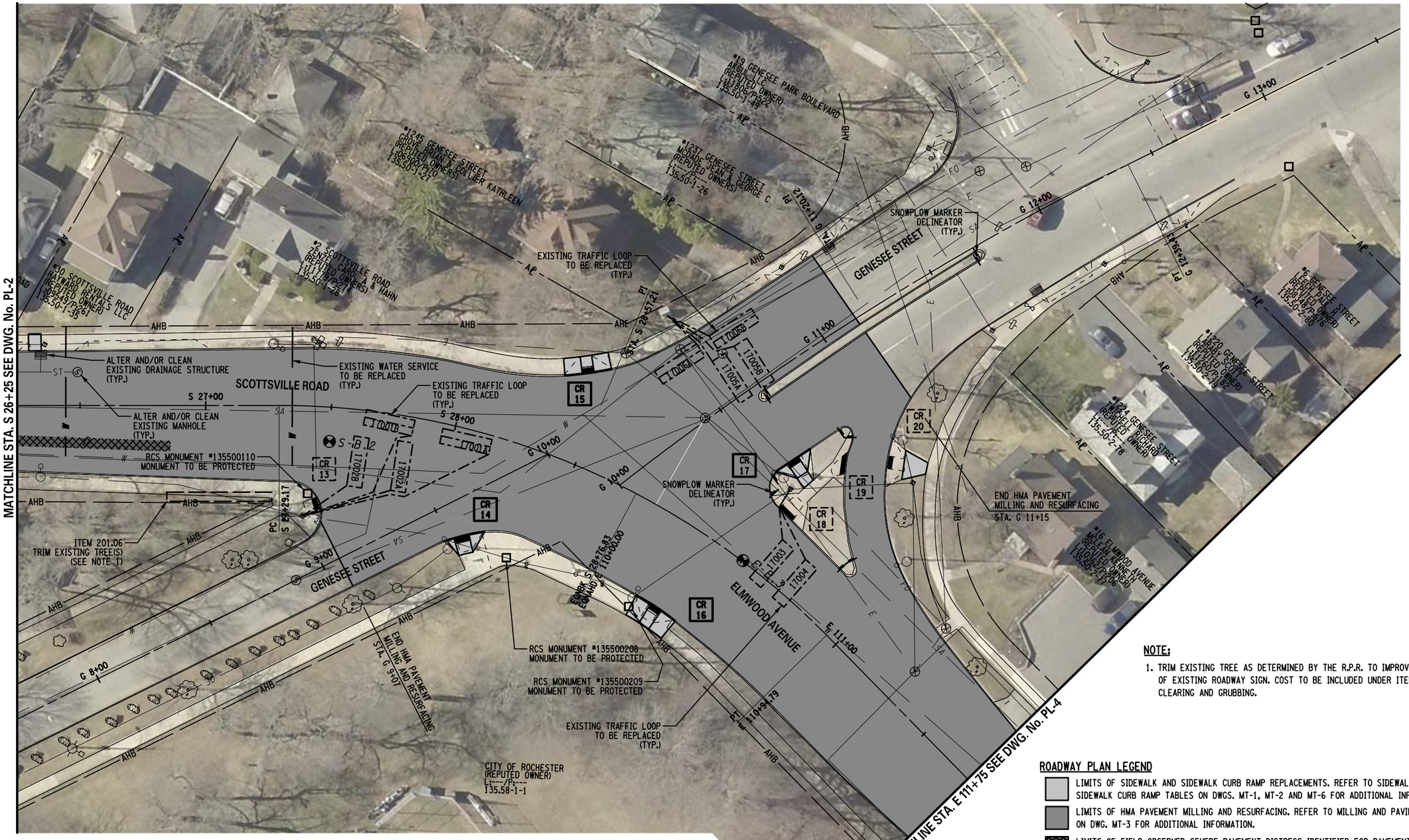
PROJECT TITLE
SCOTTSVILLE ROAD - ELMWOOD AVENUE IMPROVEMENT PROJECT (WEST CITY LINE - MT HOPE AVE)

NO.	REVISION	BY	DATE

ISSUED 03/06/2019
CHECKED JWO
DRAWN JRM
DESIGN JRM
SCALE 1" = 20'
PROJECT NUMBER 17102

DRAWING TITLE
ROADWAY PLAN PL-2

DRAWING NO. 36 OF 54



ROADWAY PLAN NOTES:

1. THE CONTRACTOR SHALL REFER TO THE GENERAL NOTES ON DWGS. GN-1 THRU GN-3, MISCELLANEOUS DETAILS ON DWGS. MD-1 THRU MD-10 AND MISCELLANEOUS TABLES ON DWGS. MT-1 THRU MT-10 FOR ADDITIONAL INFORMATION ON THE FOLLOWING PROJECT WORK ITEMS:

A. CURB RAMP REPLACEMENT
B. STONE CURB REPAIR/REPLACEMENT
C. CONCRETE SIDEWALK REPLACEMENT
D. ROADWAY PAVEMENT REPAIR
E. ALTERING DRAINAGE STRUCTURES
F. ALTERING MANHOLES
G. REPLACEMENT OF WATER SERVICES
H. ADJUSTMENT OF VALVE BOXES WITHIN WORK LIMITS
I. REPLACEMENT OF TRAFFIC SIGNAL DETECTOR LOOPS
J. CITY OF ROCHESTER SURVEY MONUMENT LOCATIONS
K. PLACEMENT OF PAVEMENT MARKINGS
L. PLACEMENT OF SNOW PLOW MARKERS

SIDEWALK CURB RAMPS LEGEND

THE CONTRACTOR SHALL REFER TO THE SIDEWALK CURB RAMP TABLE ON DWGS. MT-1 AND MT-2 FOR ADDITIONAL INFORMATION CONCERNING THE DISPOSITION OF ALL SIDEALK CURB RAMPS WITHIN THE PROJECT LIMITS. THE CONTRACTOR SHALL REFER TO THE "PEDESTRIAN ACCESSIBILITY GUIDELINES" DWG. GN-3 FOR ADDITIONAL CONSTRUCTION NOTES.

CR XX — NEW DETECTABLE WARNING UNIT (DWU) TO BE INSTALLED.

CR XX — RAMP TO BE REPLACED

NOTE:

1. TRIM EXISTING TREE AS DETERMINED BY THE R.P.R. TO IMPROVE VISIBILITY OF EXISTING ROADWAY SIGN. COST TO BE INCLUDED UNDER ITEM 201.06, CLEARING AND GRUBBING.

ROADWAY PLAN LEGEND

- LIMITS OF SIDEWALK AND SIDEWALK CURB RAMP REPLACEMENTS. REFER TO SIDEWALK AND SIDEWALK CURB RAMP TABLES ON DWGS. MT-1, MT-2 AND MT-6 FOR ADDITIONAL INFORMATION.
- LIMITS OF HMA PAVEMENT MILLING AND RESURFACING. REFER TO MILLING AND PAVING TABLE ON DWG. MT-3 FOR ADDITIONAL INFORMATION.
- LIMITS OF FIELD OBSERVED SEVERE PAVEMENT DISTRESS IDENTIFIED FOR PAVEMENT REPAIRS. REFER TO PAVEMENT REPAIR TABLE ON DWGS. MT-4 AND MT-5 FOR ADDITIONAL INFORMATION.
- PAVEMENT CORE SYMBOL

0 20 40 60

Lu Engineers
ENVIRONMENTAL • TRANSPORTATION • CIVIL

PROJECT MANAGER: J. OTTMAN
DRAWN BY: J. MESSENGER

Department of Environmental Services
Architecture and Engineering Services
City of Rochester, New York

ASSISTANT MANAGING ENGINEER: LISA Y. REYES
CITY ENGINEER: HOLLY E. BARRETT, P.E.

PROJECT TITLE
SCOTTSVILLE ROAD - ELMWOOD AVENUE IMPROVEMENT PROJECT (WEST CITY LINE - MT HOPE AVE)

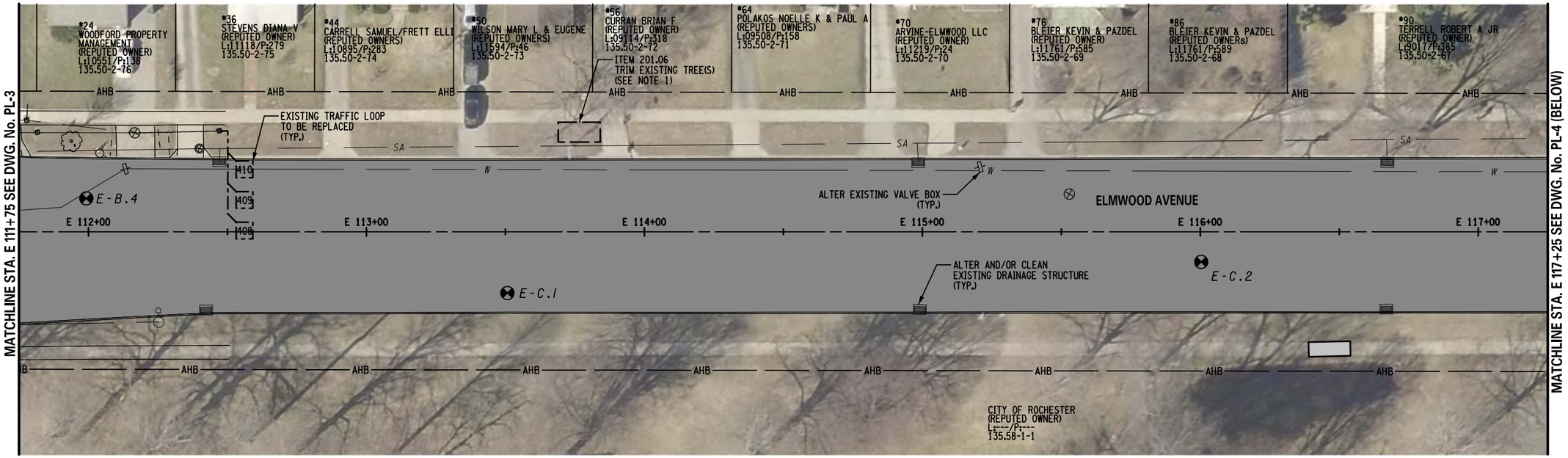
NO.	REVISION	BY	DATE

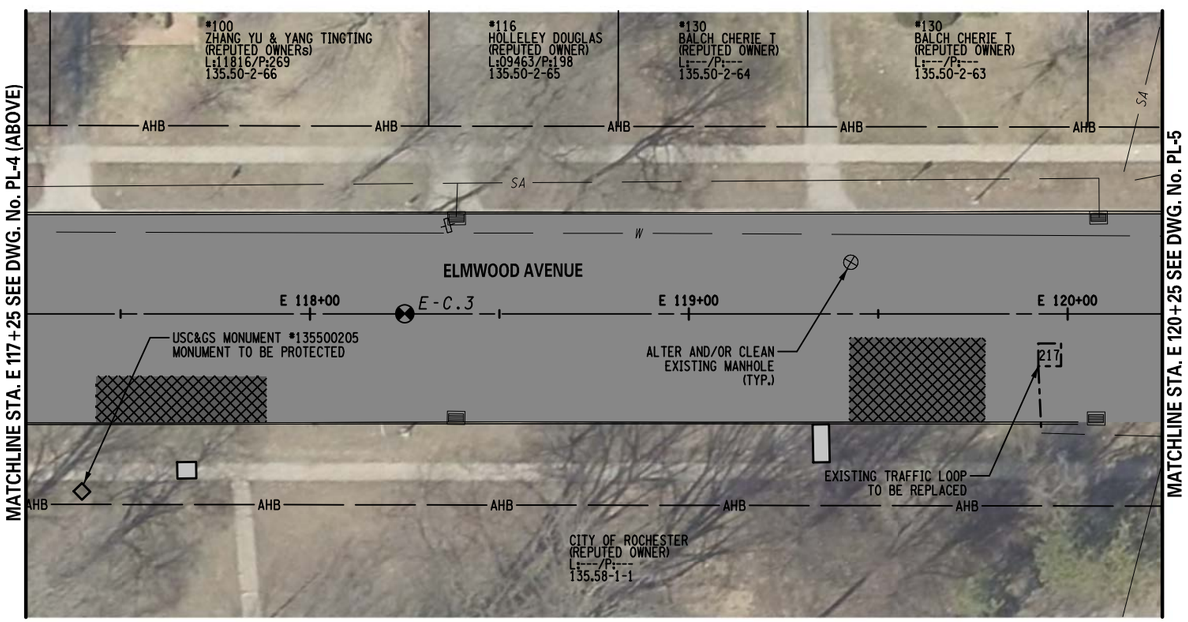
ISSUED 03/06/2019
CHECKED JWO
DRAWN JRM
DESIGN JRM
SCALE 1" = 20'
PROJECT NUMBER 17102

DRAWING TITLE
ROADWAY PLAN
PL-3

DRAWING NO. 37 OF 54

FILE NAME = Roadway Plans.dgn
DATE/TIME = 06-MAR-2019 11:53
USER = JMessenger





ROADWAY PLAN NOTES:

1. THE CONTRACTOR SHALL REFER TO THE GENERAL NOTES ON DWGS. GN-1 THRU GN-3, MISCELLANEOUS DETAILS ON DWGS. MD-1 THRU MD-10 AND MISCELLANEOUS TABLES ON DWGS. MT-1 THRU MT-10 FOR ADDITIONAL INFORMATION ON THE FOLLOWING PROJECT WORK ITEMS:

A. CURB RAMP REPLACEMENT
B. STONE CURB REPAIR/REPLACEMENT
C. CONCRETE SIDEWALK REPLACEMENT
D. ROADWAY PAVEMENT REPAIR
E. ALTERING DRAINAGE STRUCTURES
F. ALTERING MANHOLES
G. REPLACEMENT OF WATER SERVICES
H. ADJUSTMENT OF VALVE BOXES WITHIN WORK LIMITS
I. REPLACEMENT OF TRAFFIC SIGNAL DETECTOR LOOPS
J. CITY OF ROCHESTER SURVEY MONUMENT LOCATIONS
K. PLACEMENT OF PAVEMENT MARKINGS
L. PLACEMENT OF SNOW PLOW MARKERS

ROADWAY PLAN LEGEND

- LIMITS OF SIDEWALK AND SIDEWALK CURB RAMP REPLACEMENTS. REFER TO SIDEWALK AND SIDEWALK CURB RAMP TABLES ON DWGS. MT-1, MT-2 AND MT-6 FOR ADDITIONAL INFORMATION.
- LIMITS OF HMA PAVEMENT MILLING AND RESURFACING. REFER TO MILLING AND PAVING TABLE ON DWG. MT-3 FOR ADDITIONAL INFORMATION.
- LIMITS OF FIELD OBSERVED SEVERE PAVEMENT DISTRESS IDENTIFIED FOR PAVEMENT REPAIRS. REFER TO PAVEMENT REPAIR TABLE ON DWGS. MT-4 AND MT-5 FOR ADDITIONAL INFORMATION.
- PAVEMENT CORE SYMBOL

NOTE:

1. TRIM EXISTING TREE AS DETERMINED BY THE R.P.R. TO IMPROVE VISIBILITY OF EXISTING ROADWAY SIGN. COST TO BE INCLUDED UNDER ITEM 201.06, CLEARING AND GRUBBING.

SIDEWALK CURB RAMPS LEGEND

THE CONTRACTOR SHALL REFER TO THE SIDEWALK CURB RAMP TABLE ON DWGS. MT-1 AND MT-2 FOR ADDITIONAL INFORMATION CONCERNING THE DISPOSITION OF ALL SIDEALK CURB RAMPS WITHIN THE PROJECT LIMITS. THE CONTRACTOR SHALL REFER TO THE "PEDESTRIAN ACCESSIBILITY GUIDELINES" DWG. GN-3 FOR ADDITIONAL CONSTRUCTION NOTES.

CR XX — NEW DETECTABLE WARNING UNIT (DWU) TO BE INSTALLED.

CR XX — RAMP TO BE REPLACED

0 20 40 60

FILE NAME = Roadway Plans.dgn
DATE/TIME = 06-MAR-2019 11:54
USER = JMessenger

Lu Engineers
ENVIRONMENTAL • TRANSPORTATION • CIVIL

PROJECT MANAGER: J. OTTMAN
DRAWN BY: J. MESSENGER

Department of Environmental Services
Architecture and Engineering Services
City of Rochester, New York

ASSISTANT MANAGING ENGINEER: LISA Y. REYES
CITY ENGINEER: HOLLY E. BARRETT, P.E.

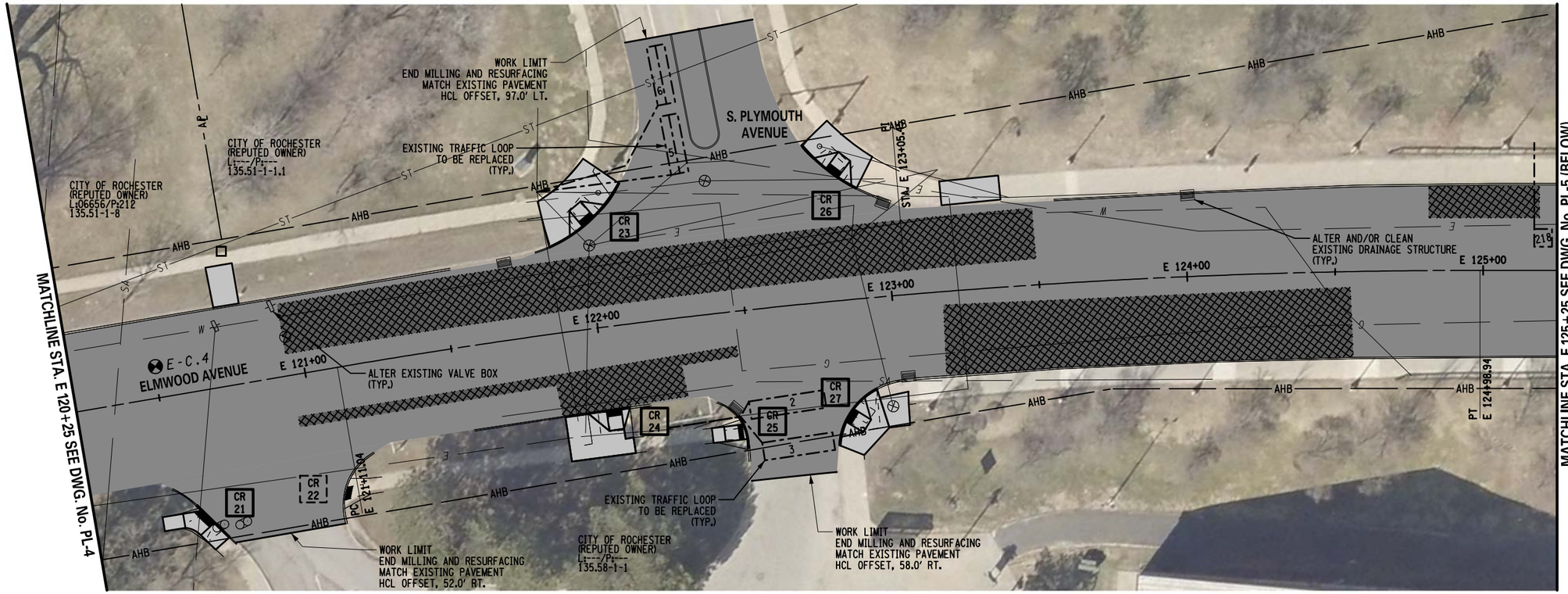
PROJECT TITLE
SCOTTSVILLE ROAD - ELMWOOD AVENUE IMPROVEMENT PROJECT (WEST CITY LINE - MT HOPE AVE)

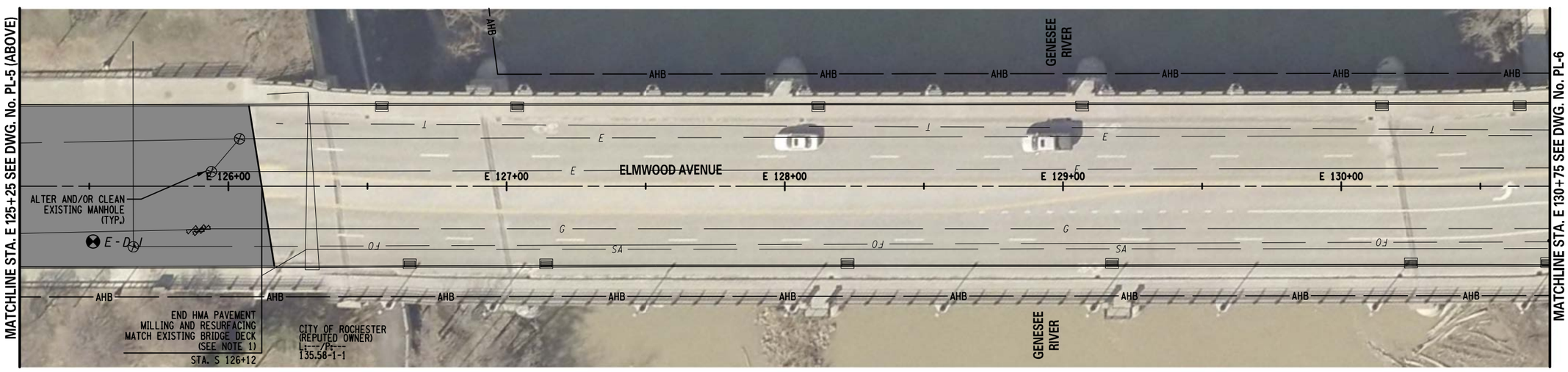
NO.	REVISION	BY	DATE

ISSUED 03/06/2019
CHECKED JWO
DRAWN JRM
DESIGN JRM
SCALE 1" = 20'
PROJECT NUMBER 17102

DRAWING TITLE
ROADWAY PLAN
PL-4

DRAWING NO. 38 OF 54





ROADWAY PLAN NOTES:

1. THE CONTRACTOR SHALL REFER TO THE GENERAL NOTES ON DWGS. GN-1 THRU GN-3, MISCELLANEOUS DETAILS ON DWGS. MD-1 THRU MD-10 AND MISCELLANEOUS TABLES ON DWGS. MT-1 THRU MT-10 FOR ADDITIONAL INFORMATION ON THE FOLLOWING PROJECT WORK ITEMS:

A. CURB RAMP REPLACEMENT
B. STONE CURB REPAIR/REPLACEMENT
C. CONCRETE SIDEWALK REPLACEMENT
D. ROADWAY PAVEMENT REPAIR
E. ALTERING DRAINAGE STRUCTURES
F. ALTERING MANHOLES
G. REPLACEMENT OF WATER SERVICES
H. ADJUSTMENT OF VALVE BOXES WITHIN WORK LIMITS
I. REPLACEMENT OF TRAFFIC SIGNAL DETECTOR LOOPS
J. CITY OF ROCHESTER SURVEY MONUMENT LOCATIONS
K. PLACEMENT OF PAVEMENT MARKINGS
L. PLACEMENT OF SNOW PLOW MARKERS

ROADWAY PLAN LEGEND

- LIMITS OF SIDEWALK AND SIDEWALK CURB RAMP REPLACEMENTS. REFER TO SIDEWALK AND SIDEWALK CURB RAMP TABLES ON DWGS. MT-1, MT-2 AND MT-6 FOR ADDITIONAL INFORMATION.
- LIMITS OF HMA PAVEMENT MILLING AND RESURFACING. REFER TO MILLING AND PAVING TABLE ON DWG. MT-3 FOR ADDITIONAL INFORMATION.
- LIMITS OF FIELD OBSERVED SEVERE PAVEMENT DISTRESS IDENTIFIED FOR PAVEMENT REPAIRS. REFER TO PAVEMENT REPAIR TABLE ON DWGS. MT-4 AND MT-5 FOR ADDITIONAL INFORMATION.
- PAVEMENT CORE SYMBOL

SIDEWALK CURB RAMPS LEGEND

THE CONTRACTOR SHALL REFER TO THE SIDEWALK CURB RAMP TABLE ON DWGS. MT-1 AND MT-2 FOR ADDITIONAL INFORMATION CONCERNING THE DISPOSITION OF ALL SIDEALK CURB RAMPS WITHIN THE PROJECT LIMITS. THE CONTRACTOR SHALL REFER TO THE "PEDESTRIAN ACCESSIBILITY GUIDELINES" DWG. GN-3 FOR ADDITIONAL CONSTRUCTION NOTES.

- CR XX — NEW DETECTABLE WARNING UNIT (DWU) TO BE INSTALLED.
- CR XX — RAMP TO BE REPLACED

NOTE:

1. THIS PROJECT DOES NOT INCLUDE ANY WORK ON THE BRIDGE, INCLUDING BUT NOT LIMITED TO: SIDEWALK, BRIDGE DECK, DRAINAGE STRUCTURES AND PAVEMENT MARKINGS.

0 20 40 60

Lu Engineers
ENVIRONMENTAL • TRANSPORTATION • CIVIL

PROJECT MANAGER: J. OTTMAN
DRAWN BY: J. MESSENGER

Department of Environmental Services
Architecture and Engineering Services
City of Rochester, New York

ASSISTANT MANAGING ENGINEER: LISA Y. REYES
CITY ENGINEER: HOLLY E. BARRETT, P.E.

PROJECT TITLE
SCOTTSVILLE ROAD - ELMWOOD AVENUE
IMPROVEMENT PROJECT
(WEST CITY LINE - MT HOPE AVE)

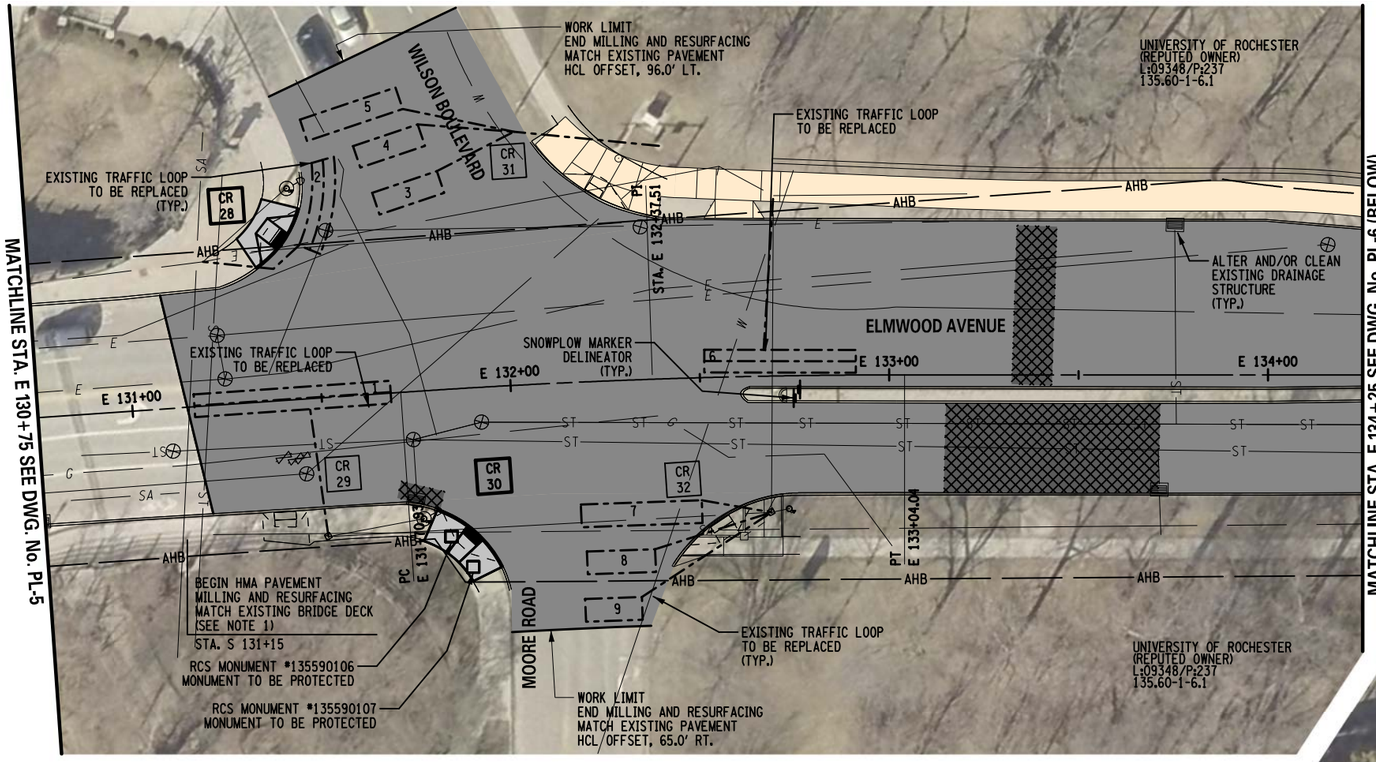
NO.	REVISION	BY	DATE

ISSUED 03/06/2019
CHECKED JWO
DRAWN JRM
DESIGN JRM
SCALE 1" = 20'
PROJECT NUMBER 17102

DRAWING TITLE
ROADWAY PLAN
PL-5

DRAWING NO. 39
OF 54

FILE NAME = Roadway Plans.dgn
DATE/TIME = 06-MAR-2019 11:54
USER = JMessenger



SIDEWALK CURB RAMPS LEGEND

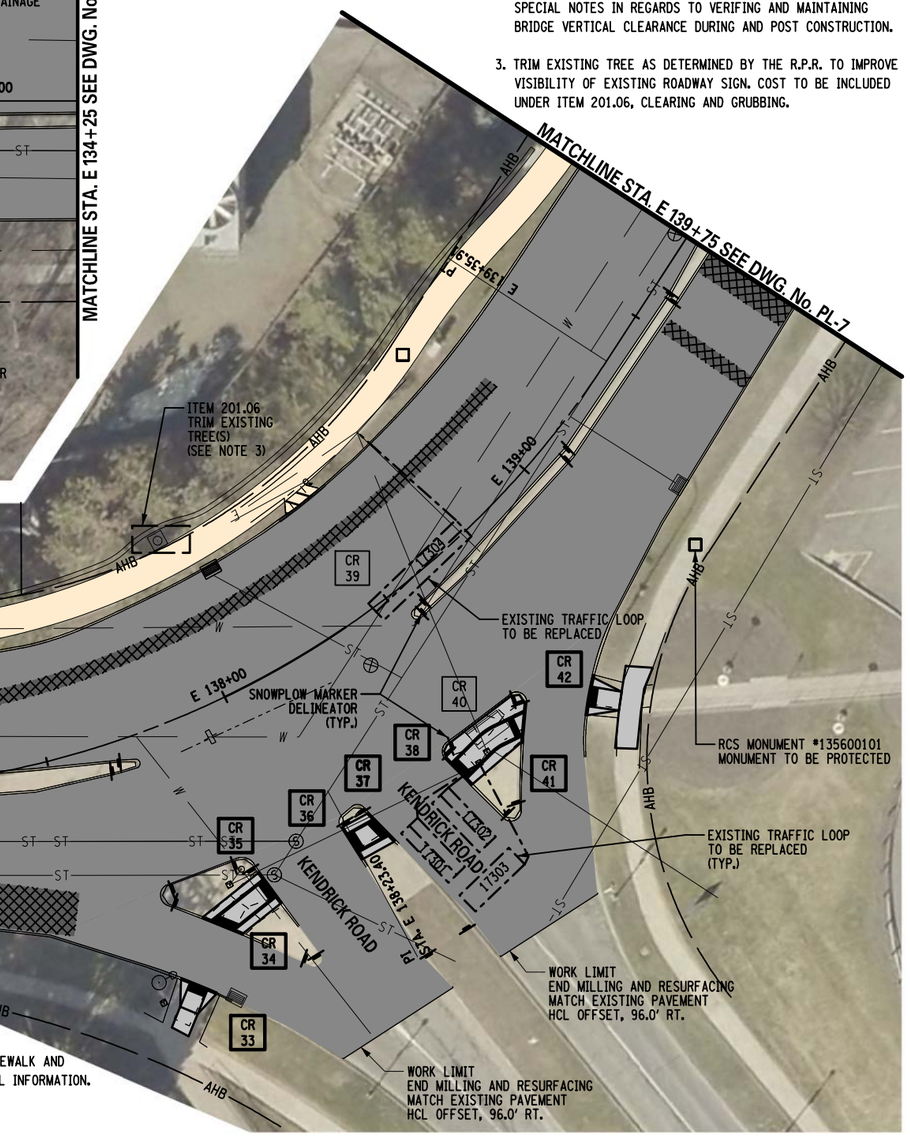
THE CONTRACTOR SHALL REFER TO THE SIDEWALK CURB RAMP TABLE ON DWGS. MT-1 AND MT-2 FOR ADDITIONAL INFORMATION CONCERNING THE DISPOSITION OF ALL SIDEALK CURB RAMPS WITHIN THE PROJECT LIMITS. THE CONTRACTOR SHALL REFER TO THE "PEDESTRIAN ACCESSIBILITY GUIDELINES" DWG. GN-3 FOR ADDITIONAL CONSTRUCTION NOTES.

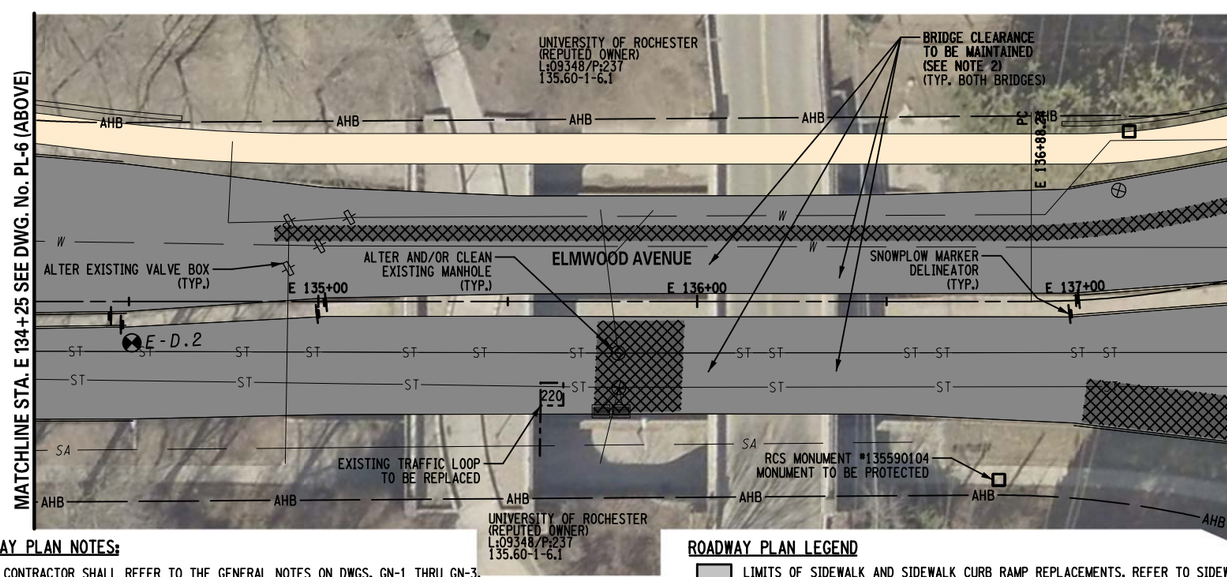
CR XX — NEW DETECTABLE WARNING UNIT (DWU) TO BE INSTALLED.

CR XX — RAMP TO BE REPLACED

NOTES:

1. THIS PROJECT DOES NOT INCLUDE ANY WORK ON THE BRIDGE, INCLUDING BUT NOT LIMITED TO: SIDEWALK, BRIDGE DECK, DRAINAGE STRUCTURES AND PAVEMENT MARKINGS.
2. REFER TO THE CONTRACT PROPOSAL SPECIFICATION BOOK FOR SPECIAL NOTES IN REGARDS TO VERIFING AND MAINTAINING BRIDGE VERTICAL CLEARANCE DURING AND POST CONSTRUCTION.
3. TRIM EXISTING TREE AS DETERMINED BY THE R.P.R. TO IMPROVE VISIBILITY OF EXISTING ROADWAY SIGN. COST TO BE INCLUDED UNDER ITEM 201.06, CLEARING AND GRUBBING.





ROADWAY PLAN NOTES:

1. THE CONTRACTOR SHALL REFER TO THE GENERAL NOTES ON DWGS. GN-1 THRU GN-3, MISCELLANEOUS DETAILS ON DWGS. MD-1 THRU MD-10 AND MISCELLANEOUS TABLES ON DWGS. MT-1 THRU MT-10 FOR ADDITIONAL INFORMATION ON THE FOLLOWING PROJECT WORK ITEMS:

A. CURB RAMP REPLACEMENT
B. STONE CURB REPAIR/REPLACEMENT
C. CONCRETE SIDEWALK REPLACEMENT
D. ROADWAY PAVEMENT REPAIR
E. ALTERING DRAINAGE STRUCTURES
F. ALTERING MANHOLES
G. REPLACEMENT OF WATER SERVICES
H. ADJUSTMENT OF VALVE BOXES WITHIN WORK LIMITS
I. REPLACEMENT OF TRAFFIC SIGNAL DETECTOR LOOPS
J. CITY OF ROCHESTER SURVEY MONUMENT LOCATIONS
K. PLACEMENT OF PAVEMENT MARKINGS
L. PLACEMENT OF SNOW PLOW MARKERS

ROADWAY PLAN LEGEND

- LIMITS OF SIDEWALK AND SIDEWALK CURB RAMP REPLACEMENTS. REFER TO SIDEWALK AND SIDEWALK CURB RAMP TABLES ON DWGS. MT-1, MT-2 AND MT-6 FOR ADDITIONAL INFORMATION.
- LIMITS OF THE ELMWOOD AVENUE COLLEGE TOWN CYCLE TRACK TO BE CONSTRUCTED UNDER SEPARATE CONTRACT D034682 IN THE SPRING OF 2019
- LIMITS OF HMA PAVEMENT MILLING AND RESURFACING. REFER TO MILLING AND PAVING TABLE ON DWG. MT-3 FOR ADDITIONAL INFORMATION.
- LIMITS OF FIELD OBSERVED SEVERE PAVEMENT DISTRESS IDENTIFIED FOR PAVEMENT REPAIRS. REFER TO PAVEMENT REPAIR TABLE ON DWGS. MT-4 AND MT-5 FOR ADDITIONAL INFORMATION.
- PAVEMENT CORE SYMBOL

0 20 40 60

Lu Engineers
ENVIRONMENTAL • TRANSPORTATION • CIVIL
PROJECT MANAGER: J. OTTMAN
DRAWN BY: J. MESSENGER

Department of Environmental Services
Architecture and Engineering Services
City of Rochester, New York
ASSISTANT MANAGING ENGINEER: LISA Y. REYES
CITY ENGINEER: HOLLY E. BARRETT, P.E.

PROJECT TITLE
SCOTTSVILLE ROAD - ELMWOOD AVENUE IMPROVEMENT PROJECT
(WEST CITY LINE - MT HOPE AVE)

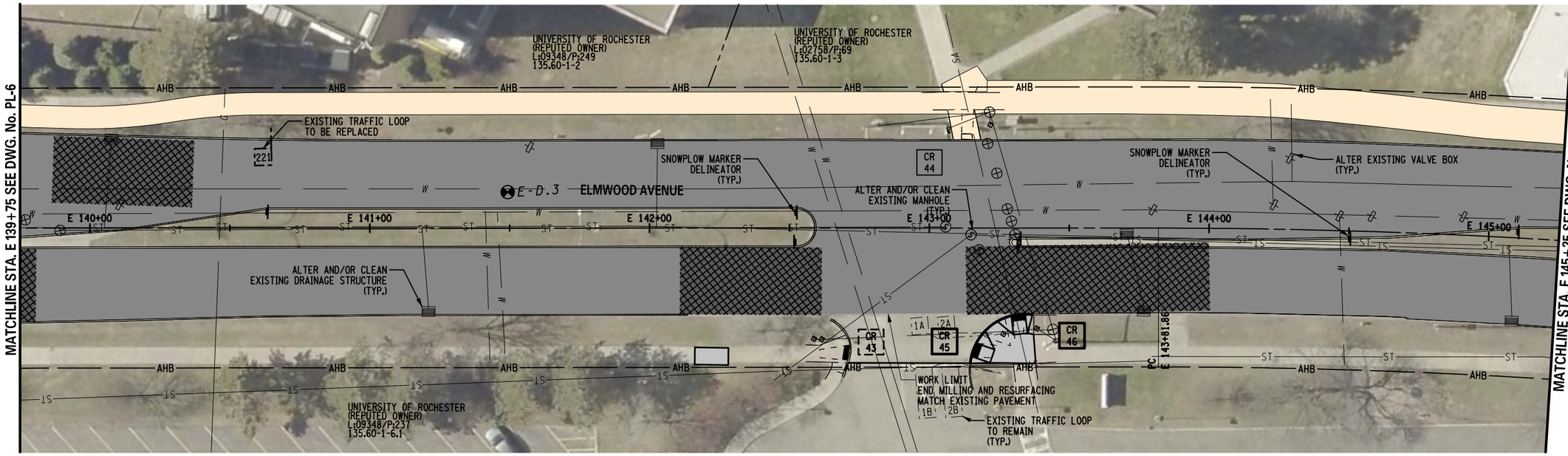
NO.	REVISION	BY	DATE

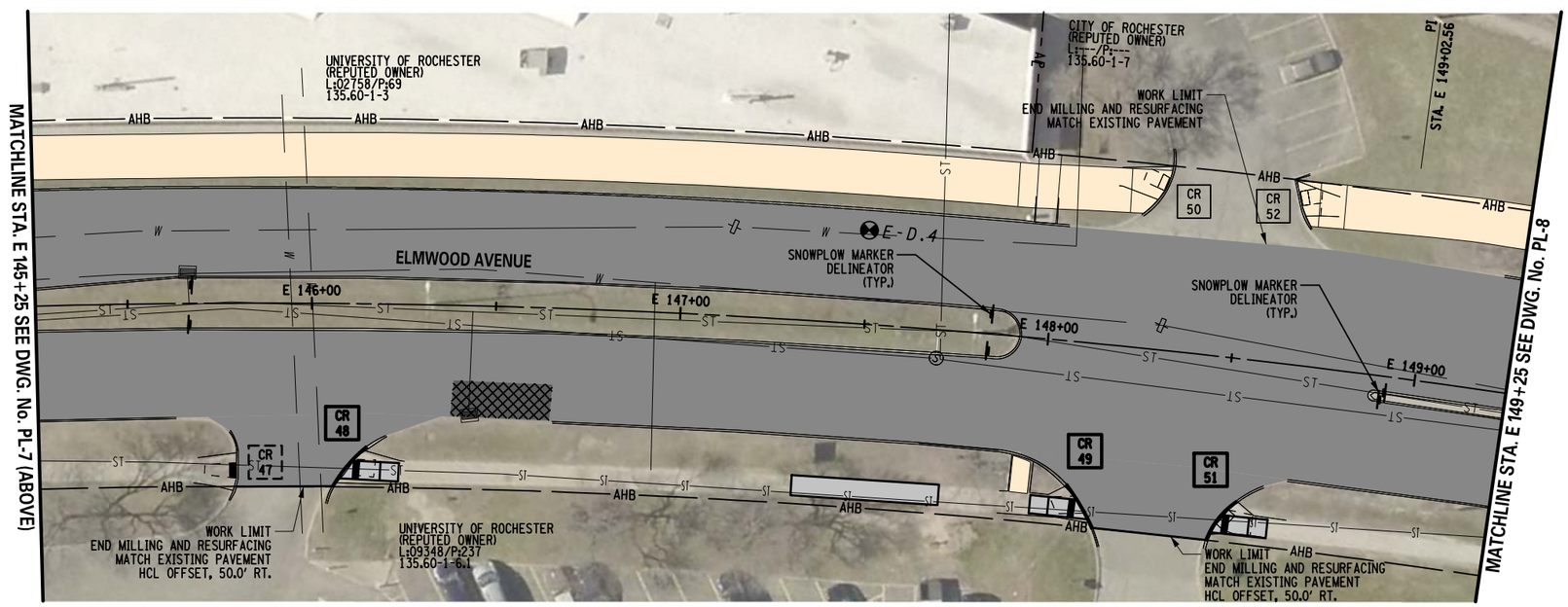
ISSUED 03/06/2019
CHECKED JWO
DRAWN JRM
DESIGN JRM
SCALE 1" = 20'
PROJECT NUMBER 17102

DRAWING TITLE
ROADWAY PLAN
PL-6

DRAWING NO. 40 OF 54

FILE NAME = Roadway Plans.dgn
DATE/TIME = 06-MAR-2019 11:55
USER = JMessenger





ROADWAY PLAN NOTES:

1. THE CONTRACTOR SHALL REFER TO THE GENERAL NOTES ON DWGS. GN-1 THRU GN-3, MISCELLANEOUS DETAILS ON DWGS. MD-1 THRU MD-10 AND MISCELLANEOUS TABLES ON DWGS. MT-1 THRU MT-10 FOR ADDITIONAL INFORMATION ON THE FOLLOWING PROJECT WORK ITEMS:

A. CURB RAMP REPLACEMENT
B. STONE CURB REPAIR/REPLACEMENT
C. CONCRETE SIDEWALK REPLACEMENT
D. ROADWAY PAVEMENT REPAIR
E. ALTERING DRAINAGE STRUCTURES
F. ALTERING MANHOLES
G. REPLACEMENT OF WATER SERVICES
H. ADJUSTMENT OF VALVE BOXES WITHIN WORK LIMITS
I. REPLACEMENT OF TRAFFIC SIGNAL DETECTOR LOOPS
J. CITY OF ROCHESTER SURVEY MONUMENT LOCATIONS
K. PLACEMENT OF PAVEMENT MARKINGS
L. PLACEMENT OF SNOW PLOW MARKERS

ROADWAY PLAN LEGEND

- LIMITS OF SIDEWALK AND SIDEWALK CURB RAMP REPLACEMENTS. REFER TO SIDEWALK AND SIDEWALK CURB RAMP TABLES ON DWGS. MT-1, MT-2 AND MT-6 FOR ADDITIONAL INFORMATION.
- LIMITS OF THE ELMWOOD AVENUE COLLEGE TOWN CYCLE TRACK TO BE CONSTRUCTED UNDER SEPARATE CONTRACT D034682 IN THE SPRING OF 2019
- LIMITS OF HMA PAVEMENT MILLING AND RESURFACING. REFER TO MILLING AND PAVING TABLE ON DWG. MT-3 FOR ADDITIONAL INFORMATION.
- LIMITS OF FIELD OBSERVED SEVERE PAVEMENT DISTRESS IDENTIFIED FOR PAVEMENT REPAIRS. REFER TO PAVEMENT REPAIR TABLE ON DWGS. MT-4 AND MT-5 FOR ADDITIONAL INFORMATION.
- PAVEMENT CORE SYMBOL

SIDEWALK CURB RAMPS LEGEND

THE CONTRACTOR SHALL REFER TO THE SIDEWALK CURB RAMP TABLE ON DWGS. MT-1 AND MT-2 FOR ADDITIONAL INFORMATION CONCERNING THE DISPOSITION OF ALL SIDEALK CURB RAMPS WITHIN THE PROJECT LIMITS. THE CONTRACTOR SHALL REFER TO THE "PEDESTRIAN ACCESSIBILITY GUIDELINES" DWG. GN-3 FOR ADDITIONAL CONSTRUCTION NOTES.

CR XX — NEW DETECTABLE WARNING UNIT (DWU) TO BE INSTALLED.

CR XX — RAMP TO BE REPLACED

0 20 40 60

FILE NAME = Roadway Plans.dgn
DATE/TIME = 06-MAR-2019 11:55
USER = JMessenger

Lu Engineers
ENVIRONMENTAL • TRANSPORTATION • CIVIL

PROJECT MANAGER: J. OTTMAN
DRAWN BY: J. MESSENGER

Department of Environmental Services
Architecture and Engineering Services
City of Rochester, New York

ASSISTANT MANAGING ENGINEER: LISA Y. REYES
CITY ENGINEER: HOLLY E. BARRETT, P.E.

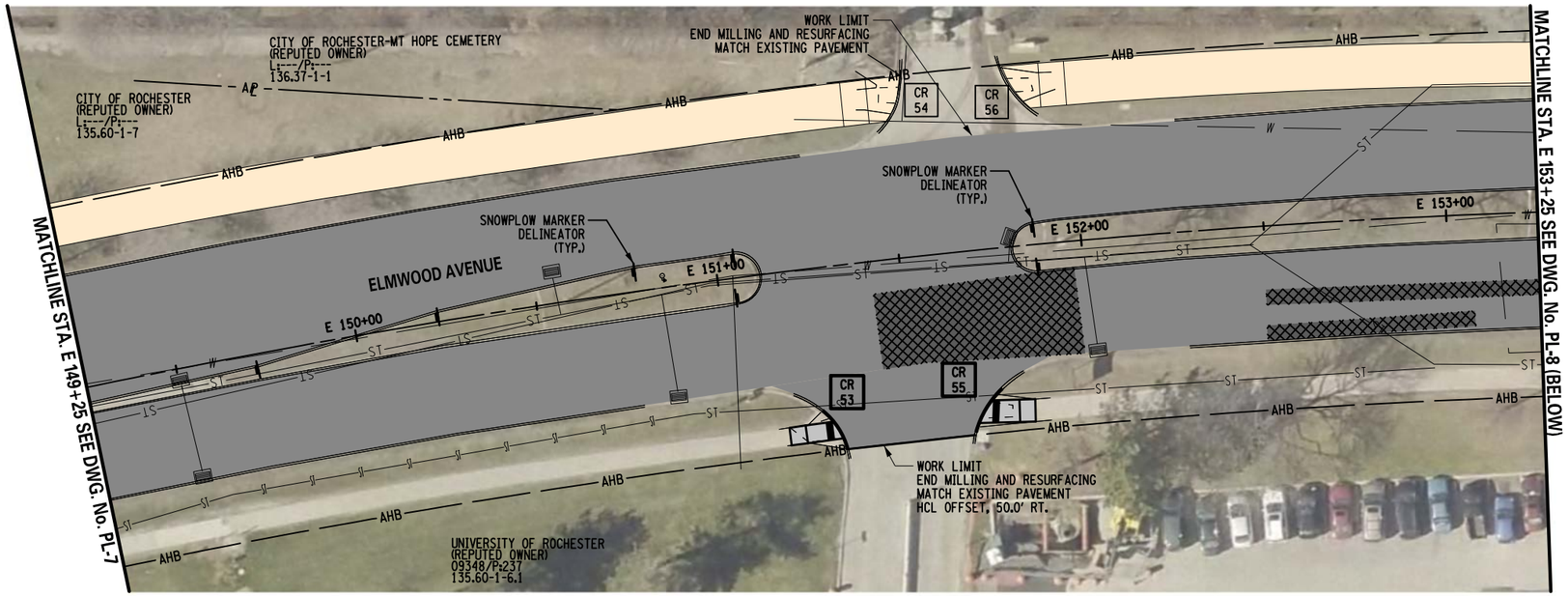
PROJECT TITLE
SCOTTSVILLE ROAD - ELMWOOD AVENUE
IMPROVEMENT PROJECT
(WEST CITY LINE - MT HOPE AVE)

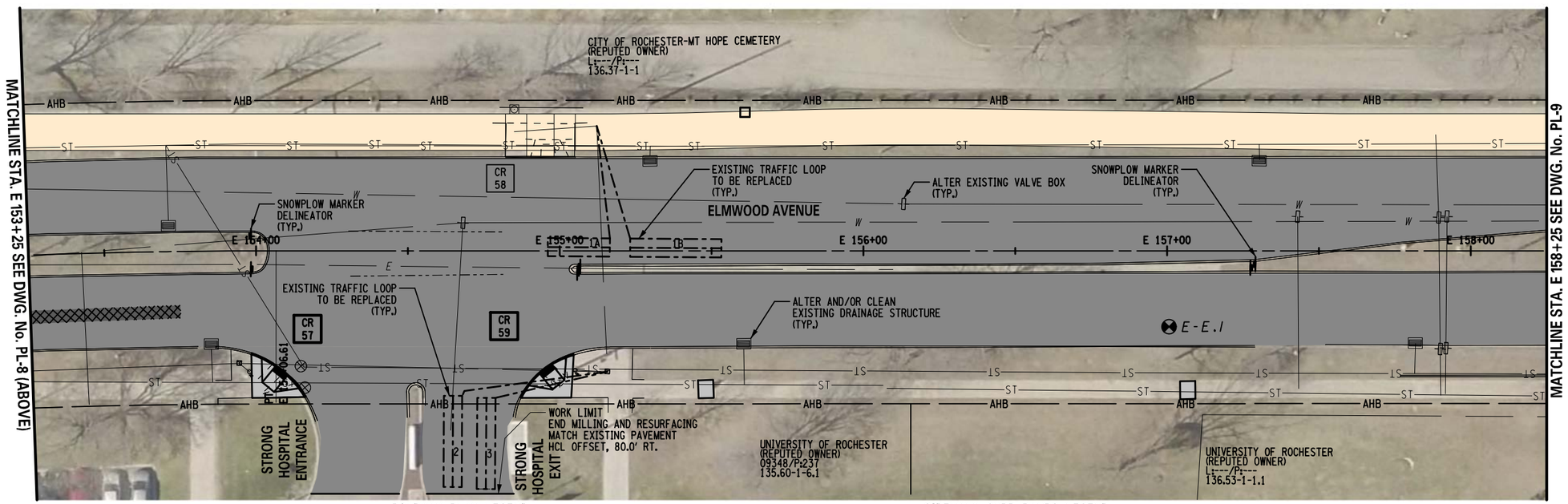
NO.	REVISION	BY	DATE

ISSUED 03/06/2019
CHECKED JWO
DRAWN JRM
DESIGN JRM
SCALE 1" = 20'
PROJECT NUMBER 17102

DRAWING TITLE
ROADWAY PLAN
PL-7

DRAWING NO.
41
OF
54





ROADWAY PLAN NOTES:

1. THE CONTRACTOR SHALL REFER TO THE GENERAL NOTES ON DWGS. GN-1 THRU GN-3, MISCELLANEOUS DETAILS ON DWGS. MD-1 THRU MD-10 AND MISCELLANEOUS TABLES ON DWGS. MT-1 THRU MT-10 FOR ADDITIONAL INFORMATION ON THE FOLLOWING PROJECT WORK ITEMS:

A. CURB RAMP REPLACEMENT
B. STONE CURB REPAIR/REPLACEMENT
C. CONCRETE SIDEWALK REPLACEMENT
D. ROADWAY PAVEMENT REPAIR
E. ALTERING DRAINAGE STRUCTURES
F. ALTERING MANHOLES
G. REPLACEMENT OF WATER SERVICES
H. ADJUSTMENT OF VALVE BOXES WITHIN WORK LIMITS
I. REPLACEMENT OF TRAFFIC SIGNAL DETECTOR LOOPS
J. CITY OF ROCHESTER SURVEY MONUMENT LOCATIONS
K. PLACEMENT OF PAVEMENT MARKINGS
L. PLACEMENT OF SNOW PLOW MARKERS

ROADWAY PLAN LEGEND

- LIMITS OF SIDEWALK AND SIDEWALK CURB RAMP REPLACEMENTS. REFER TO SIDEWALK AND SIDEWALK CURB RAMP TABLES ON DWGS. MT-1, MT-2 AND MT-6 FOR ADDITIONAL INFORMATION.
- LIMITS OF THE ELMWOOD AVENUE COLLEGE TOWN CYCLE TRACK TO BE CONSTRUCTED UNDER SEPARATE CONTRACT D034682 IN THE SPRING OF 2019
- LIMITS OF HMA PAVEMENT MILLING AND RESURFACING. REFER TO MILLING AND PAVING TABLE ON DWG. MT-3 FOR ADDITIONAL INFORMATION.
- LIMITS OF FIELD OBSERVED SEVERE PAVEMENT DISTRESS IDENTIFIED FOR PAVEMENT REPAIRS. REFER TO PAVEMENT REPAIR TABLE ON DWGS. MT-4 AND MT-5 FOR ADDITIONAL INFORMATION.
- PAVEMENT CORE SYMBOL

SIDEWALK CURB RAMPS LEGEND

THE CONTRACTOR SHALL REFER TO THE SIDEWALK CURB RAMP TABLE ON DWGS. MT-1 AND MT-2 FOR ADDITIONAL INFORMATION CONCERNING THE DISPOSITION OF ALL SIDEALK CURB RAMPS WITHIN THE PROJECT LIMITS. THE CONTRACTOR SHALL REFER TO THE "PEDESTRIAN ACCESSIBILITY GUIDELINES" DWG. GN-3 FOR ADDITIONAL CONSTRUCTION NOTES.

CR XX — NEW DETECTABLE WARNING UNIT (DWU) TO BE INSTALLED.

CR XX — RAMP TO BE REPLACED

0 20 40 60

Lu Engineers
ENVIRONMENTAL • TRANSPORTATION • CIVIL

PROJECT MANAGER: J. OTTMAN
DRAWN BY: J. MESSENGER

Department of Environmental Services
Architecture and Engineering Services
City of Rochester, New York

ASSISTANT MANAGING ENGINEER: LISA Y. REYES
CITY ENGINEER: HOLLY E. BARRETT, P.E.

PROJECT TITLE
SCOTTSVILLE ROAD - ELMWOOD AVENUE
IMPROVEMENT PROJECT
(WEST CITY LINE - MT HOPE AVE)

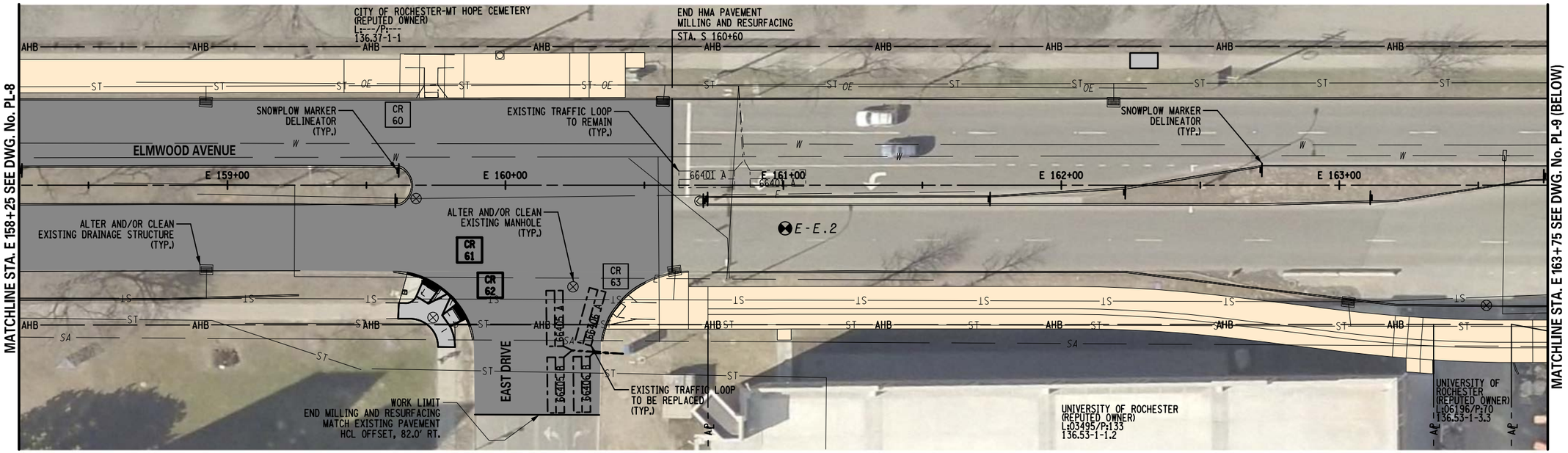
NO.	REVISION	BY	DATE

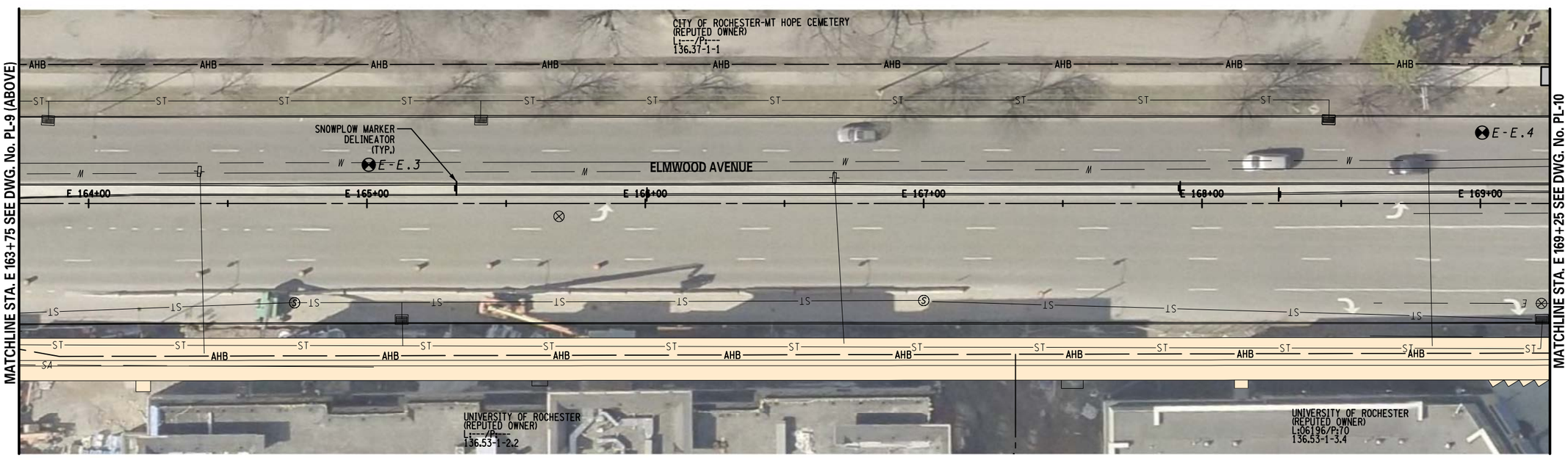
ISSUED 03/06/2019
CHECKED JWO
DRAWN JRM
DESIGN JRM
SCALE 1" = 20'
PROJECT NUMBER 17102

DRAWING TITLE
ROADWAY PLAN
PL-8

DRAWING NO. 42 OF 54

FILE NAME = Roadway Plans.dgn
DATE/TIME = 06-MAR-2019 11:56
USER = iMessenger





ROADWAY PLAN NOTES:

1. THE CONTRACTOR SHALL REFER TO THE GENERAL NOTES ON DWGS. GN-1 THRU GN-3, MISCELLANEOUS DETAILS ON DWGS. MD-1 THRU MD-10 AND MISCELLANEOUS TABLES ON DWGS. MT-1 THRU MT-10 FOR ADDITIONAL INFORMATION ON THE FOLLOWING PROJECT WORK ITEMS:

A. CURB RAMP REPLACEMENT
B. STONE CURB REPAIR/REPLACEMENT
C. CONCRETE SIDEWALK REPLACEMENT
D. ROADWAY PAVEMENT REPAIR
E. ALTERING DRAINAGE STRUCTURES
F. ALTERING MANHOLES
G. REPLACEMENT OF WATER SERVICES
H. ADJUSTMENT OF VALVE BOXES WITHIN WORK LIMITS
I. REPLACEMENT OF TRAFFIC SIGNAL DETECTOR LOOPS
J. CITY OF ROCHESTER SURVEY MONUMENT LOCATIONS
K. PLACEMENT OF PAVEMENT MARKINGS
L. PLACEMENT OF SNOW PLOW MARKERS

ROADWAY PLAN LEGEND

- LIMITS OF SIDEWALK AND SIDEWALK CURB RAMP REPLACEMENTS. REFER TO SIDEWALK AND SIDEWALK CURB RAMP TABLES ON DWGS. MT-1, MT-2 AND MT-6 FOR ADDITIONAL INFORMATION.
- LIMITS OF THE ELMWOOD AVENUE COLLEGE TOWN CYCLE TRACK TO BE CONSTRUCTED UNDER SEPARATE CONTRACT D034682 IN THE SPRING OF 2019
- LIMITS OF HMA PAVEMENT MILLING AND RESURFACING. REFER TO MILLING AND PAVING TABLE ON DWG. MT-3 FOR ADDITIONAL INFORMATION.
- LIMITS OF FIELD OBSERVED SEVERE PAVEMENT DISTRESS IDENTIFIED FOR PAVEMENT REPAIRS. REFER TO PAVEMENT REPAIR TABLE ON DWGS. MT-4 AND MT-5 FOR ADDITIONAL INFORMATION.
- PAVEMENT CORE SYMBOL

SIDEWALK CURB RAMPS LEGEND

THE CONTRACTOR SHALL REFER TO THE SIDEWALK CURB RAMP TABLE ON DWGS. MT-1 AND MT-2 FOR ADDITIONAL INFORMATION CONCERNING THE DISPOSITION OF ALL SIDEALK CURB RAMPS WITHIN THE PROJECT LIMITS. THE CONTRACTOR SHALL REFER TO THE "PEDESTRIAN ACCESSIBILITY GUIDELINES" DWG. GN-3 FOR ADDITIONAL CONSTRUCTION NOTES.

CR XX — NEW DETECTABLE WARNING UNIT (DWU) TO BE INSTALLED.

CR XX — RAMP TO BE REPLACED

0 20 40 60

Lu Engineers
ENVIRONMENTAL • TRANSPORTATION • CIVIL

PROJECT MANAGER: J. OTTMAN
DRAWN BY: J. MESSENGER

Department of Environmental Services
Architecture and Engineering Services
City of Rochester, New York

ASSISTANT MANAGING ENGINEER: LISA Y. REYES
CITY ENGINEER: HOLLY E. BARRETT, P.E.

PROJECT TITLE
SCOTTSVILLE ROAD - ELMWOOD AVENUE
IMPROVEMENT PROJECT
(WEST CITY LINE - MT HOPE AVE)

NO.	REVISION	BY	DATE

ISSUED 03/06/2019
CHECKED JWO
DRAWN JRM
DESIGN JRM
SCALE 1" = 20'
PROJECT NUMBER 17102

DRAWING TITLE
ROADWAY PLAN
PL-9

DRAWING NO. 43
OF 54

FILE NAME = Roadway Plans.dgn
DATE/TIME = 06-MAR-2019 11:56
USER = JMessenger

MATCHLINE STA. E 169+25 SEE DWG. No. PL-9

ROADWAY PLAN NOTES:

1. THE CONTRACTOR SHALL REFER TO THE GENERAL NOTES ON DWGS. GN-1 THRU GN-3, MISCELLANEOUS DETAILS ON DWGS. MD-1 THRU MD-10 AND MISCELLANEOUS TABLES ON DWGS. MT-1 THRU MT-10 FOR ADDITIONAL INFORMATION ON THE FOLLOWING PROJECT WORK ITEMS:

A. CURB RAMP REPLACEMENT
B. STONE CURB REPAIR/REPLACEMENT
C. CONCRETE SIDEWALK REPLACEMENT
D. ROADWAY PAVEMENT REPAIR
E. ALTERING DRAINAGE STRUCTURES
F. ALTERING MANHOLES
G. REPLACEMENT OF WATER SERVICES
H. ADJUSTMENT OF VALVE BOXES WITHIN WORK LIMITS
I. REPLACEMENT OF TRAFFIC SIGNAL DETECTOR LOOPS
J. CITY OF ROCHESTER SURVEY MONUMENT LOCATIONS
K. PLACEMENT OF PAVEMENT MARKINGS
L. PLACEMENT OF SNOW PLOW MARKERS

ROADWAY PLAN LEGEND

- LIMITS OF SIDEWALK AND SIDEWALK CURB RAMP REPLACEMENTS. REFER TO SIDEWALK AND SIDEWALK CURB RAMP TABLES ON DWGS. MT-1, MT-2 AND MT-6 FOR ADDITIONAL INFORMATION.
- LIMITS OF THE ELMWOOD AVENUE COLLEGE TOWN CYCLE TRACK TO BE CONSTRUCTED UNDER SEPARATE CONTRACT D034682 IN THE SPRING OF 2019
- LIMITS OF HMA PAVEMENT MILLING AND RESURFACING. REFER TO MILLING AND PAVING TABLE ON DWG. MT-3 FOR ADDITIONAL INFORMATION.
- LIMITS OF FIELD OBSERVED SEVERE PAVEMENT DISTRESS IDENTIFIED FOR PAVEMENT REPAIRS. REFER TO PAVEMENT REPAIR TABLE ON DWGS. MT-4 AND MT-5 FOR ADDITIONAL INFORMATION.
- PAVEMENT CORE SYMBOL

SIDEWALK CURB RAMPS LEGEND

THE CONTRACTOR SHALL REFER TO THE SIDEWALK CURB RAMP TABLE ON DWGS. MT-1 AND MT-2 FOR ADDITIONAL INFORMATION CONCERNING THE DISPOSITION OF ALL SIDEALK CURB RAMPS WITHIN THE PROJECT LIMITS. THE CONTRACTOR SHALL REFER TO THE "PEDESTRIAN ACCESSIBILITY GUIDELINES" DWG. GN-3 FOR ADDITIONAL CONSTRUCTION NOTES.

- CR XX — NEW DETECTABLE WARNING UNIT (DWU) TO BE INSTALLED.
- CR XX — RAMP TO BE REPLACED

0 20 40 60

ROADWAY PLAN NOTES:

FILE NAME = Roadway Plans.dgn
DATE/TIME = 06-MAR-2019 11:56
USER = JMessenger

Lu Engineers
ENVIRONMENTAL • TRANSPORTATION • CIVIL

PROJECT MANAGER: J. OTTMAN
DRAWN BY: J. MESSENGER

Department of Environmental Services
Architecture and Engineering Services
City of Rochester, New York

ASSISTANT MANAGING ENGINEER: LISA Y. REYES
CITY ENGINEER: HOLLY E. BARRETT, P.E.

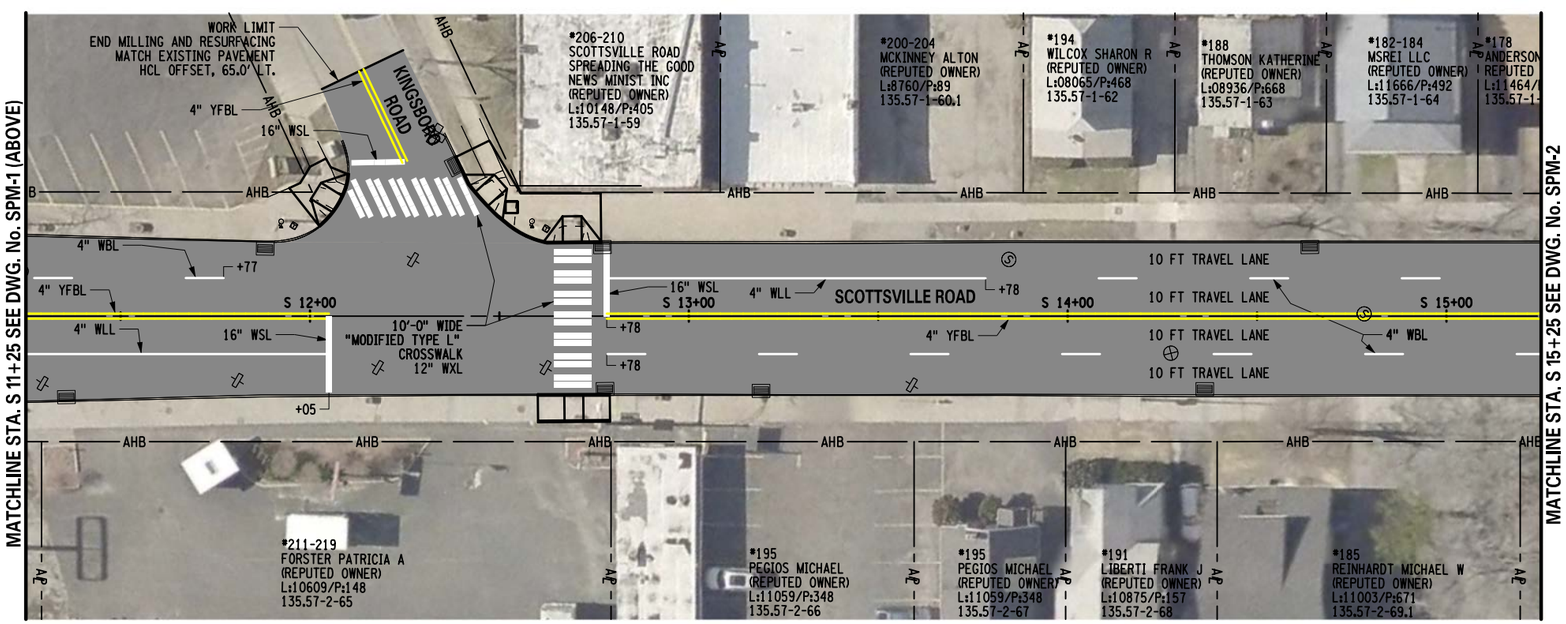
PROJECT TITLE
SCOTTSVILLE ROAD - ELMWOOD AVENUE
IMPROVEMENT PROJECT
(WEST CITY LINE - MT HOPE AVE)

NO.	REVISION	BY	DATE

ISSUED 03/06/2019
CHECKED JWO
DRAWN JRM
DESIGN JRM
SCALE 1" = 20'
PROJECT NUMBER 17102

DRAWING TITLE
ROADWAY PLAN
PL-10

DRAWING NO.
44
OF
54



PAVEMENT MARKING PAY ITEMS:

1. ITEM 685.11 - WHITE EPOXY REFLECTORIZED PAVEMENT STRIPES - 20MILS
   - WLL - WHITE LANE LINE
   - WBL - WHITE BROKEN LINE
   - WDLL - WHITE DOTTED LANE LINE
   - WDEL - WHITE DOTTED EXTENSION LINE
   - WHL - WHITE HATCH LINE
   - WEL - WHITE EDGE LINE
2. ITEM 685.12 - YELLOW EPOXY REFLECTORIZED PAVEMENT STRIPES - 20MILS
   - YEL - YELLOW EDGE LINE
   - YFBL - YELLOW FULL BARRIER LINE
   - YHL - YELLOW HATCH LINE
   - YPBL - YELLOW PARTIAL BARRIER LINE
3. ITEM 686.01000011 - WHITE PREFORMED THERMOPLASTIC REFLECTORIZED PAVEMENT STRIPE
   - WSL - WHITE STOP LINE
   - WXL - WHITE CROSSWALK LINE
4. ITEM 686.03000011 - WHITE PREFORMED THERMOPLASTIC REFLECTORIZED PAVEMENT LETTERS
   - WL - WHITE LETTER
5. ITEM 686.04000011 - WHITE PREFORMED THERMOPLASTIC REFLECTORIZED PAVEMENT SYMBOLS
   - WAS - WHITE ARROW SYMBOL
   - WYS - WHITE YIELD SYMBOL

PAVEMENT MARKING PLAN LEGEND

LIMITS OF HMA PAVEMENT MILLING AND RESURFACING

0 20 40 60

FILE NAME = Striping Plans.dgn
DATE/TIME = 08-MAR-2019 11:57
USER = JMessenger

Lu Engineers
ENVIRONMENTAL • TRANSPORTATION • CIVIL

PROJECT MANAGER: J. OTTMAN
DRAWN BY: J. MESSENGER

Department of Environmental Services
Architecture and Engineering Services
City of Rochester, New York

ASSISTANT MANAGING ENGINEER: LISA Y. REYES
CITY ENGINEER: HOLLY E. BARRETT, P.E.

PROJECT TITLE
SCOTTSVILLE ROAD - ELMWOOD AVENUE
IMPROVEMENT PROJECT
(WEST CITY LINE - MT HOPE AVE)

NO.	REVISION	BY	DATE

ISSUED 03/06/2019
CHECKED JWO
DRAWN JRM
DESIGN JRM
SCALE 1" = 20'
PROJECT NUMBER 17102

DRAWING TITLE
PAVEMENT MARKING PLAN
PMP-1

DRAWING NO. 45
OF 54

PAVEMENT MARKING PAY ITEMS:

1. ITEM 685.11 - WHITE EPOXY REFLECTORIZED PAVEMENT STRIPES - 20MILS
   - WLL - WHITE LANE LINE
   - WBL - WHITE BROKEN LINE
   - WDLL - WHITE DOTTED LANE LINE
   - WDEL - WHITE DOTTED EXTENSION LINE
   - WHL - WHITE HATCH LINE
   - WEL - WHITE EDGE LINE
2. ITEM 685.12 - YELLOW EPOXY REFLECTORIZED PAVEMENT STRIPES - 20MILS
   - YEL - YELLOW EDGE LINE
   - YFBL - YELLOW FULL BARRIER LINE
   - YHL - YELLOW HATCH LINE
   - YPBL - YELLOW PARTIAL BARRIER LINE
3. ITEM 686.01000011 - WHITE PREFORMED THERMOPLASTIC REFLECTORIZED PAVEMENT STRIPE
   - WSL - WHITE STOP LINE
   - WXL - WHITE CROSSWALK LINE
4. ITEM 686.03000011 - WHITE PREFORMED THERMOPLASTIC REFLECTORIZED PAVEMENT LETTERS
   - WL - WHITE LETTER
5. ITEM 686.04000011 - WHITE PREFORMED THERMOPLASTIC REFLECTORIZED PAVEMENT SYMBOLS
   - WAS - WHITE ARROW SYMBOL
   - WYS - WHITE YIELD SYMBOL

PAVEMENT MARKING PLAN LEGEND

LIMITS OF HMA PAVEMENT MILLING AND RESURFACING

0 20 40 60

Lu Engineers
ENVIRONMENTAL • TRANSPORTATION • CIVIL

PROJECT MANAGER: J. OTTMAN
DRAWN BY: J. MESSENGER

Department of Environmental Services
Architecture and Engineering Services
City of Rochester, New York

ASSISTANT MANAGING ENGINEER: LISA Y. REYES
CITY ENGINEER: HOLLY E. BARRETT, P.E.

PROJECT TITLE
SCOTTSVILLE ROAD - ELMWOOD AVENUE
IMPROVEMENT PROJECT
(WEST CITY LINE - MT HOPE AVE)

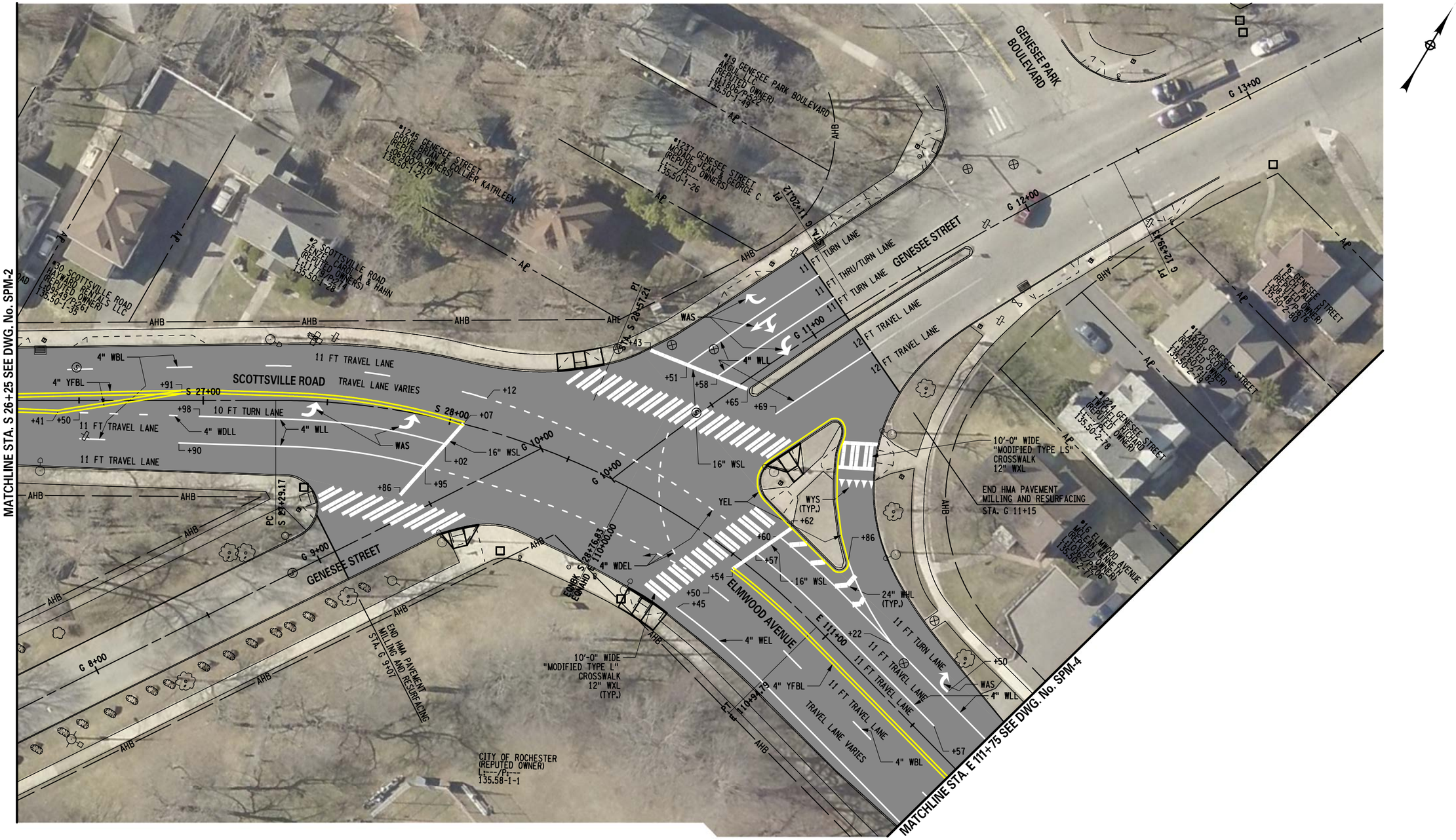
NO.	REVISION	BY	DATE

ISSUED 03/06/2019
CHECKED JWO
DRAWN JRM
DESIGN JRM
SCALE 1" = 20'
PROJECT NUMBER 17102

DRAWING TITLE
PAVEMENT
MARKING PLAN
PMP-2

DRAWING NO. 46
OF 54

FILE NAME = Striping Plans.dgn
DATE/TIME = 06-MAR-2019 11:57
USER = JMessenger



PAVEMENT MARKING PAY ITEMS:

1. ITEM 685.11 - WHITE EPOXY REFLECTORIZED PAVEMENT STRIPES - 20MILS
   - WLL - WHITE LANE LINE
   - WBL - WHITE BROKEN LINE
   - WDLL - WHITE DOTTED LANE LINE
   - WDEL - WHITE DOTTED EXTENSION LINE
   - WHL - WHITE HATCH LINE
   - WEL - WHITE EDGE LINE
2. ITEM 685.12 - YELLOW EPOXY REFLECTORIZED PAVEMENT STRIPES - 20MILS
   - YEL - YELLOW EDGE LINE
   - YFBL - YELLOW FULL BARRIER LINE
   - YHL - YELLOW HATCH LINE
   - YPBL - YELLOW PARTIAL BARRIER LINE
3. ITEM 686.01000011 - WHITE PREFORMED THERMOPLASTIC REFLECTORIZED PAVEMENT STRIPE
   - WSL - WHITE STOP LINE
   - WXL - WHITE CROSSWALK LINE
4. ITEM 686.03000011 - WHITE PREFORMED THERMOPLASTIC REFLECTORIZED PAVEMENT LETTERS
   - WL - WHITE LETTER
5. ITEM 686.04000011 - WHITE PREFORMED THERMOPLASTIC REFLECTORIZED PAVEMENT SYMBOLS
   - WAS - WHITE ARROW SYMBOL
   - WYS - WHITE YIELD SYMBOL

PAVEMENT MARKING PLAN LEGEND

LIMITS OF HMA PAVEMENT MILLING AND RESURFACING

0 20 40 60

Lu Engineers
ENVIRONMENTAL • TRANSPORTATION • CIVIL

PROJECT MANAGER: J. OTTMAN
DRAWN BY: J. MESSENGER

Department of Environmental Services
Architecture and Engineering Services
City of Rochester, New York

ASSISTANT MANAGING ENGINEER: LISA Y. REYES
CITY ENGINEER: HOLLY E. BARRETT, P.E.

PROJECT TITLE
SCOTTSVILLE ROAD - ELMWOOD AVENUE IMPROVEMENT PROJECT (WEST CITY LINE - MT HOPE AVE)

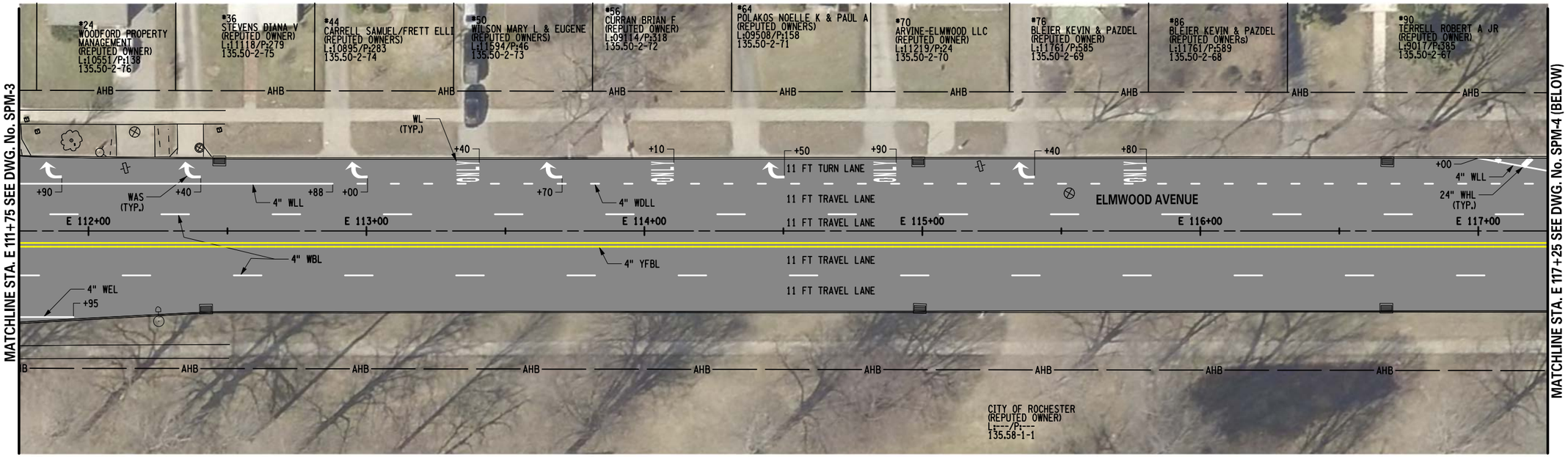
NO.	REVISION	BY	DATE

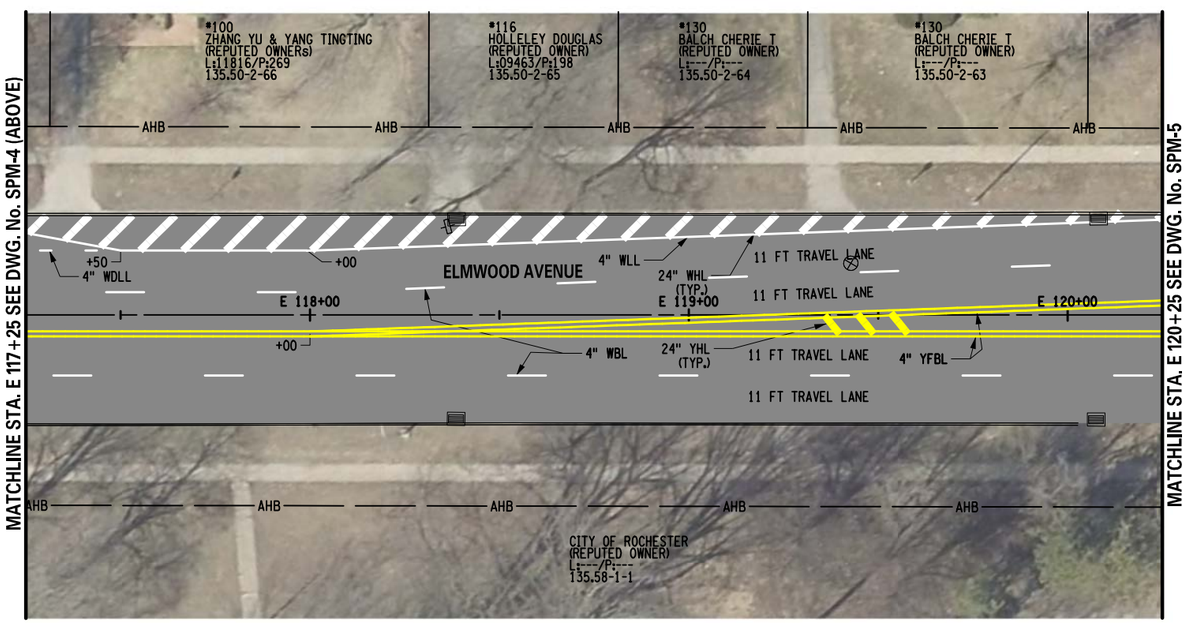
ISSUED 03/06/2019
CHECKED JWO
DRAWN JRM
DESIGN JRM
SCALE 1" = 20'
PROJECT NUMBER 17102

DRAWING TITLE
PAVEMENT MARKING PLAN PMP-3

DRAWING NO. 47 OF 54

FILE NAME = Striping Plans.dgn
DATE/TIME = 06-MAR-2019 11:58
USER = JMessenger





PAVEMENT MARKING PAY ITEMS:

1. ITEM 685.11 - WHITE EPOXY REFLECTORIZED PAVEMENT STRIPES - 20MILS
   - WLL - WHITE LANE LINE
   - WBL - WHITE BROKEN LINE
   - WDLL - WHITE DOTTED LANE LINE
   - WDEL - WHITE DOTTED EXTENSION LINE
   - WHL - WHITE HATCH LINE
   - WEL - WHITE EDGE LINE
2. ITEM 685.12 - YELLOW EPOXY REFLECTORIZED PAVEMENT STRIPES - 20MILS
   - YEL - YELLOW EDGE LINE
   - YFBL - YELLOW FULL BARRIER LINE
   - YHL - YELLOW HATCH LINE
   - YPBL - YELLOW PARTIAL BARRIER LINE
3. ITEM 686.01000011 - WHITE PREFORMED THERMOPLASTIC REFLECTORIZED PAVEMENT STRIPE
   - WSL - WHITE STOP LINE
   - WXL - WHITE CROSSWALK LINE
4. ITEM 686.03000011 - WHITE PREFORMED THERMOPLASTIC REFLECTORIZED PAVEMENT LETTERS
   - WL - WHITE LETTER
5. ITEM 686.04000011 - WHITE PREFORMED THERMOPLASTIC REFLECTORIZED PAVEMENT SYMBOLS
   - WAS - WHITE ARROW SYMBOL
   - WYS - WHITE YIELD SYMBOL

PAVEMENT MARKING PLAN LEGEND

LIMITS OF HMA PAVEMENT MILLING AND RESURFACING

0 20 40 60

FILE NAME = Striping Plans.dgn
DATE/TIME = 06-MAR-2019 11:58
USER = JMessenger

Lu Engineers
ENVIRONMENTAL • TRANSPORTATION • CIVIL

PROJECT MANAGER: J. OTTMAN
DRAWN BY: J. MESSENGER

Department of Environmental Services
Architecture and Engineering Services
City of Rochester, New York

ASSISTANT MANAGING ENGINEER: LISA Y. REYES
CITY ENGINEER: HOLLY E. BARRETT, P.E.

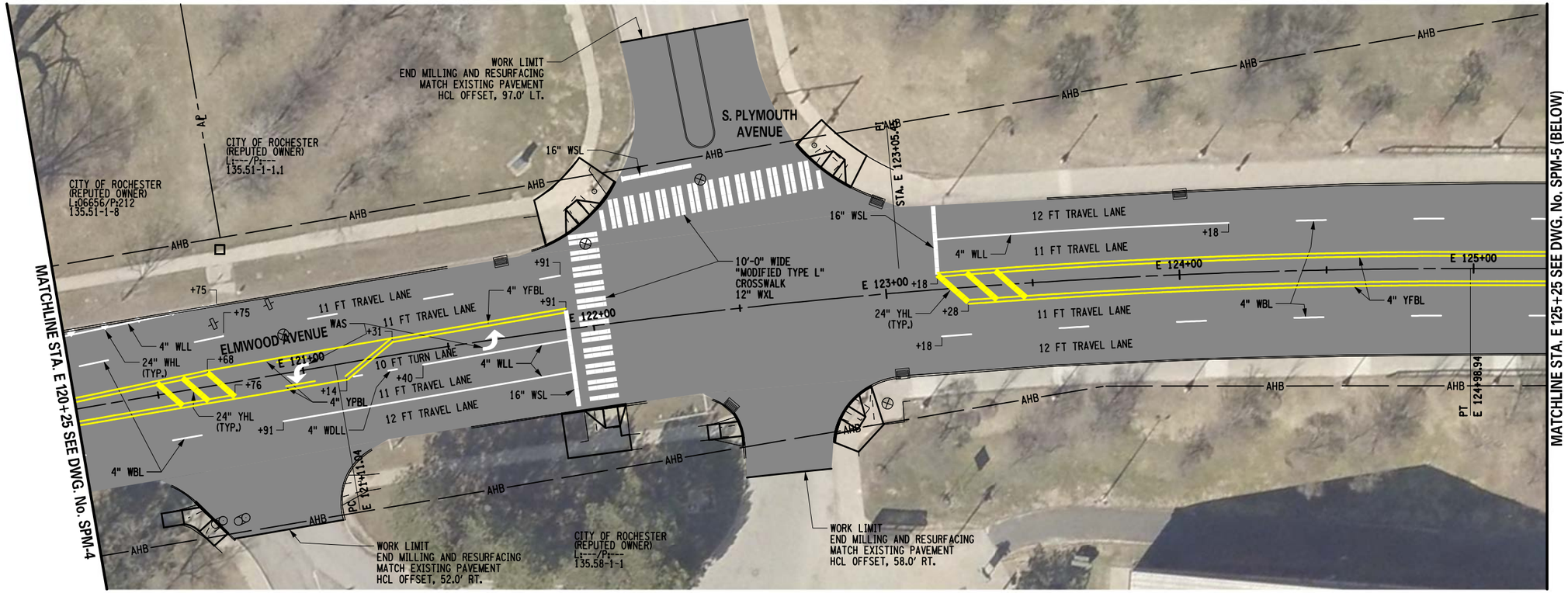
PROJECT TITLE
SCOTTSVILLE ROAD - ELMWOOD AVENUE IMPROVEMENT PROJECT (WEST CITY LINE - MT HOPE AVE)

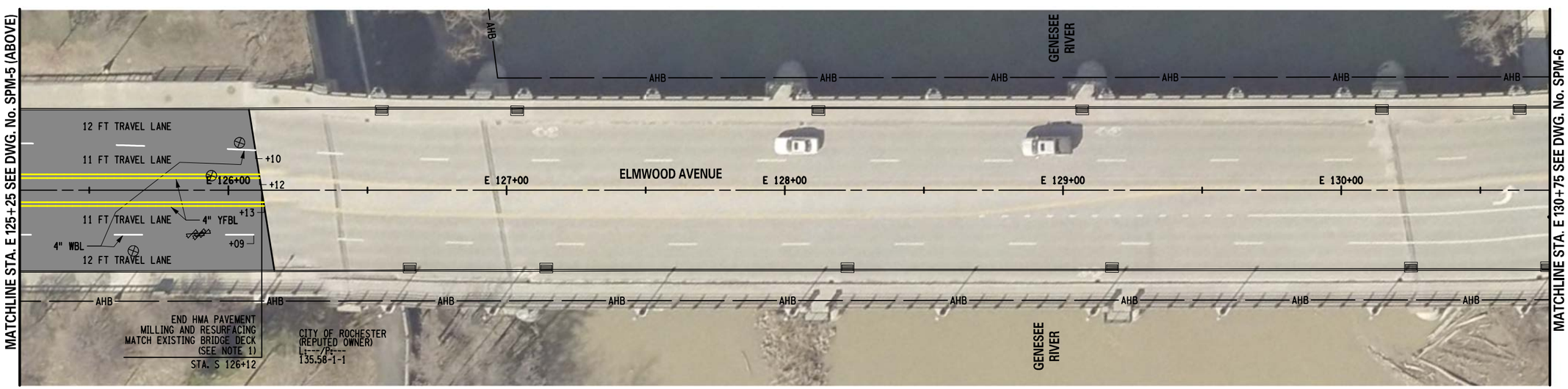
NO.	REVISION	BY	DATE

ISSUED 03/06/2019
CHECKED JWO
DRAWN JRM
DESIGN JRM
SCALE 1" = 20'
PROJECT NUMBER 17102

DRAWING TITLE
PAVEMENT MARKING PLAN PMP-4

DRAWING NO. 48 OF 54





PAVEMENT MARKING PAY ITEMS:

1. ITEM 685.11 - WHITE EPOXY REFLECTORIZED PAVEMENT STRIPES - 20MILS
   - WLL - WHITE LANE LINE
   - WBL - WHITE BROKEN LINE
   - WDLL - WHITE DOTTED LANE LINE
   - WDEL - WHITE DOTTED EXTENSION LINE
   - WHL - WHITE HATCH LINE
   - WEL - WHITE EDGE LINE
2. ITEM 685.12 - YELLOW EPOXY REFLECTORIZED PAVEMENT STRIPES - 20MILS
   - YEL - YELLOW EDGE LINE
   - YFBL - YELLOW FULL BARRIER LINE
   - YHL - YELLOW HATCH LINE
   - YPBL - YELLOW PARTIAL BARRIER LINE
3. ITEM 686.01000011 - WHITE PREFORMED THERMOPLASTIC REFLECTORIZED PAVEMENT STRIPE
   - WSL - WHITE STOP LINE
   - WXL - WHITE CROSSWALK LINE
4. ITEM 686.03000011 - WHITE PREFORMED THERMOPLASTIC REFLECTORIZED PAVEMENT LETTERS
   - WL - WHITE LETTER
5. ITEM 686.04000011 - WHITE PREFORMED THERMOPLASTIC REFLECTORIZED PAVEMENT SYMBOLS
   - WAS - WHITE ARROW SYMBOL
   - WYS - WHITE YIELD SYMBOL

PAVEMENT MARKING PLAN LEGEND

LIMITS OF HMA PAVEMENT MILLING AND RESURFACING

0 20 40 60

FILE NAME = Striping Plans.dgn
DATE/TIME = 06-MAR-2019 11:59
USER = JMessenger

Lu Engineers
ENVIRONMENTAL • TRANSPORTATION • CIVIL

PROJECT MANAGER: J. OTTMAN
DRAWN BY: J. MESSENGER

Department of Environmental Services
Architecture and Engineering Services
City of Rochester, New York

ASSISTANT MANAGING ENGINEER: LISA Y. REYES
CITY ENGINEER: HOLLY E. BARRETT, P.E.

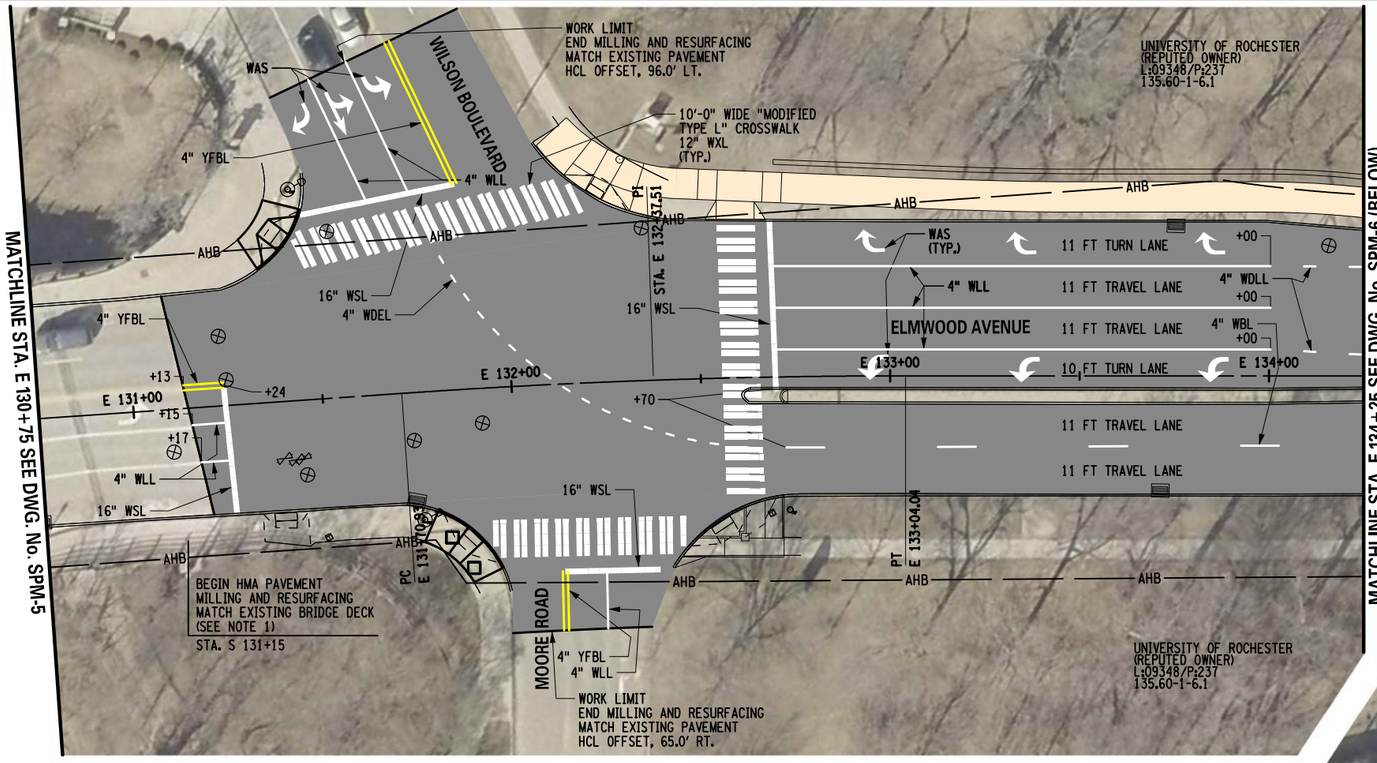
PROJECT TITLE
SCOTTSVILLE ROAD - ELMWOOD AVENUE
IMPROVEMENT PROJECT
(WEST CITY LINE - MT HOPE AVE)

NO.	REVISION	BY	DATE

ISSUED 03/06/2019
CHECKED JWO
DRAWN JRM
DESIGN JRM
SCALE 1" = 20'
PROJECT NUMBER 17102

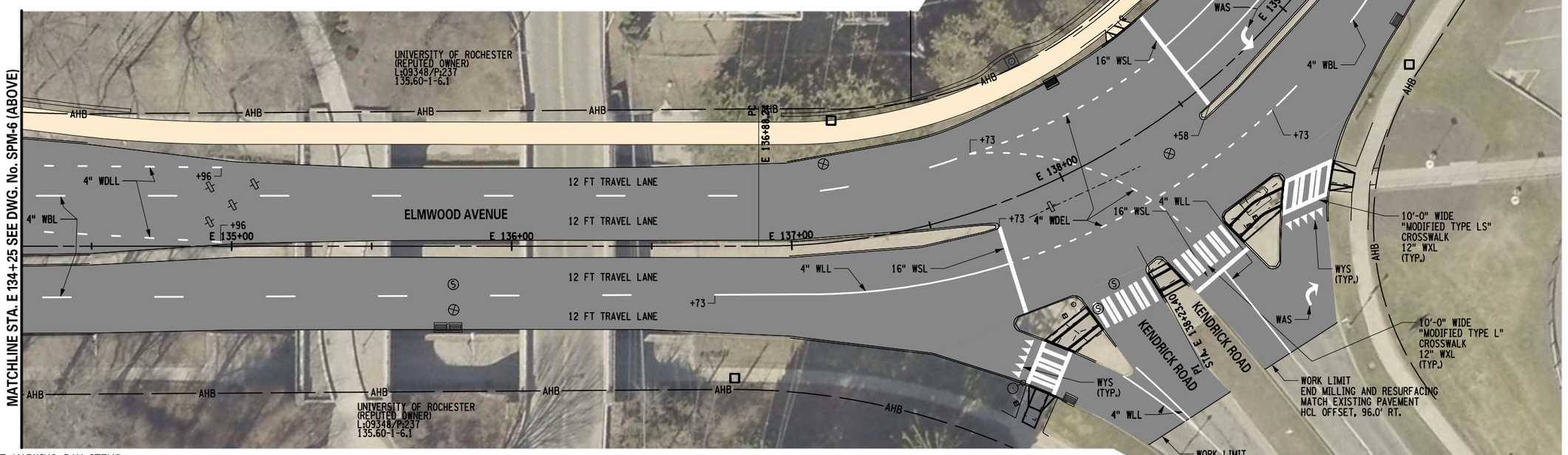
DRAWING TITLE
PAVEMENT MARKING PLAN
PMP-5

DRAWING NO. 49 OF 54



PAVEMENT MARKING PLAN LEGEND

- LIMITS OF THE ELMWOOD AVENUE COLLEGE TOWN CYCLE TRACK TO BE CONSTRUCTED UNDER SEPARATE CONTRACT D034682 IN THE SPRING OF 2019
- LIMITS OF HMA PAVEMENT MILLING AND RESURFACING



PAVEMENT MARKING PAY ITEMS:

1. ITEM 685.11 - WHITE EPOXY REFLECTORIZED PAVEMENT STRIPES - 20MILS
   - WLL - WHITE LANE LINE
   - WBL - WHITE BROKEN LINE
   - WDLL - WHITE DOTTED LANE LINE
   - WDEL - WHITE DOTTED EXTENSION LINE
   - WHL - WHITE HATCH LINE
   - WEL - WHITE EDGE LINE
2. ITEM 685.12 - YELLOW EPOXY REFLECTORIZED PAVEMENT STRIPES - 20MILS
   - YEL - YELLOW EDGE LINE
   - YFBL - YELLOW FULL BARRIER LINE
   - YHL - YELLOW HATCH LINE
   - YPBL - YELLOW PARTIAL BARRIER LINE
3. ITEM 686.01000011 - WHITE PREFORMED THERMOPLASTIC REFLECTORIZED PAVEMENT STRIPE
   - WSL - WHITE STOP LINE
   - WXL - WHITE CROSSWALK LINE
4. ITEM 686.03000011 - WHITE PREFORMED THERMOPLASTIC REFLECTORIZED PAVEMENT LETTERS
   - WL - WHITE LETTER
5. ITEM 686.04000011 - WHITE PREFORMED THERMOPLASTIC REFLECTORIZED PAVEMENT SYMBOLS
   - WAS - WHITE ARROW SYMBOL
   - WYS - WHITE YIELD SYMBOL

0 20 40 60

Lu Engineers
ENVIRONMENTAL • TRANSPORTATION • CIVIL

PROJECT MANAGER: J. OTTMAN
DRAWN BY: J. MESSENGER

Department of Environmental Services
Architecture and Engineering Services
City of Rochester, New York

ASSISTANT MANAGING ENGINEER: LISA Y. REYES
CITY ENGINEER: HOLLY E. BARRETT, P.E.

PROJECT TITLE
SCOTTSVILLE ROAD - ELMWOOD AVENUE
IMPROVEMENT PROJECT
(WEST CITY LINE - MT HOPE AVE)

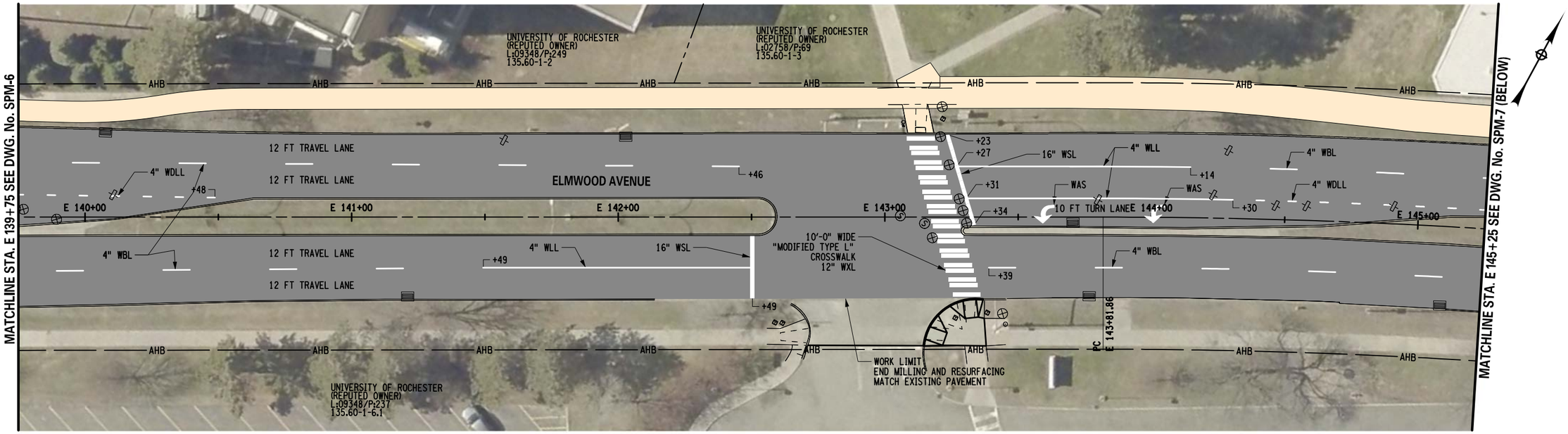
NO.	REVISION	BY	DATE

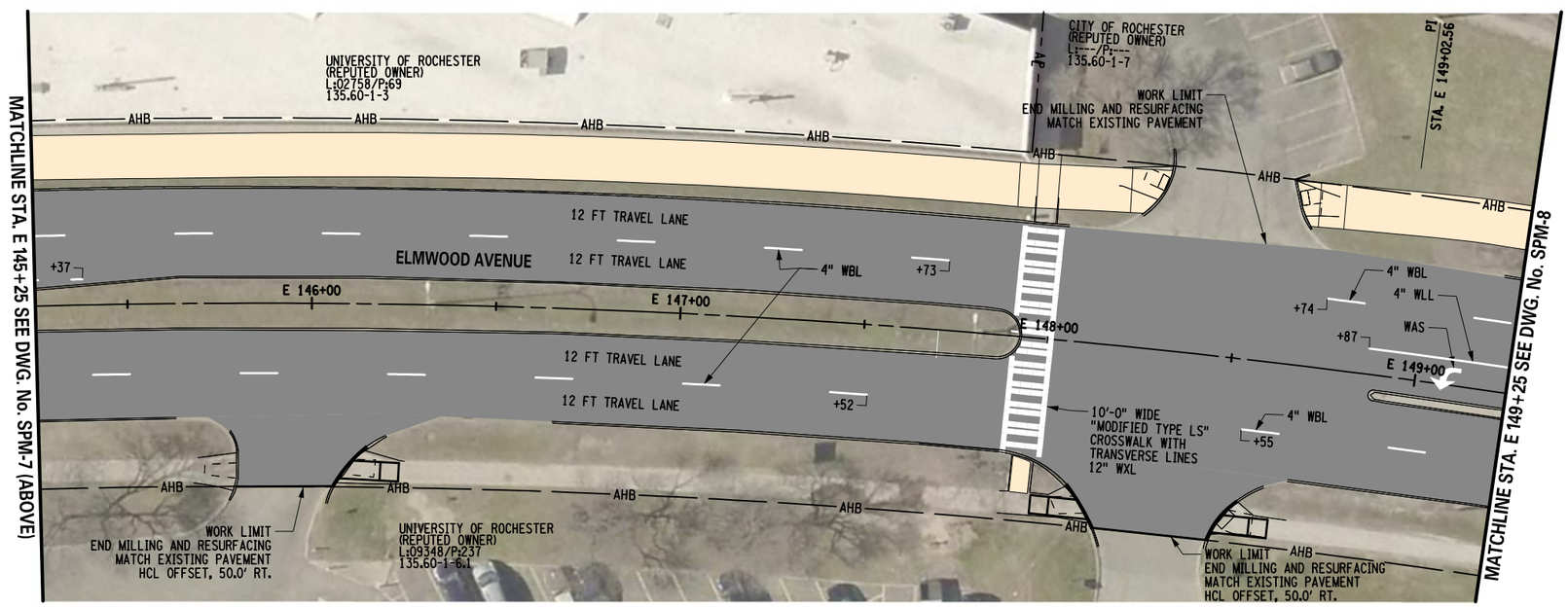
ISSUED 03/06/2019
CHECKED JWO
DRAWN JRM
DESIGN JRM
SCALE 1" = 20'
PROJECT NUMBER 17102

DRAWING TITLE
PAVEMENT MARKING PLAN
PMP-6

DRAWING NO.
50
OF
54

FILE NAME = Striping Plans.dgn
DATE/TIME = 06-MAR-2019 11:59
USER = JMessenger





PAVEMENT MARKING PAY ITEMS:

1. ITEM 685.11 - WHITE EPOXY REFLECTORIZED PAVEMENT STRIPES - 20MILS
   - WLL - WHITE LANE LINE
   - WBL - WHITE BROKEN LINE
   - WDLL - WHITE DOTTED LANE LINE
   - WDEL - WHITE DOTTED EXTENSION LINE
   - WHL - WHITE HATCH LINE
   - WEL - WHITE EDGE LINE
2. ITEM 685.12 - YELLOW EPOXY REFLECTORIZED PAVEMENT STRIPES - 20MILS
   - YEL - YELLOW EDGE LINE
   - YFBL - YELLOW FULL BARRIER LINE
   - YHL - YELLOW HATCH LINE
   - YPBL - YELLOW PARTIAL BARRIER LINE
3. ITEM 686.01000011 - WHITE PREFORMED THERMOPLASTIC REFLECTORIZED PAVEMENT STRIPE
   - WSL - WHITE STOP LINE
   - WXL - WHITE CROSSWALK LINE
4. ITEM 686.03000011 - WHITE PREFORMED THERMOPLASTIC REFLECTORIZED PAVEMENT LETTERS
   - WL - WHITE LETTER
5. ITEM 686.04000011 - WHITE PREFORMED THERMOPLASTIC REFLECTORIZED PAVEMENT SYMBOLS
   - WAS - WHITE ARROW SYMBOL
   - WYS - WHITE YIELD SYMBOL

PAVEMENT MARKING PLAN LEGEND

- LIMITS OF THE ELMWOOD AVENUE COLLEGE TOWN CYCLE TRACK TO BE CONSTRUCTED UNDER SEPARATE CONTRACT D034682 IN THE SPRING OF 2019
- LIMITS OF HMA PAVEMENT MILLING AND RESURFACING

0 20 40 60

Lu Engineers
ENVIRONMENTAL • TRANSPORTATION • CIVIL

PROJECT MANAGER: J. OTTMAN
DRAWN BY: J. MESSENGER

Department of Environmental Services
Architecture and Engineering Services
City of Rochester, New York

ASSISTANT MANAGING ENGINEER: LISA Y. REYES
CITY ENGINEER: HOLLY E. BARRETT, P.E.

PROJECT TITLE
SCOTTSVILLE ROAD - ELMWOOD AVENUE IMPROVEMENT PROJECT
(WEST CITY LINE - MT HOPE AVE)

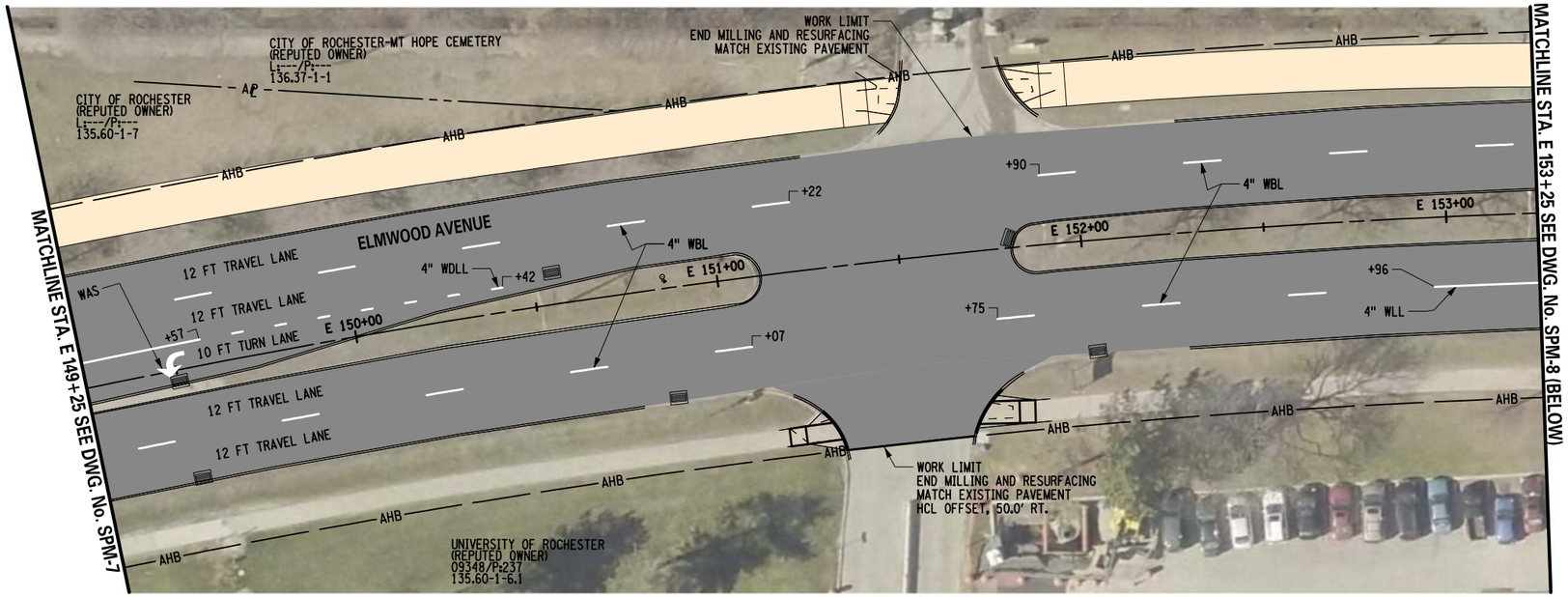
NO.	REVISION	BY	DATE

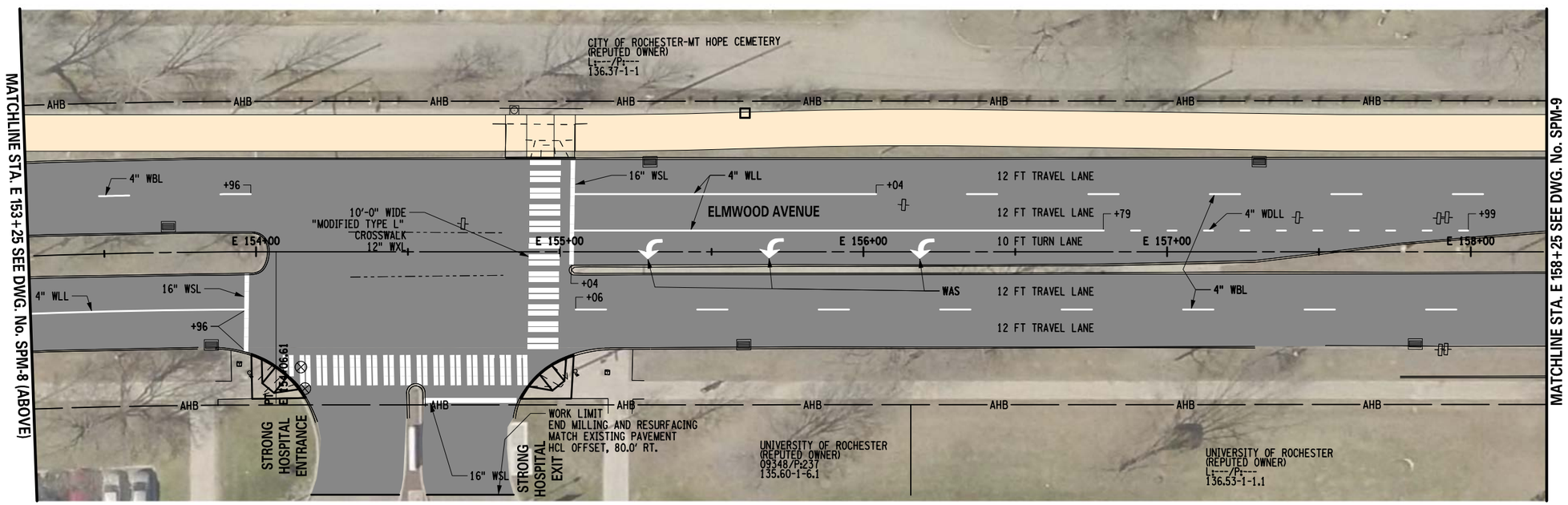
ISSUED 03/06/2019
CHECKED JWO
DRAWN JRM
DESIGN JRM
SCALE 1" = 20'
PROJECT NUMBER 17102

DRAWING TITLE
PAVEMENT MARKING PLAN PMP-7

DRAWING NO. 51 OF 54

FILE NAME = Striping Plans.dgn
DATE/TIME = 06-MAR-2019 12:00
USER = JMessenger





PAVEMENT MARKING PAY ITEMS:

1. ITEM 685.11 - WHITE EPOXY REFLECTORIZED PAVEMENT STRIPES - 20MILS
   - WLL - WHITE LANE LINE
   - WBL - WHITE BROKEN LINE
   - WDLL - WHITE DOTTED LANE LINE
   - WDEL - WHITE DOTTED EXTENSION LINE
   - WHL - WHITE HATCH LINE
   - WEL - WHITE EDGE LINE
2. ITEM 685.12 - YELLOW EPOXY REFLECTORIZED PAVEMENT STRIPES - 20MILS
   - YEL - YELLOW EDGE LINE
   - YFBL - YELLOW FULL BARRIER LINE
   - YHL - YELLOW HATCH LINE
   - YPBL - YELLOW PARTIAL BARRIER LINE
3. ITEM 686.01000011 - WHITE PREFORMED THERMOPLASTIC REFLECTORIZED PAVEMENT STRIPE
   - WSL - WHITE STOP LINE
   - WXL - WHITE CROSSWALK LINE
4. ITEM 686.03000011 - WHITE PREFORMED THERMOPLASTIC REFLECTORIZED PAVEMENT LETTERS
   - WL - WHITE LETTER
5. ITEM 686.04000011 - WHITE PREFORMED THERMOPLASTIC REFLECTORIZED PAVEMENT SYMBOLS
   - WAS - WHITE ARROW SYMBOL
   - WYS - WHITE YIELD SYMBOL

PAVEMENT MARKING PLAN LEGEND

- LIMITS OF THE ELMWOOD AVENUE COLLEGE TOWN CYCLE TRACK TO BE CONSTRUCTED UNDER SEPARATE CONTRACT D034682 IN THE SPRING OF 2019
- LIMITS OF HMA PAVEMENT MILLING AND RESURFACING

0 20 40 60

Lu Engineers
ENVIRONMENTAL • TRANSPORTATION • CIVIL

PROJECT MANAGER: J. OTTMAN
DRAWN BY: J. MESSENGER

Department of Environmental Services
Architecture and Engineering Services
City of Rochester, New York

ASSISTANT MANAGING ENGINEER: LISA Y. REYES
CITY ENGINEER: HOLLY E. BARRETT, P.E.

PROJECT TITLE
SCOTTSVILLE ROAD - ELMWOOD AVENUE
IMPROVEMENT PROJECT
(WEST CITY LINE - MT HOPE AVE)

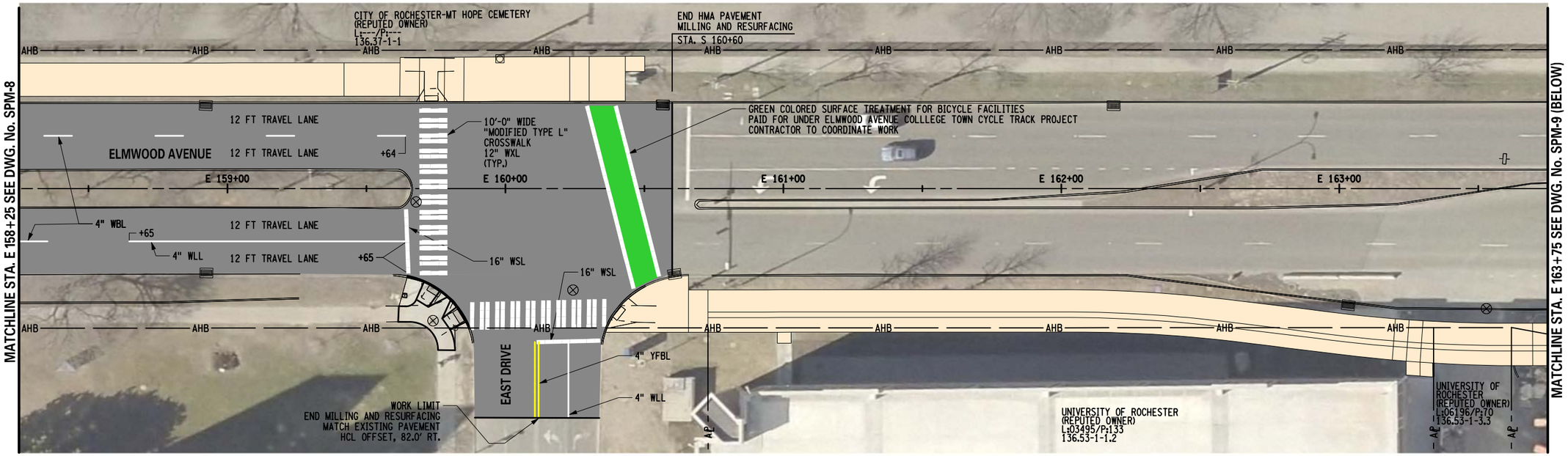
NO.	REVISION	BY	DATE

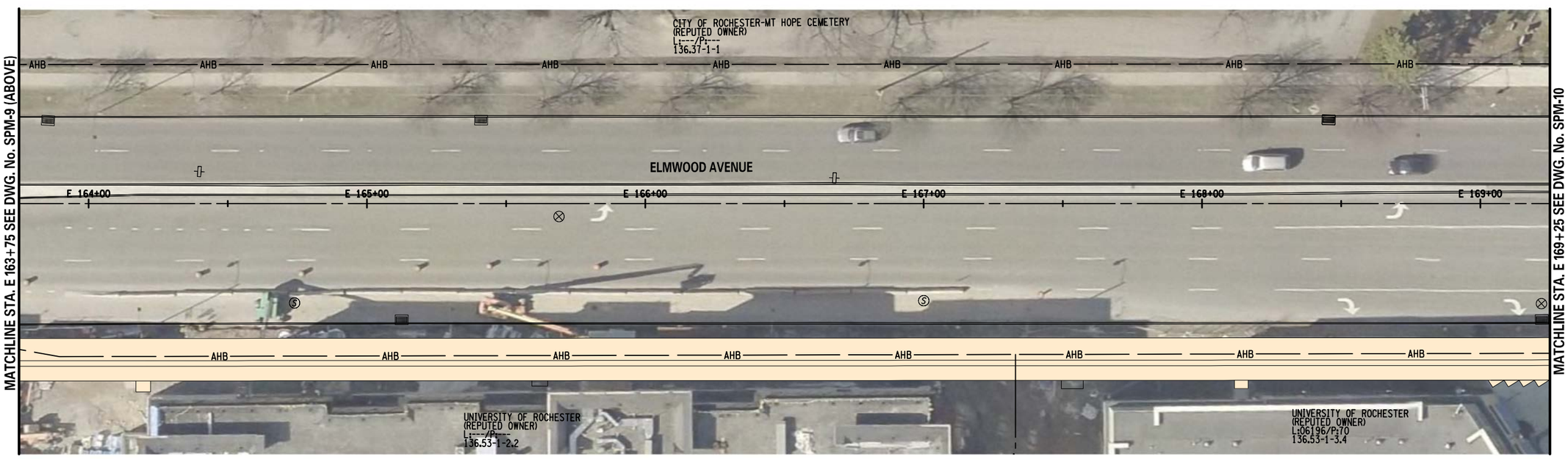
ISSUED 03/06/2019
CHECKED JWO
DRAWN JRM
DESIGN JRM
SCALE 1" = 20'
PROJECT NUMBER 17102

DRAWING TITLE
PAVEMENT MARKING PLAN
PMP-8

DRAWING NO. 52
OF 54

FILE NAME = Striping Plans.dgn
DATE/TIME = 06-MAR-2019 12:00
USER = JMessenger





PAVEMENT MARKING PAY ITEMS:

1. ITEM 685.11 - WHITE EPOXY REFLECTORIZED PAVEMENT STRIPES - 20MILS
   - WLL - WHITE LANE LINE
   - WBL - WHITE BROKEN LINE
   - WDLL - WHITE DOTTED LANE LINE
   - WDEL - WHITE DOTTED EXTENSION LINE
   - WHL - WHITE HATCH LINE
   - WEL - WHITE EDGE LINE
2. ITEM 685.12 - YELLOW EPOXY REFLECTORIZED PAVEMENT STRIPES - 20MILS
   - YEL - YELLOW EDGE LINE
   - YFBL - YELLOW FULL BARRIER LINE
   - YHL - YELLOW HATCH LINE
   - YPBL - YELLOW PARTIAL BARRIER LINE
3. ITEM 686.01000011 - WHITE PREFORMED THERMOPLASTIC REFLECTORIZED PAVEMENT STRIPE
   - WSL - WHITE STOP LINE
   - WXL - WHITE CROSSWALK LINE
4. ITEM 686.03000011 - WHITE PREFORMED THERMOPLASTIC REFLECTORIZED PAVEMENT LETTERS
   - WL - WHITE LETTER
5. ITEM 686.04000011 - WHITE PREFORMED THERMOPLASTIC REFLECTORIZED PAVEMENT SYMBOLS
   - WAS - WHITE ARROW SYMBOL
   - WYS - WHITE YIELD SYMBOL

PAVEMENT MARKING PLAN LEGEND

- LIMITS OF THE ELMWOOD AVENUE COLLEGE TOWN CYCLE TRACK TO BE CONSTRUCTED UNDER SEPARATE CONTRACT D034682 IN THE SPRING OF 2019
- LIMITS OF HMA PAVEMENT MILLING AND RESURFACING

0 20 40 60

Lu Engineers
ENVIRONMENTAL • TRANSPORTATION • CIVIL

PROJECT MANAGER: J. OTTMAN
DRAWN BY: J. MESSENGER

Department of Environmental Services
Architecture and Engineering Services
City of Rochester, New York

ASSISTANT MANAGING ENGINEER: LISA Y. REYES
CITY ENGINEER: HOLLY E. BARRETT, P.E.

PROJECT TITLE
SCOTTSVILLE ROAD - ELMWOOD AVENUE
IMPROVEMENT PROJECT
(WEST CITY LINE - MT HOPE AVE)

NO.	REVISION	BY	DATE

ISSUED 03/06/2019
CHECKED JWO
DRAWN JRM
DESIGN JRM
SCALE 1" = 20'
PROJECT NUMBER 17102

DRAWING TITLE
PAVEMENT MARKING PLAN
PMP-9

DRAWING NO. 53 OF 54

FILE NAME = Striping Plans.dgn
DATE/TIME = 06-MAR-2019 12:01
USER = JMessenger

PAVEMENT MARKING PAY ITEMS:

1. ITEM 685.11 - WHITE EPOXY REFLECTORIZED PAVEMENT STRIPES - 20MILS
   - WLL - WHITE LANE LINE
   - WBL - WHITE BROKEN LINE
   - WDLL - WHITE DOTTED LANE LINE
   - WDEL - WHITE DOTTED EXTENSION LINE
   - WHL - WHITE HATCH LINE
   - WEL - WHITE EDGE LINE
2. ITEM 685.12 - YELLOW EPOXY REFLECTORIZED PAVEMENT STRIPES - 20MILS
   - YEL - YELLOW EDGE LINE
   - YFBL - YELLOW FULL BARRIER LINE
   - YHL - YELLOW HATCH LINE
   - YPBL - YELLOW PARTIAL BARRIER LINE
3. ITEM 686.01000011 - WHITE PREFORMED THERMOPLASTIC REFLECTORIZED PAVEMENT STRIPE
   - WSL - WHITE STOP LINE
   - WXL - WHITE CROSSWALK LINE
4. ITEM 686.03000011 - WHITE PREFORMED THERMOPLASTIC REFLECTORIZED PAVEMENT LETTERS
   - WL - WHITE LETTER
5. ITEM 686.04000011 - WHITE PREFORMED THERMOPLASTIC REFLECTORIZED PAVEMENT SYMBOLS
   - WAS - WHITE ARROW SYMBOL
   - WYS - WHITE YIELD SYMBOL

PAVEMENT MARKING PLAN LEGEND

- LIMITS OF THE ELMWOOD AVENUE COLLEGE TOWN CYCLE TRACK TO BE CONSTRUCTED UNDER SEPARATE CONTRACT D034682 IN THE SPRING OF 2019
- LIMITS OF HMA PAVEMENT MILLING AND RESURFACING

0 20 40 60

Lu Engineers
ENVIRONMENTAL • TRANSPORTATION • CIVIL

PROJECT MANAGER: J. OTTMAN
DRAWN BY: J. MESSENGER

Department of Environmental Services
Architecture and Engineering Services
City of Rochester, New York

ASSISTANT MANAGING ENGINEER: LISA Y. REYES
CITY ENGINEER: HOLLY E. BARRETT, P.E.

PROJECT TITLE
SCOTTSVILLE ROAD - ELMWOOD AVENUE
IMPROVEMENT PROJECT
(WEST CITY LINE - MT HOPE AVE)

NO.	REVISION	BY	DATE

ISSUED 03/06/2019
CHECKED JMO
DRAWN JRM
DESIGN JRM
SCALE 1" = 20'
PROJECT NUMBER 17102

DRAWING TITLE
PAVEMENT MARKING PLAN
PMP-10

DRAWING NO. 54
OF 54

FILE NAME = Striping Plans.dgn
DATE/TIME = 06-MAR-2019 12:01
USER = JMessenger